

LAND USE APPLICATION SUMMARY

Property Location: 2025 West River Rd N
Project Name: 2025 West River Road
Prepared By: Mei-Ling Smith, Senior City Planner, (612) 673-5342
Applicant: SRPB Strategic Housing, LLC
Project Contact: Trevor Martinez
Request: To construct a new seven-story building with 163 dwelling units.

Dwelling Units	163 dwelling units
Non-Residential Uses	Not applicable

Required Applications:

Conditional Use Permit	To increase the maximum height of a principal structure in the SH Shoreland Overlay District and MR Mississippi River Critical Area Overlay District from 2.5 stories or 35 feet, whichever is less, to 7 stories/83 feet, 11.5 inches
Variance	To allow development within 40' of the top of a steep slope in the SH Shoreland Overlay District and MR Mississippi River Critical Area Overlay District
Variance	Of the PO Pedestrian Oriented Overlay District standards to allow the building to be located more than eight (8) feet from the front lot line adjacent to West River Rd N and adjacent to the corner side lot line along West Broadway Ave, and to increase the allowed parking lot street frontage along West River Rd N from 40 to 58 feet
Variance	To increase the maximum size of a kennel or exercise run from 200 square feet to 735 square feet
Site Plan Review	For a new, seven-story building with 163 dwelling units

SITE DATA

Existing Zoning	C3A Community Activity Center District MR Mississippi River Critical Area Overlay District PO Pedestrian Oriented Overlay District SH Shoreland Overlay District
Lot Area	105,846 square feet / 2.43 acres
Ward(s)	5
Neighborhood(s)	Hawthorne Neighborhood Council; adjacent to Northside Residents Redevelopment Council

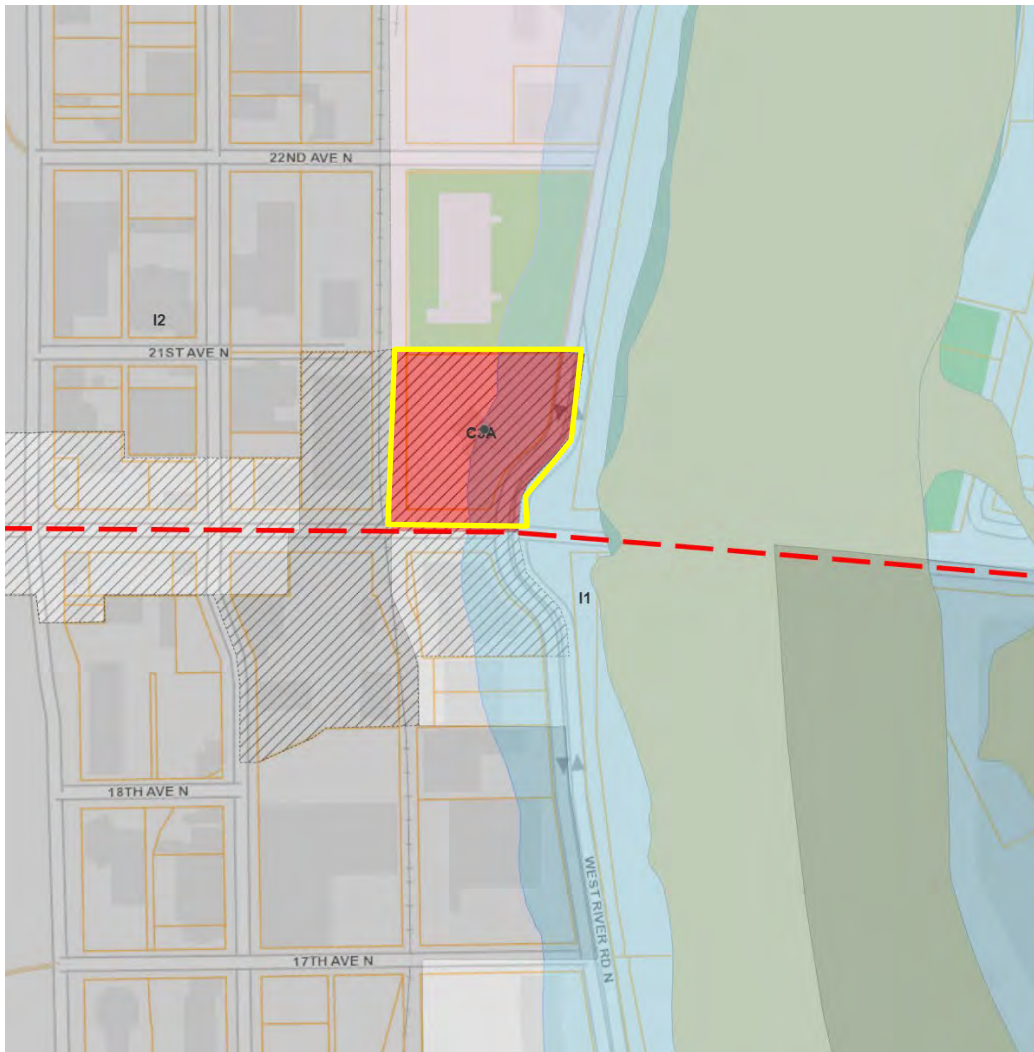
Date Application Deemed Complete	August 27, 2020	Date Extension Letter Sent	Not applicable
End of 60-Day Decision Period	October 25, 2020	End of 120-Day Decision Period	Not applicable

Future Land Use	Neighborhood Mixed Use
Goods and Services Corridor	West Broadway Ave/Broadway St NE
Built Form	Corridor 6

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The subject site is a corner lot at the intersection of West Broadway Ave and West River Rd N. The site faces the Broadway St NE bridge and the Mississippi River, which is between 175 and 345 feet to the east. It has an irregular shape and varied topography, including a steep slope. There is a one-story commercial building currently on the site (Broadway Bar & Pizza) and a surface parking lot that is accessed from a single curb cut from West River Rd N.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The area immediately adjacent to the site contains industrial uses and offices. The Minneapolis Parks and Recreation Board office building is directly to the north of the subject property, and there are railroad tracks bordering the west side of the property.



PROJECT DESCRIPTION. The project would consist of a seven-story residential building with 163 units and 163 vehicle parking stalls (74 below-grade and 89 surface stalls). All units would be affordable and would include one-

, two-, and three-bedroom units. The building is designed with two main wings that are perpendicular to the Mississippi River. All parking would be accessed from a single curb cut off West River Rd N in the northeast portion of the site. The surface parking area has partial frontage on West River Rd N and wraps around the north and west sides of the property, while the proposed building and open space area is located near the corner and street frontages along the south and east. The surface parking area also leads to the below-grade parking area.

Since the May 14, 2020, Committee of the Whole meeting, the applicant has added a cantilevered, illuminated roof feature to add more prominence at this corner of the intersection at West Broadway Ave and West River Rd N. In addition, more window area has been added to the building at the corner.

PUBLIC COMMENTS. Staff has received several public comments in opposition to the proposed project. The Hawthorne Neighborhood Council has submitted a letter of support for the project. Any additional correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to increase the maximum height of a principal structure in the SH Shoreland Overlay District and MR Mississippi River Critical Area Overlay District from 2.5 stories or 35 feet, whichever is less, to 7 stories/83 feet, 11.5 inches, based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The maximum allowed height of a building in the SH Shoreland Overlay District and MR Mississippi River Critical Area Overlay District is 2.5 stories or 35 feet, whichever is less. The height of the proposed building is seven stories, or 83 feet, 11.5 inches, which includes the rooftop elevator overrun, while the roofline is approximately 77.5 feet at its tallest point. The majority of the building along West River Rd will appear to have six stories due to the higher grade elevation on this side of the site. In addition, the applicant states that the building's varied façade and first floor inset will help to create the appearance of a five-story building. The footprint of the building allows for separation between adjacent uses, and the site is surrounded by industrial properties and office buildings. Height, in general, has no impact on public health or safety; allowing an additional 4.5 stories above the permitted height would not be detrimental to or endanger the public health, safety, comfort or general welfare provided the development complies with all applicable building codes, life safety ordinances, and Public Works Department standards.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The area immediately adjacent to the site contains industrial uses and offices in the I1 Light Industrial and I2 Medium Industrial Districts. The Minneapolis Parks and Recreation Board office building is directly to the north of the subject property, and there are railroad tracks bordering the west side of the property. The subject site is the only one on the block that has commercial zoning. The increased height would not be injurious to the use and enjoyment of other property in the vicinity or impede the normal or orderly development and improvement of surrounding property.

The development would include substantial site improvements that would lead to an enhancement relative to current conditions. The proposed building would provide pedestrian-scale activity along West Broadway

Ave and would provide visual interest along West River Rd N. The building would exceed the minimum window requirements on both street-facing elevations. Approximately 34 percent of the site (35,684 square feet) would be landscaped, whereas only 14,499 square feet is required through Chapter 530, Site Plan Review. These measures will help to mitigate the overall height of the building.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

Increasing the height of the proposed building will not have an impact on utilities, access roads, or drainage. The applicant has prepared a comprehensive utility and drainage plan for the subject site. The applicant will be required to continue to work closely with the Public Works Department, the Plan Review Section of CPED and the various utility companies during the duration of the development to ensure that all procedures are followed and that the development complies with all City and other applicable requirements.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

Increasing the height of the proposed building would have a minimal impact on traffic congestion in the public streets. All vehicles would access the site from the east side of the property. The development includes a total of 163 on-site vehicle parking spaces and 156 bicycle parking spaces serving the 163 dwelling units. In addition, the applicant is proposing to add a new sidewalk and bumpouts along the east side of the property which will narrow the width of West River Rd and improve traffic and pedestrian safety conditions. The submitted Travel Demand Management Plan indicates that traffic generated by the proposed development is not expected to have a significant negative impact on traffic in the area. Adequate measures will be taken to minimize traffic congestion in the public streets.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The site is located in the Corridor 6 built form category, which calls for building heights of between two and six stories. The proposed use would be consistent with the applicable guidance and policies of Minneapolis 2040 (2020):

Future Land Use	Guidance	Staff Comment
Neighborhood Mixed Use	Neighborhood Mixed Use includes individual commercial uses and small collections of commercial uses, located primarily away from major streets, that should continue to serve their existing commercial function. Commercial zoning is appropriate, while expansion of commercial uses and zoning into surrounding areas is not encouraged. Semi-permanent or temporary commercial retail establishments such as farmer's markets are also appropriate in this category.	The proposed building does not contain any commercial uses at this time. This category does allow for residential uses on all levels, including the ground floor.
Goods and Services Corridor	Guidance	Staff Comment
West Broadway Ave / Broadway St NE	Goods and Services Corridors serve two purposes: 1) To indicate where commercial uses should front in relation to properties guided for commercial future land uses, and 2) In addition to the guidance for the	The site is located along a Goods and Services corridor to the south, West Broadway Ave. The comprehensive plan encourages

	mixed use land use categories found in this section, Goods and Services Corridors identify where the establishment or expansion of commercial uses can be considered. Properties immediately adjacent to a Goods and Services Corridor may be considered for commercial activity, allowing for uses similar in scale and scope to the Neighborhood and Corridor Mixed Use categories.	the establishment of commercial uses on this side of the property.
Built Form	Guidance	Staff Comment
Corridor 6	New and remodeled buildings in the Corridor 6 district should reflect a variety of building types on both moderate and large sized lots. Building heights should be 2 to 6 stories. Building heights should be at least 2 stories in order to best take advantage of the access to transit, jobs, and goods and services provided by the Corridor 6 district. Requests to exceed 6 stories will be evaluated on the basis of whether or not a taller building is a reasonable means for further achieving Comprehensive Plan goals.	The proposed building would be 7 stories at its tallest point, which exceeds the guidance for buildings between 2 and 6 stories for this site. Staff finds that the additional height is a reasonable means to further comprehensive plan goals, as detailed below.

The following policies and action steps from Minneapolis 2040 (2020) apply to this proposal:

Policy 1. Access to Housing: Increase the supply of housing and its diversity of location and types.

- a. Allow housing to be built in all areas of the city, except in Production and Distribution areas.
- b. Allow the highest-density housing in and near Downtown.
- c. Allow multifamily housing on public transit routes, with higher densities along high-frequency routes and near METRO stations.
- f. Encourage inclusion of units that can accommodate families in new and rehabilitated multifamily housing developments.

Policy 5. Visual Quality of New Development: Ensure a high-quality and distinctive physical environment in all parts of the city through building and site design requirements for both large and small projects.

- a. Allow and encourage a variety of architectural styles.
- b. Require multiple buildings on development sites outside of the downtown core that encompass most of an entire block or block frontage to increase visual interest. On sites in the downtown core buildings that encompass an entire city block are encouraged to use massing that is responsive to the human-scale and provides pedestrian through-block connections through existing super blocks or to re-establish the street grid.
- c. Ensure that exterior building materials are durable, sustainable, create a lasting addition to the built environment, and contribute positively to the public realm and reflect existing context.
- d. Require that the appearance and materials of the rear and side walls of new buildings are similar to and compatible with the front of the building.

- e. Require adequate distribution of windows and architectural features in order to create visual interest.
- f. Consider design approaches that encourage creative solutions for transitions between varying intensities of building types and land uses.
- g. Apply design standards, guidance, and regulation consistently across the city regardless of market conditions or rent structure of development.
- j. Require the screening of utilities, communication, transformers, and other service connections to buildings. Burying connections and lines is encouraged.
- k. Encourage roof lines and upper levels of tall buildings to be articulated with a distinguishable design.

Policy 6. Pedestrian-Oriented Building and Site Design: Regulate land uses, building design, and site design of new development consistent with a transportation system that prioritizes walking first, followed by bicycling and transit use, and lastly motor vehicle use.

- a. Orient buildings and building entrances to the street. Encourage multiple entrances to multi-family residential buildings. The number of entrances in non-residential uses should increase in proportion to the length of the building and be located along main corridors or at the street corner.
- c. Require windows and window treatments on buildings that allow clear views into and out of the building.
- d. Ensure that buildings incorporate design elements that eliminate long stretches of blank, inactive exterior walls through provision of windows, multiple entrance doors, green walls, and architectural details.
- e. Integrate components in building designs that offer seasonal protection to pedestrians, such as awnings and canopies, to encourage pedestrian activity along the street.
- f. Consider the arrangement of buildings within a site to minimize the generation of wind currents at ground level.
- i. Coordinate site designs and public right-of-way improvements to provide adequate sidewalk space for pedestrian movement, curb extensions, street trees, street lighting, landscaping, street furniture, sidewalk cafes, and other elements of active pedestrian areas.
- k. Implement and expand regulations and incentives that promote bicycling, such as the provision of secured storage for bikes near building entrances, storage lockers, and changing and shower facilities.
- y. Encourage building designs that reflect the unique site and context where they lie within the city.

Policy 7. Public Realm: Proactively improve the public realm to support a pedestrian friendly, high-quality and distinctive built environment.

- c. Require developers to implement public realm improvements called for in the citywide framework in conjunction with approvals for building construction and site modification.

Policy 8. Public Safety Through Environmental Design: Use design principles that ensure a safe and welcoming environment when designing all projects that impact the public realm, including open spaces and parks, on publicly-owned and private land.

- a. Integrate “eyes on the street” concepts into building design through the use of windows to foster safer and more successful areas throughout the city.
- b. Orient new development to the street, or other public ways, to foster safe neighborhoods.
- c. Design the site, lighting, landscaping, and buildings to promote natural observation and maximize the opportunities for people to observe adjacent spaces and public sidewalks.

- d. Provide on-site, non-glare producing lighting at all building entrances and along walkways that maintains a minimum acceptable level of security while not creating excessive lighting of the site.
- e. Locate landscaping, sidewalks, lighting, fencing, and building features to clearly guide pedestrian movement on or through the site and to provide clear delineation between public and private spaces.

In addition, staff finds that the additional height is needed to advance specific comprehensive plan goals from *Minneapolis 2040 (2020)*. In particular, the additional story beyond what is called for in the Corridor 6 built form category is needed to achieve the plan's goals related to affordable and accessible housing:

- Goal 3. Affordable and accessible housing: In 2040, all Minneapolis residents will be able to afford and access quality housing throughout the city.
- Goal 6. High-quality physical environment: In 2040, Minneapolis will enjoy a high-quality and distinctive physical environment in all parts of the city.
- Goal 9. Complete neighborhoods: In 2040, all Minneapolis residents will have access to employment, retail services, healthy food, parks, and other daily needs via walking, biking, and public transit.

The site is located within the boundaries of the Mississippi River Corridor Critical Area Plan in *Minneapolis 2040* (Appendix A). The site is identified as Urban Mixed District (CA-UM), which calls for a structure height maximum of 65 feet unless increased by conditional use permit according to the required findings:

1. *Assessment of the visual impact of the proposed building on public river corridor views, including view from other communities;*
2. *Identification and application of techniques to minimize the perceived bulk of the proposed building, such as:*
 - a. *Placing the long axis of the building perpendicular to the river;*
 - b. *Stepping back portions of the façade;*
 - c. *Narrowing the profile of upper floors of the building; or*
 - d. *Increasing the setbacks of the building from the Mississippi River or blufflines;*
3. *Identification of techniques for preservation of those view corridors identified in the local government's plan; and*
4. *Opportunities for creation or enhancement of public river corridor views.*

Staff finds that the proposed building successfully meets the above criteria. The footprint of the proposed structure, with two wings that are perpendicular to the Mississippi River, the stepped back portions of the façade, and the increased setbacks of the building from the river are all consistent with the findings that must be met in order to allow a building taller than 65 feet in the CA-UM portion of the Mississippi corridor. The building will not hinder the view corridors that are identified in the plan.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use applications are approved, the proposal will comply with all provisions of the C3A, PO, SH, and MR districts.

Additional Standards to Increase Maximum Height

In addition to the conditional use permit standards, the Planning Commission shall consider, but not be limited to, the following factors when determining the maximum height of principal structures in commercial districts:

1. *Access to light and air of surrounding properties.*

The development addresses light and air of surrounding properties by varying the massing and building setbacks of the structures on the site. The footprint of the building allows for separation between adjacent uses, and the site is surrounded by industrial properties, railroad tracks, an office building, which is approximately 70 feet to the north, and public streets along the east and south. It is located at an intersection next to the Broadway Ave bridge and the West River Rd. It is not directly adjacent to any residential properties, and the additional height should not impede access to the light and air that the surrounding properties receive.

2. *Shadowing of residential properties, significant public spaces, or existing solar energy systems.*

The applicant has submitted a shadowing study that demonstrates the proposed development's impacts on nearby properties during the summer solstice, spring equinox, summer solstice, and winter solstice at 9 am, noon, and 3 pm in each scenario. The shadow study indicates that there would be some shadowing of industrial structures to the west of the property during the morning of the winter solstice. The direction and intensity of the shadowing varies throughout the day and there would be very few shadowing impacts at other times of day or in other seasons throughout the year.

There are no significant public spaces or existing solar energy systems in the immediate vicinity that would be affected by the development; the nearest solar energy system is over 1,500 feet to east of the site on the opposite side of the river. The proposed project would not shadow this solar energy system.

3. *The scale and character of surrounding uses.*

The area immediately adjacent to the site contains industrial uses and offices in the I1 Light Industrial and I2 Medium Industrial Districts. The Minneapolis Parks and Recreation Board office building is directly to the north of the subject property, and there are railroad tracks bordering the west side of the property. There is a multifamily residential development on the north end of the same block. The block on which the building would be located does not retain a consistent character in scale nor in use. The proposed building will use durable materials, including brick, concealed fastener metal panel, and glass, to ensure compatibility with the character and scale of the surrounding industrial, office, and multiple-family residential uses in the area.

4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*

The proposed development will not impede views of landmark buildings, significant open spaces, or bodies of water.

Additional Standards for Conditional Use Permits within the SH Shoreland Overlay District

In addition, the Planning Commission shall consider, but not be limited to, the following factors when considering conditional use permit or variance requests within the SH Shoreland Overlay District:

1. *The prevention of soil erosion or other possible pollution of public waters, both during and after construction.*

The applicant would be required to comply with all applicable City regulations during construction, and erosion control methods would be expected to be in place while implementing all site modifications. According to the applicant's materials, the project does include site grading and landscaping to stabilize the soil conditions for the site as a whole and to protect the nearby public waters.

2. *Limiting the visibility of structures and other development from protected waters.*

The proposed seven-story building would have some visibility from the Mississippi River, which is located between 175 and 345 feet to the east of the subject site. There are trees, a parkway, a public street, and a

slope separating the subject site from the protected waters. The applicant has varied the building's footprint to further limit the visibility of the development from the river.

3. *The suitability of the protected water to safely accommodate the types, uses and numbers of watercraft that the development may generate.*

This standard is not applicable for the proposed development.

VARIANCE – STEEP SLOPE

The Department of Community Planning and Economic Development has analyzed the application for a variance to allow development within 40' the top of a steep slope in the SH Shoreland Overlay District and MR Mississippi River Critical Area Overlay District based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Staff finds that practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The site is unique in that the majority of the property is located within 40 feet of the top of a steep slope and, as a result, any new structures on the property would likely require a variance. In addition, the applicant is trying to comply with the policies which encourage active building walls in close proximity to both the east and south property lines, which are adjacent to public streets. The circumstances which necessitate the variance request create a practical difficulty in complying with the ordinance and are unique to the parcel and were not created by the current property owner and applicant. These circumstances were not created by the property owner, nor are they based on economic considerations alone.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The proposal to build within 40 feet of the top of a steep slope is reasonable and in keeping with the spirit and intent of the ordinance. The project would be located on a portion of the property that already contains development. The proposed development will improve upon the current stormwater management conditions on the site. The redevelopment would not compromise the intent of the SH Shoreland or MR Mississippi River Overlay Districts to preserve, protect, and enhance the shoreland and Mississippi River corridor areas and protect the public health, safety, and welfare.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

The proposed variance would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. The new building has been designed to be compatible with the scale and character of the surrounding neighborhood. Provided that the building is constructed in compliance with all applicable building codes, the proposed variance to allow development should not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

Additional Standards for Variances within the SH Shoreland Overlay District

In addition, the Planning Commission shall consider, but not be limited to, the following factors when considering conditional use permit or variance requests within the SH Shoreland Overlay District:

1. *The prevention of soil erosion or other possible pollution of public waters, both during and after construction.*

The responses to these findings in the conditional use permit section also apply to the variance.

2. *Limiting the visibility of structures and other development from protected waters.*

The responses to these findings in the conditional use permit section also apply to the variance.

3. *The suitability of the protected water to safely accommodate the types, uses and numbers of watercraft that the development may generate.*

The responses to these findings in the conditional use permit section also apply to the variance.

Additional Standards To Permit Development On A Steep Slope, Within 40 Feet Of The Top Of A Steep Slope Or Bluff, Or Within 50 Feet Of The Ordinary High Water Mark Of Any Protected Water in the SH Shoreland Overlay District:

In addition, the City Planning Commission shall consider, but not be limited to, the following factors when considering variance requests to allow development on a steep slope, within 40 feet of the top of a steep slope or bluff, or within 50 feet of the ordinary high water mark of any protected water within the SH Shoreland Overlay District:

1. *Development must currently exist on the steep slope or within forty (40) feet of the top of a steep slope within five hundred (500) feet of the proposed development.*

The existing development on the site is currently located within forty feet of the top of the steep slope.

2. *The foundation and underlying material shall be adequate for the slope condition and soil type.*

The foundation and material underlying the proposed building would be adequate for the slope condition and soil type.

3. *The development shall present no danger of falling rock, mud, uprooted trees or other materials.*

The impact to the existing slope would be minimal. The steep slope is currently covered with turf grass and there are no existing trees on the site. The development does not present a danger of falling rock, mud, or uprooted trees. The applicant has stated that they will follow best management practices to control erosion and sediment during construction and manage stormwater to minimize the degradation of surface water.

4. *The view of the developed slope from the protected water shall be consistent with the natural appearance of the slope, with any historic areas, and with the surrounding physical context.*

There would be limited visibility of this slope from the protected water, the Mississippi River. The proposed development on the slope would be consistent with the natural appearance of the slope and with the surrounding neighborhood, and the applicant is proposing to install native plantings on the slope where it is currently grass. Staff recommends a condition that the landscaping plan shall be implemented for the slope area as proposed.

VARIANCE – PEDESTRIAN ORIENTED OVERLAY DISTRICT

The Department of Community Planning and Economic Development has analyzed the application for a variance of the PO Pedestrian Oriented Overlay District standards to a) allow the building to be located more than eight (8) feet from the front lot line adjacent to West River Rd N and adjacent to the corner side lot line along West Broadway Ave, and b) to increase the allowed parking lot street frontage along West River Rd N from 40 to 58 feet, based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

Both variances

The requirements that the first floor of a building shall be placed not more than eight (8) feet from a front or corner side lot line and which limit parking lot street frontage to a maximum of forty (40) feet are intended to reinforce the street wall, maximize natural surveillance and visibility, facilitate pedestrian access and circulation, and encourage the pedestrian character and street life in the PO Pedestrian Oriented Overlay District. This site is unique in that it has an unusual shape with a curvilinear front property line. There is also a significant grade change across the site of approximately 13 feet. In addition, there is a utility easement along the West Broadway Ave side of the property that makes it impossible to develop the building within eight feet of the corner side lot line. These circumstances make it difficult to develop the property in a way that would result in a continual street wall along multiple frontages. The applicant is trying to balance these constraints while also screening the parking as much possible. If the overall parking count were reduced, a variance of the minimum parking requirement would be required. This unique combination of policies make it difficult to fully comply with the first floor building placement and parking lot standards.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

Both variances

The existing site contains a building that is in the middle of the property. The proposed development would bring the property into greater compliance with the PO standards in terms of building placement. While the building is set back between 10 and 15 feet from the south corner side property line adjacent to West Broadway Ave, it provides a continuous street frontage along this side of the site while also providing a landscaped buffer between the sidewalk and the residential use. The increased setback provides a safer and more pedestrian-friendly experience along this side of the site. The first floor of the building is located between 14 feet and 154 feet from the front line abutting West River Rd N to accommodate a staggered building footprint for the reasons described above in finding #1. The parking lot frontage of 58 feet is not excessive given the overall scale of the site. The applicant is also proposing a 43-foot wide buffer with dense landscaping between the parking lot and the front property line to reduce the visibility of the parking area. Staff recommends a condition of approval that the surface parking area shall not be closer than 43 feet to the front property line, as proposed.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Both variances

The proposed variances of the PO standards would not alter the essential character of the locality of be injurious to the use or enjoyment of other property in the vicinity, nor would they be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

VARIANCE – EXERCISE RUN

The Department of Community Planning and Economic Development has analyzed the application for a variance to increase the maximum size of a kennel or exercise run from 200 square feet to 735 square feet based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

CPED staff finds that practical difficulties exist in complying with the ordinance because of the circumstances unique to the property. The zoning code requirement that limits the size of the dog run to 200 square feet is intended to restrict the amount of area on a residential parcel that can be used for a kennel or exercise run for both long and short periods of time. The applicant's proposal is unique in that the dog run would be located next to an industrial use and railroad tracks. In addition, the property's varied topography also limits the space that might otherwise be available for pet relief areas. These circumstances are unique to the property and were not created by the applicant, nor are they based on economic considerations, alone.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The increased size of the exercise run would provide ample space for residents of the 163 units in the building to provide outdoor space for their pets in an area while maintaining separation from other uses. The subject property is over two acres in size and the dog run would be separated from the residential units on-site by landscaping, a walkway, and a driveway. It would be located next to the west property line, which is adjacent to railroad tracks and an industrial use. The applicant is proposing a five-foot fence and landscaping around the perimeter of the exercise run to provide screening. These site improvements would comply with the spacing and screening requirement in section 537.110. The applicant's proposal is reasonable and would be in keeping with the spirit and intent of the ordinance and comprehensive plan.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting the variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. The proposed variance to increase the allowable size of a dog run to 735 square feet would not be detrimental to health, safety, or welfare of the general public or of those utilizing the property or nearby properties, provided that the dog run complies with the standards in section 537.110 of the zoning code. Staff recommends a condition of approval that requires the final site plan to show the location of the animal waste containers.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

Applicable Standards of Chapter 530, Site Plan Review

BUILDING PLACEMENT AND DESIGN

Building placement – Requires variance(s) and alternative compliance

- The first floor of the building is located between 14 feet and 154 feet from the front lot line abutting West River Rd N, and between 10 and 15 feet from the corner lot line abutting West Broadway. Alternative compliance and a variance are required to allow the building placement more than 8 feet from the front and corner side lot lines.

Principal entrances – Meets requirements

- The proposed project would comply with the principal entrance standards.

Visual interest – Meets requirements

- The proposed project would comply with the visual interest standards.

Exterior materials – Meets requirements

- The applicant is proposing concealed fastener metal panel, brick veneer, and fiber cement panel as the building's primary exterior materials. The application is consistent with the City's policy of allowing no more than three exterior materials per elevation, excluding windows, doors, and foundation materials. Exterior material changes at a later date may require review by the Planning Commission and an amendment to the site plan review.
- Each elevation would comply with the City's durability standards for exterior materials. The table below shows the proportion of materials on each elevation. There are also courtyard-facing elevations with similar distributions of materials and which are consistent with the durability standards.

Percentage of Exterior Materials per Elevation

Material	Allowed Max	South	East	West	North
Glass	100%	35.6%	36.5%	36%	34.6%
Brick	75%	12.6%	7.4%	7.8%	7.8%
Metal Panel	75%	42.9%	51.9%	51.2%	50.7%
Fiber Cement Panel	30%	8.9%	4.2%	5%	6.9%

Windows – Meets requirements

- The proposed project would comply with the minimum window requirements.

Window Requirements for Residential Uses

Floor	Code		Proposed
1st floor (East)	20% minimum	106 sq. ft.	Exceeds 20%
2nd floor and above	10% minimum	274 sq. ft.	Exceeds 10%
1st floor (South)	20% minimum	281 sq. ft.	Exceeds 20%
2nd floor and above	10% minimum	198 sq. ft.	Exceeds 10%
1st floor (North facing parking)	20% minimum	365 sq. ft.	Exceeds 20%
2nd floor and above	10% minimum	251 sq. ft.	Exceeds 10%
1st floor (West facing parking)	20% minimum	50 sq. ft.	Exceeds 20%
2nd floor and above	10% minimum	36 sq. ft.	Exceeds 10%

Ground floor active functions – Meets requirements

- The proposed project would comply with the ground floor active functions requirements.

Roof line – Meets requirements

- The principal roof line of the building would be similar to that of surrounding buildings.

Parking garages – Meets requirements

- The proposed parking garage would comply with the applicable site plan review standards.

ACCESS AND CIRCULATION**Pedestrian access – Meets requirements**

- There would be clear and well-lit walkways at least four feet in width connecting building entrances to the adjacent public sidewalk and on-site parking facilities.

Transit access – Not applicable

- No transit shelters are proposed as part of this development.

Vehicular access – Meets requirements

- The proposed project would comply with the vehicular access requirements.

LANDSCAPING AND SCREENING**General landscaping and screening – Meets requirements**

- The proposed project would comply with the general landscaping and screening requirements.

Landscaping and Screening Requirements

Requirement	Required	Proposed
Lot Area	--	105,846 sq. ft.
Building Footprint	--	33,351 sq. ft.
Area Not Covered by Buildings	--	72,495 sq. ft.
Landscaped Area	14,499 sq. ft.	35,684 sq. ft.
Canopy Trees (1:500 sq. ft.)	29 trees	53 trees
Shrubs (1:100 sq. ft.)	145 shrubs	1,046 shrubs

Parking and loading landscaping and screening – Requires alternative compliance

- The parking and loading area facing the public street or public sidewalk contains an on-site landscaped yard of at least seven feet in width.
- The applicant is proposing a three-foot screen consisting of a hedge that is at least 60 percent opaque.
- The applicant is not providing at least one canopy tree for each 25 linear feet, or fraction thereof, of parking area frontage. The trees proposed between the parking lot and West River Rd N (Autumn Brilliance Serviceberry) are considered ornamental trees, not canopy trees, as they do not reach at least 35 feet at maturity. Alternative compliance is requested.
- The corners of the parking lot that are unavailable for parking or vehicular circulation are not landscaped as specified for a required landscaped yard. Alternative compliance is requested.
- Because the proposed surface parking lot contains ten or more spaces, each parking space must be located within 50 feet of the center of an on-site deciduous tree. This standard is not met along the west side of the parking area, and alternative compliance is requested.

Additional landscaping requirements – Meets requirements

- The project appears to comply with the additional landscaping requirements in sections 530.180, 530.190, 530.200, and 530.210 of the zoning code.

ADDITIONAL STANDARDS

Concrete curbs and wheel stops – Meets requirements

- The subject site would be designed to provide on-site retention and filtration of stormwater.

Site context – Meets requirements

- The proposed project would comply with the site context requirements. The nearby Mississippi River will not be obstructed by the proposed building, and the proposed building should have minimal shadowing effects on public spaces and adjacent properties.

Crime prevention through environmental design – Meets requirements

- The proposed project would comply with crime prevention through environmental design (CPTED) standards.

Historic preservation – Meets requirements

- This site is neither historically designated or located in a designated historic district, nor has it been determined to be eligible for designation.

Applicable Regulations of the Zoning Ordinance

The proposed use is permitted in the C3A district.

Off-street Parking and Loading – Meets requirements

- The proposed project complies with the applicable vehicle parking, bicycle parking, and loading requirements.

Vehicle Parking Requirements Per Use (Chapter 541)

Use	Minimum	Reductions	Minimum	Maximum	Proposed
Residential Dwellings	163	None	163	N/A	163

Bicycle Parking Requirements (Chapter 541)

Use	Minimum	Short-Term	Long-Term	Proposed
Residential Dwellings	82	N/A	Not less than 90% (73)	156 (146 long-term)

Loading Requirements (Chapter 541)

Use	Loading Requirement	Proposed
Residential Dwellings	1 small loading space	1 small loading space

Building Bulk and Height – Requires conditional use permit

- The proposed project would require a conditional use permit to increase the maximum allowed height in the SH and MR overlay districts.

Building Bulk and Height Requirements

Requirement	Code	Bonuses	Total	Proposed
Lot Area	5,000 sq. ft.	--	--	105,846 sq. ft. / 2.43 acres
Gross Floor Area	--	--	--	234,555 sq. ft.
Min. Floor Area Ratio	1.0	--	--	2.2
Max. Floor Area Ratio	2.7	--	--	
Max. Building Height	2.5 stories or 35 feet, whichever is less			7 stories ft. / 83 feet, 11.5 inches

Lot and Residential Unit Requirements – Meets requirements

- The proposed project would meet the applicable lot and residential unit requirements.
- Inclusionary housing is not required, as all units are proposed as affordable. The applicant will be required to work with CPED Housing to ensure that the applicable requirements are met.

Lot and Residential Unit Requirements Summary

Requirement	Code	Proposed
Lot Area	5,000 sq. ft. min.	105,846 sq. ft. / 2.43 acres
Lot Width	40 ft. min.	400 ft.
Impervious Surface Area	100% max.	35%
Lot Coverage	100% max.	32%
Dwelling Units (DU)	--	163 DUs
Net Residential Area	--	155,151 sq. ft.

Yard Requirements – Not applicable

- The project is not subject to minimum yard requirements.

Signs – Meets requirements with Conditions of Approval

- All signs are subject to Chapter 543, On-Premise Signs. The applicant will be required to submit a separate sign permit application for any signage that is proposed.

Screening of Mechanical Equipment – Meets requirements

- Mechanical equipment is subject to the screening requirements of Chapter 535 and district requirements.
- There are rooftop mechanical units that would be fully screened, as well as a transformer on the north side of the building that would be screened with landscaping.

Refuse Screening – Meets requirements

- Refuse and recycling storage containers are subject to the screening requirements in Chapter 535.
- All refuse and recycling storage containers are located within the building.

Lighting – Meets requirements

- Existing and proposed lighting must comply with Chapter 535 and Chapter 541 of the zoning code.

- The project is consistent with the applicable lighting requirements.

Fences – Meets requirements

- Fences must comply with the requirements in Chapter 535. The proposed fencing meets the applicable standards.

Specific Development Standards – Not applicable

- The applicant's proposal is not subject to specific development standards in Chapter 536.

PO Pedestrian Oriented Overlay District Standards – Requires variance(s)

- The project requires a variance to allow the building to be placed more than 8 feet from a front or corner side lot line, as well as a variance to allow more than 40 feet of parking lot frontage along West River Rd N in the PO Pedestrian Oriented Overlay District.
- The proposal is otherwise in compliance with the PO Pedestrian Oriented Overlay District standards, including those of the West Broadway area.

Applicable Policies of the Comprehensive Plan

The proposed use would be consistent with the applicable guidance and policies of Minneapolis 2040 (2020), as described in finding #5 of the conditional use permit application analysis.

Alternative Compliance

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

Standard	Description	Staff Recommendation
Building placement	The first floor of the building is located between 14 feet and 154 feet from the front lot line abutting West River Rd N, and between 10 and 15 feet from the corner lot line abutting West Broadway. Alternative compliance and a variance are required to allow the building placement more than 8 feet from the front and corner lot lines.	As described in detail in the variance findings, staff is also <u>recommending granting alternative compliance</u> for the building placement due to challenges related to the varying grade, utility easements, and conflicting policies that apply to this site.
Linear tree requirement	The applicant is not providing at least one canopy tree provided for each 25 linear feet, or fraction thereof, of parking area frontage. The trees proposed between the parking lot and West River Rd N (Autumn Brilliance Serviceberry) are considered ornamental trees, not canopy trees, as they do not reach at least 35 feet at maturity.	<u>Staff does not recommend granting alternative compliance.</u> Staff recommends a condition of approval that the final landscaping plan shall be revised so that no fewer than two canopy trees shall be planted within the landscaped area between the parking lot and the east property line.

Corners of parking lots	The corners of the parking lot that are unavailable for parking or vehicular circulation are not landscaped as specified for a required landscaped yard.	<u>Staff does not recommend granting alternative compliance</u> , and recommends a condition of approval that the corners of the parking lot that are unavailable for parking or circulation shall meet the minimum tree and shrub ratios as specified in section 530.160.
50-foot tree requirement	Because the proposed surface parking lot contains ten or more spaces, each parking space must be located within 50 feet of the center of an on-site deciduous tree. This standard is not met along the west side of the parking lot.	The applicant is proposing 53 canopy trees, where 29 are required. Staff finds that the 24 trees in excess of the minimum requirement is a reasonable alternative to meeting the 50-foot tree requirement throughout the parking area, and <u>recommends granting alternative compliance</u> with the condition that no fewer than 53 canopy trees shall be included in the final landscaping plan.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by Trevor Martinez for the property located at 2025 West River Rd N:

A. Conditional Use Permit.

Recommended motion: **Approve** the conditional use permit to increase the maximum height of a principal structure in the SH Shoreland Overlay District and MR Mississippi River Critical Area Overlay District from 2.5 stories or 35 feet, whichever is less, to 7 stories/83 feet, 11.5 inches, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

B. Variance to allow development on a steep slope.

Recommended motion: **Approve** the variance to allow development within 40' of the top of a steep slope in the SH Shoreland Overlay District and MR Mississippi River Critical Area Overlay District, subject to the following conditions:

1. The landscaping plan shall be implemented on the slope between the building and front and corner property lines as proposed.

C. Variance of the PO Pedestrian Oriented Overlay District standards.

Recommended motion: **Approve** the variance of the PO Pedestrian Oriented Overlay District standards to allow the building to be located more than eight (8) feet from the front lot line adjacent to West River

Rd N and adjacent to the corner side lot line along West Broadway Ave, and to increase the allowed parking lot street frontage along West River Rd N from 40 to 58 feet, subject to the following conditions:

1. The surface parking area shall not be closer than 43 feet to the front property line, as proposed.

D. Variance to increase the size of a kennel or exercise run.

Recommended motion: **Approve** the variance to increase the maximum size of a kennel or exercise run from 200 square feet to 735 square feet, subject to the following conditions:

1. The pet exercise area shall contain accommodations for the proper disposal of animal waste.

E. Site Plan Review.

Recommended motion: **Approve** the site plan review for a new, seven-story building with 163 dwelling units, subject to the following conditions:

1. All site improvements shall be completed by September 21, 2020, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
2. CPED staff shall review and approve the final site, elevation, landscaping, and lighting plans before building permits may be issued.
3. All signs shall comply with Chapter 543 of the zoning code. All signage requires a separate permit from CPED.
4. The final landscaping plan shall be revised so that no fewer than two canopy trees shall be planted within the landscaped area between the parking lot and the east property line and so that the corners of the parking lot that are unavailable for parking or circulation shall meet the minimum tree and shrub ratios as specified in section 530.160 and 530.170 of the zoning code.
5. The final landscaping plan shall include no fewer than 53 canopy trees, as proposed.

ATTACHMENTS

1. Oblique aerial photo
2. Written description and findings submitted by applicant
3. Photos
4. Survey
5. Plans
6. Building elevations
7. Renderings
8. Shadow study
9. Travel Demand Management Plan Report
10. Letter from the Hawthorne Neighborhood Council
11. Public comments

Diamond Vogel
Paint Store
Paint store

Minneapolis Park
& Recreation

ChargePoint
Charging Station

2025 West River
Road North
18 min drive-home

Diamond Vogel
Manufacturing

W Broadway Ave

W Broadway Ave

Broadway St NE

Red Circle Agency
Max Home
Care Services

arelick Steel Company

DC Group

2025 West River Road Project Description

2025 West River Road is located at the corner of West River Road and West Broadway Avenue in the Hawthorne neighborhood of Minneapolis. The site size is 2.43 acres and it currently consists of a one-story commercial building, constructed in 1974, and a large surface parking lot. The site is an irregular shape with varied topography, including a grade change of about 15' from the highest to lowest points. The site is adjacent to the Minneapolis Park and Recreation Board office to the north; a railway and industrial uses to the west; Broadway Street to the south; and West River Road, parkland, and the Mississippi River to the east.

The 2025 West River Road development will consist of a single seven-story building with multifamily housing on levels two through seven. In total, there will be 163 one, two, and three-bedroom units, attracting both individuals and families. Level one will be partially below ground and consist of enclosed parking, residential amenities, on-site management space, and a bike storage room. In addition, there will be a community-facing space of approximately 2,000-square feet that will be accessible from Broadway Avenue by the public; programming for this space is in progress. Exterior amenities will be accessed from the second level and include a courtyard patio and a tot lot. There will be a total of 163 parking stalls in the project, 74 enclosed stalls and 89 surface parking stalls, which will serve building residents and guests. Vehicular access will utilize the existing curb cut on the Northeast corner of the site off of West River Road. The secured bicycle storage room will provide parking for approximately 80 bicycles with convenient access directly from the building to Broadway Avenue and riverfront trails. There will be additional bicycle parking on site for guests. The existing single-story restaurant and surface lot on the site will be removed as part of this development.

Exterior cladding for the 2025 West River Road development will be a combination of masonry and storefront materials at the first and second floors, specifically an architectural brick, and a chevron pattern concealed fastener metal panel in bronze and black on levels 3-7. There will be limited use of a dark fiber cement panel in the vertical reveals and other accent areas. The building will have aluminum hung balconies facing the riverfront, primarily at the two and three-bedroom locations. At the corner of West River Road and Broadway Avenue there will be a cantilevered metal-clad roof feature that will be illuminated at the underside that will help to frame this prominent corner.

The primary residential entrance will be at the corner of West River Road and Broadway Avenue where there will be a two-story storefront entryway demarcated with a metal canopy and ample building signage. New landscape and streetscape elements will be added to the right-of-way along both West River Road and Broadway Avenue. These streetscape enhancements will create a pedestrian-oriented pathway that features native, urban-tolerant trees & plantings, a widened sidewalk, bicycle amenities, and seating creating a safe and welcoming gateway from the heart of the Hawthorne neighborhood to the riverfront.

The Project will adhere to green building principles including following the Enterprise Green Communities green building standard. There will be energy and water saving measures in and around the building. The building will be, at a minimum, solar-ready and the Project Team is exploring the viability of installing a photovoltaic array at the time of construction. In addition, stormwater will be managed on site with a combination of rain gardens, bioswales, and an underground catchment system.

It is anticipated that construction will start in spring 2021 with an approximate 16-month construction period.

The property is in the C3A Community Activity Center District, PO Overlay District, SH Shoreland, and MR Mississippi River Critical Area. Schafer Richardson is seeking the following Land Use Applications:

- Conditional Use Permit to increase the maximum height above 2.5 stories or 35 feet, whichever is less, in the SH Shoreland Overlay District and MR Mississippi River Critical Area Overlay District, to 7 stories/approximately 76.5 feet
- Variance to allow development within 40' of a steep slope in the SH Shoreland Overlay District and MR Mississippi River Critical Area Overlay District
- Variances of the PO Pedestrian Oriented Overlay District to allow the building to be located more than eight (8) feet from the front lot line along West River Rd and the corner side lot line along West Broadway Ave
- Variances of the PO Pedestrian Oriented Overlay District to allow a parking lot to exceed forty (40) feet of the W River Rd street frontage
- Variance to allow the exercise run to exceed 200 square feet in area
- Site plan review, including, but not limited to, possible alternative compliance for:
 - The location of the first floor of the building more than 8 feet from the front/corner lot lines

CONDITIONAL USE PERMIT

Conditional Use Permit to increase the maximum height above 2.5 stories or 35 feet, whichever is less, in the SH Shoreland Overlay District and MR Mississippi River Critical Area Overlay District, to 7 stories/approximately 76.5 feet

1. The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort, or general welfare.
 - o The conditional use permit applied for 2025 West River Road, to increase the maximum height from 2.5 stories, or 35 feet, whichever is less, to 7 stories would not be detrimental to, nor endanger the public health, safety, comfort or general welfare of the community. While the variance would be for 7 stories to accommodate the tallest portion of the building along West Broadway, the building will appear as 6 stories along West River Road due to grade changes upwards of 15 feet from the southernmost bound of the property to the northernmost. Additionally, the building's façade will be broken up vertically with a first floor inset to create an illusion of a 5-story height.
2. The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.
 - o The conditional use will not be injurious to the use and enjoyment of other property in the vicinity. The improvements will create a more active use at the site that will be beneficial to the public realm and surrounding properties. The proposed height would not adversely impact surrounding properties. The parcels immediately adjacent to the site contain industrial uses and offices. The Minneapolis Parks and Recreation Board office building is directly to the north of the subject property, and there are railroad tracks bordering the west side of the property.
3. Adequate utilities, access roads, drainage, necessary facilities, or other measures, have been or will be provided.
 - o The redevelopment will improve upon the existing conditions on the site including utilizing existing utility infrastructure, existing roadways, and the existing curb cut location on West River Road. No additional vehicular access points are proposed resulting in no change to the number of curb cuts. The development will also add a new sidewalk along West River Road from the site access to the intersection with Broadway Avenue. The addition of the new sidewalk will narrow the width of West River Road to accommodate the same number of cars that it does currently, but the narrowing of the roadway is anticipated to reduce speeds and improve safety. The project team will work with the City to add bump-outs at the corner of Broadway Avenue and West River Road to improve safety for pedestrians crossing the intersection.

4. Adequate measures have been or will be taken to minimize traffic congestion in public streets.
 - o Vehicular access to the building will be from the existing curb cut on West River Road. The project will have below ground and surface parking and will also include ample bicycle parking for commuters and residents that will serve to reduce vehicular traffic to and from the site. A Travel Demand Management Plan has been performed for the project, which indicates that traffic from the new development is not anticipated to have significant impact on the area traffic flow. The Owner will provide information to building residents on non-motorized transportation alternatives including transit, safe bicycling routes and NiceRide facilities, and carsharing options such as HourCar.
5. The conditional use is consistent with the applicable policies of the comprehensive plan.
 - o Minneapolis 2040 identifies 100 policies which the development has incorporated in the most beneficial ways possible. The policies for which the development most strongly aligns are policies:
 - 33) Affordable Housing Production and Preservation by creating new affordable housing options, especially accessible to families;
 - 45) Leverage Housing Programs to Benefit Community by prioritizing local hiring, contracting and development as defined by CPED bonding policies;
 - 62) Contaminated Sites by facilitating the removal of site contamination as a barrier to investment and redevelopment and assisting local brownfield grantors in promoting the availability of their respective brownfield grant funds and programs;
 - 68) Energy Efficient and Sustainably Buildings through design, construction, operations, and ongoing benchmarking, particularly given the site's location in the Northside Green Zone;
 - 87) Northside by increasing access to affordable housing options in neighborhoods, particularly multifamily housing near transit routes such as the 14 and 30.
6. The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.
 - o Except for the variance applications addressed herein, the development will conform to all other applicable regulations of the districts in which it is located.

Increasing Maximum Height

1. Access to light and air of surrounding properties.
 - o The proposed increase in height will not change the access to light or air of surrounding properties, predominantly due to the distance from existing structures and the lack of adjacent parcels. The Subject Site is bound by public streets to the south and east, by a railroad to the west, and by the Minneapolis Parks and Recreation Board office to the north, which sits roughly 70 feet away from the site and will be separated by existing and new surface parking.

2. Shadowing of residential properties, significant public spaces, or existing solar energy systems.
 - o The proposed project will not substantially shadow any residential properties, significant public spaces, or existing solar energy systems. There are no residential properties or solar energy systems adjacent to the site.
3. The scale and character of surrounding uses
 - o This is a neighborhood with a varied tapestry of building types and uses including commercial, residential, and industrial. The proposed development attempts to appropriately demarcate an important neighborhood corner, while fitting in with the scale and character of the area, which generally speaking is broad and inconsistent. The height of the building is taller than others in the area, but should not stand out due to the varied height, uses, and density throughout the immediate neighborhood. The exterior will consist of brick at the first two levels and a bronze chevron concealed fastener metal panel in two colors on the upper levels, which reflect both the traditional nature of the older buildings in the area while at the same time bringing a modern touch. There will be large glass storefront at the main building entrance and ample windows and decks on the upper levels.
4. Preservation of views of landmark buildings, significant open spaces, or water bodies.
 - o The proposed project will preserve most views of landmark buildings, significant open spaces, or water bodies, primarily views of the downtown Minneapolis skyline and the Mississippi River and adjacent park land. Buildings to the west of the site by and large do not have views of river due to the industrial nature of facilities and the elevated rail line. Neighbors to the south of the site will not have their views of downtown and the river impeded by the redevelopment. The Minneapolis Parks and Recreation Board building to the north may have its view of the downtown skyline impeded by the new building; however, building occupants will continue to have views of the Mississippi River and the parkland adjacent to the River.

VARIANCES

Variance to allow development within 40' of a steep slope in the SH Shoreland Overlay District and MR Mississippi River Critical Area Overlay District

1. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.
 - o The site is located within the Mississippi River Critical Area Overlay District and partially in the Shoreland Overlay District and therefore, the development is seeking a variance for redevelopment of the site. The existing site includes a grade change of about 15 feet from the lowest point on Broadway Avenue to the highest point on the site. This is driven by the naturally occurring topography of the area and were not created by the Applicant.

2. The property owner or authorized applicant proposes to use the property in a reasonable manner that will be keeping with the spirit and intent of the ordinance and the comprehensive plan.
 - o The proposed development will keep with the spirit of the Mississippi River Critical Area Overlay District and Shoreland Overlay District by utilizing the site topography in the building design. Furthermore, the development will follow best management practices to control erosion and sediment during construction, as well as manage stormwater throughout operations in a way that minimizes degradation of surface water. The site is currently developed with a surface parking lot and a commercial building. Stormwater management will be improved with the new development by managing rainwater on-site through a combination of rain gardens, bioswales, and an underground catchment system.
3. The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.
 - o The proposed variance will not negatively alter the essential character of the community, will not be injurious to the use or enjoyment of other property in the area, nor will it be detrimental to the health, safety, or welfare of the general public. Views of the riverfront will be maintained for surrounding properties and stormwater management will be improved with the new development. The foundation and material underlying the apartment building will be engineered to be adequate to ensure soil stability and the proposed development will present no danger of falling rock, mud, uprooted trees or other materials. Furthermore, the development will not diminish the natural, aesthetic, cultural, or historic value of the River.
4. The prevention of soil erosion or other possible pollution of public waters, both during and after construction.
 - o Construction and other development activities on the site will use industry best practices to prevent soil erosion and other possible pollution from entering public waters both during and after construction. During construction, the contractor will follow a Stormwater Pollution Prevention Plan to mitigate runoff of dirt and pollutants from the site. The combination of landscape infiltration elements and structural catchment systems will be used to manage stormwater quantity and quality on-site during operations.
5. Limiting the visibility of structures and other development from protected waters.
 - o The development will not limit the visibility of structures and other development from protected waters, namely the Mississippi River, due to the proximity of the building to adjacent structures, as well as the lack of views buildings to the west currently have due to their industrial nature and the elevated railway.
6. The suitability of the protected water to safely accommodate the types, uses and numbers of watercraft that development may generate.

- The development is not projected to generate any additional watercraft on the Mississippi River and, due to stormwater management requirements, the site will increase the sustainability of the waterway by reducing both runoff and erosion.
- 7. Development must currently exist on the steep slope or within forty (40) feet of the top of a steep slope within five hundred (500) feet of the proposed development
 - There is an existing commercial building and surface parking lot on the site with a similar impervious area.
- 8. The foundation and underlying material shall be adequate for the slope condition and soil type.
 - The Applicant has retained the services of civil, structural, geotechnical and architectural engineers to ensure best practices are employed in designing the foundation and other structural and geotechnical support systems for the slope and condition of the site.
- 9. The development shall present no danger of falling rock, mud, uprooted trees or other materials.
 - The development will present no danger of falling debris during construction nor ongoing usage of the Project.
- 10. The view of the developed slope from the protected water shall be consistent with the natural appearance of the slope, with any historic areas, and with the surrounding physical context.
 - The existing views of the slope will be maintained, with the exception that natural/native landscaping will replace existing turfgrass. However, during construction and development, the views of the turfgrass and slope may be disturbed in order to complete MPCA required environmental contamination remediation and otherwise complete the project.

Variances of the PO Pedestrian Oriented Overlay District to allow the building to be located more than eight (8) feet from the front lot line along West River Rd and the corner side lot line along West Broadway Ave

1. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.
 - There is a utility easement that runs parallel to Broadway Avenue and makes development of a building within eight feet of the property line impossible. The decision to set the building back along West River Road was driven by community and Planning Commission member's desire to make a stronger connection between the river and open space across West River Road, and the natural and open areas on the site. The Applicant agrees that creating resident-oriented open space and visual and physical connections on the river side of the building makes for an overall better site plan given the unique location of this property.

2. The property owner or authorized applicant proposes to use the property in a reasonable manner that will be keeping with the spirit and intent of the ordinance and the comprehensive plan.
 - o The proposed building setback along West Broadway Avenue is driven by a utility easement along Broadway, which increases the setback from eight feet to 10 feet four inches from the property line. This increased setback allows for a wider pedestrian realm including street trees, bicycle parking, and plantings, which keep with the spirit and intent of both the ordinance and comprehensive plan.
 - o The building and site design along West River Road are meant to create a safe pedestrian experience by adding a sidewalk, boulevard strip, and street trees along the road, which follows the intent of the ordinance. However, the building on this side utilizes a U-Shaped layout both to accommodate the challenging site grade and to create a better visual connection with the natural areas and riverfront across West River Road.
3. The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.
 - o The proposed variance will not alter the essential character, nor will it be injurious to the use or enjoyment of other property in the area. Furthermore, the proposed variance will allow for enhanced health, safety, and welfare of the general public.

Variances of the PO Pedestrian Oriented Overlay District to allow a parking lot to exceed forty (40) feet of the W River Rd street frontage

1. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.
 - o A minimum of 1.0 spaces per dwelling unit is required for this development per the zoning code and based on the family-size units planned for the development. In order to achieve this ratio and work within site constraints due to topography, soil contamination, and limited curb cut locations, it makes sense to utilize the existing curb cut for both in-and-out traffic and double load parking along the drive aisle. The double loaded parking and two-way drive aisle exceed the forty feet allowed in a pedestrian-oriented overlay, but the proposed strategy minimizes overall car-oriented street frontage.
2. The property owner or authorized applicant proposes to use the property in a reasonable manner that will be keeping with the spirit and intent of the ordinance and the comprehensive plan.
 - o In order to minimize the impact from the proposed drive lane and parking, the parking area will be set back from the street and there will be a landscaped buffer

between the new pedestrian sidewalk and the surface parking. The majority of the surface parking is tucked behind the building in the rear of the site and away from view of the public realm. The landscaped buffer will utilize native plantings and trees of varying heights to create a visually interesting barrier and a continuation of the landscaping to the south and to the north along the Minneapolis Park and Recreation Board property. The boulevard strip, new sidewalk, and landscaping along the length of West River Road is intended to create a safe and pleasant pedestrian-oriented amenity for the neighborhood at-large.

3. The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.
 - o The proposed variance will not alter the essential character of the Project, would not be injurious to the use or enjoyment of other property in the area, nor would it be detrimental to the health, safety, or welfare of the general public.

Variance to allow the exercise run to exceed 200 square feet in area

1. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.
 - o Per section 537.110, outdoor exercise runs shall be allowed accessory to a dwelling, subject to the following:
 - 1) The use shall not exceed two hundred (200) square feet in area.
 - 2) The use shall be located not less than twenty (20) feet from any habitable building on adjacent property.
 - 3) The use shall be visually screened from adjacent residential property.
 - 4) The number of animals allowed on-site shall comply with the requirements of Chapter 64, Dogs and Cats, of the Minneapolis Code of Ordinances.
 - o In the Applicant's experience, there is strong demand for pet-friendly apartment buildings in the City of Minneapolis. Creating a dedicated exercise area for pets is both desirable and beneficial to residents and the neighborhood at-large. With 163 apartments and the configuration of the site, the Applicant is requesting a variance to exceed the 200 square feet maximum in order to provide ample area for multiple residents to utilize the exercise run with their pets. The proposed exercise run is approximately 735 square feet and resides between the drive lane to the underground parking garage and the adjacent railroad tracks. The Applicant will meet the remaining provisions of the ordinance.

2. The property owner or authorized applicant proposes to use the property in a reasonable manner that will be keeping with the spirit and intent of the ordinance and the comprehensive plan.
 - o The exercise run will be located on the western edge of the property adjacent to the railroad right-of-way. The nearest building is an industrial use and is more than twenty (20) feet from the proposed exercise run. The exercise run will be tucked back on the development site, and to minimize the effects of a larger exercise run, the developer will utilize different strategies such as landscaping, fencing, and the building itself to ensure that there is no visual impact from West River Road or Broadway Avenue. The applicant has experience designing and managing larger scale exercise runs and will utilize best practices to ensure compliance with the intent of the ordinance.
3. The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.
 - o The demand for pet-friendly buildings and amenities in rental properties is high and the Applicant is confident that the larger exercise run will be beneficial to the use and enjoyment of the property without altering the essential character of the neighborhood. The exercise run will provide a dedicated space for residents to exercise their pets and will be an alternative to using Broadway and West River Road. Furthermore, tucking the exercise run between the ramp to the parking garage and the adjacent railway will further mitigate the impact of a pet run on the neighborhood at-large. The Applicant has not had issues with pets, specifically dogs, being a safety concern to the public at any of their properties.

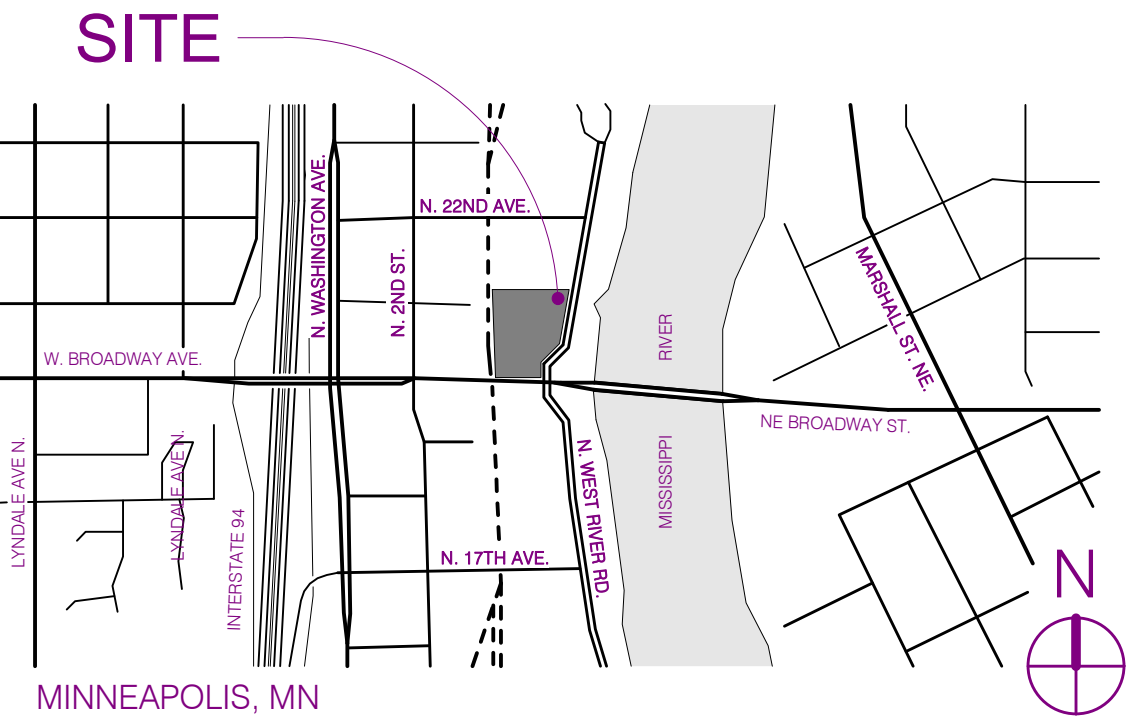
2025 WEST RIVER ROAD
LAND USE APPLICATION

JULY 1, 2020



VICINITY MAP

NOT TO SCALE



GENERAL NOTES

- DO NOT SCALE DRAWINGS.
- SUBCONTRACTORS SHALL VISIT THE JOB SITE AND SHALL REVIEW THE CONTRACT DOCUMENTS TO FAMILIARIZE THEMSELVES WITH THE REQUIREMENTS AND INTENT OF THE SCOPE OF THE WORK PRIOR COMMENCEMENT OF WORK. DISCREPANCIES SHALL BE REPORTED TO GENERAL CONTRACTOR FOR CLARIFICATION.

UNIT COUNT

UNIT TYPE	LEVEL 1	LEVEL 2	LEVEL 3	LEVEL 4	LEVEL 5	LEVEL 6	TOTALS
1BR	9	10	10	10	10	10	59
2BR	8	11	11	11	11	11	63
3BR	6	7	7	7	7	7	41
GRAND TOTAL	23	28	28	28	28	28	163

GROSS AREA SUMMARY

Area Schedule (Gross Building)	
SUBLEVEL	33,214 SF
LEVEL 1	32,268 SF
LEVEL 2	33,815 SF
LEVEL 3	33,815 SF
LEVEL 4	33,815 SF
LEVEL 5	33,815 SF
LEVEL 6	33,815 SF
GRAND TOTAL	234,555 SF

NET RESIDENTIAL AREA SUMMARY

Unit Schedule - Net Leasable Residential Square Footage by Level							
LEVEL	AREA	LEVEL 1	LEVEL 2	LEVEL 3	LEVEL 4	LEVEL 5	LEVEL 6
LEVEL 1	20,896 SF	23	0	0	0	0	0
LEVEL 2	26,849 SF	0	28	0	0	0	0
LEVEL 3	26,849 SF	0	0	28	0	0	0
LEVEL 4	26,852 SF	0	0	0	28	0	0
LEVEL 5	26,852 SF	0	0	0	0	28	0
LEVEL 6	26,852 SF	0	0	0	0	0	28
Grand total: 163	155,151 SF	23	28	28	28	28	28

PARKING STALLS

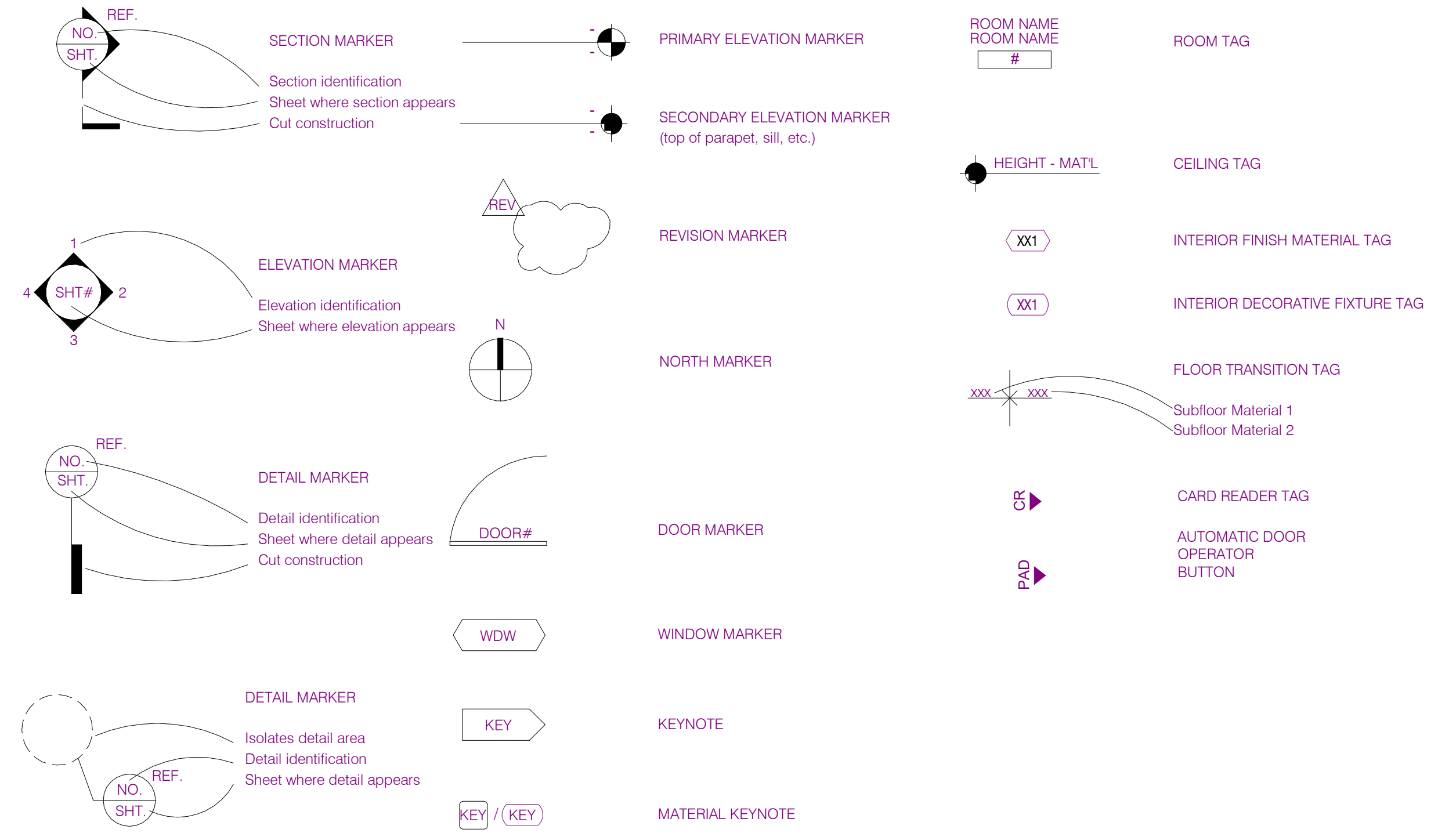
DESCRIPTION	SUBLEVEL ENCLOSED	SURFACE	TOTAL PARKING
ACCESSIBLE PARKING STALL	2	2	4
COMPACT PARKING STALL	8	12	20
STANDARD PARKING STALL	64	75	139
GRAND TOTAL	74	89	163

(IN ADDITION TO THE STALLS SHOWN ABOVE, (1) 10' x 25' LOADING STALL IS PROVIDED)

LAND USE APPLICATION - SHEET LIST

- G001 - TITLE SHEET
- SURVEY - ALTA/NSPS LAND TITLE SURVEY (WENCK ASSOCIATES)
- C100 - SELECTIVE SITE DEMOLITION AND EROSION CONTROL PLAN
C200 - GRADING, DRAINAGE, AND EROSION CONTROL PLAN
C300 - UTILITY PLAN
C400 - PAVING AND GEOMETRIC PLAN
C500 - CIVIL DETAILS
C501 - CIVIL DETAILS
C502 - CIVIL DETAILS
C503 - CIVIL DETAILS
C600 - STORM WATER POLLUTION PREVENTION PLAN
- L010 - GENERAL NOTES
L011 - REFERENCE NOTES & PLANTING SCHEDULES
L110 - OVERALL SITE PLAN
L140 - OVERALL LANDSCAPE PLAN
L500 - LANDSCAPE DETAILS
L501 - LANDSCAPE DETAILS
- AL101 - ARCHITECTURAL SITE PLAN
AL102 - SHADOW STUDY & CONTEXT MAP
AL103 - EXISTING CONTEXT PHOTOGRAPHS
- A001.0 - SUBLEVEL PLAN - OVERALL
A101.0 - LEVEL 1 PLAN - OVERALL
A102.0 - LEVEL 2 PLAN - OVERALL
A103.0 - LEVEL 3 PLAN - OVERALL
A104.0 - LEVEL 4 PLAN - OVERALL
A105.0 - LEVEL 5 PLAN - OVERALL
A106.0 - LEVEL 6 PLAN - OVERALL
- A201.0 - ROOF PLAN - OVERALL
- A301 - BUILDING ELEVATIONS
A302 - BUILDING ELEVATIONS
A303 - BUILDING ELEVATIONS
A351 - BUILDING AXONOMETRIC VIEWS
A356 - BUILDING PERSPECTIVE VIEWS
- E-L-S - SITE PHOTOMETRICS

SYMBOLS



CONTACTS

DEVELOPER/CLIENT

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CONTRACTOR

NAME: TBD
CONTACT: TBD

ADDRESS: TBD
PHONE No.: TBD
EMAIL: TBD

ARCHITECT

NAME: URBANWORKS ARCHITECTURE
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DAVID MILLER

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BRIAN DOUCETTE

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IAN FRAND

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EMAIL: RMS@DTE-LS.COM
I2F@DTE-LS.COM

PLUMBING ENGINEER

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CONTACT: ROBERT SCHUSTER

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ROGERS, MN 55374
PHONE No.: 763.270.6316
EMAIL: RMS@DTE-LS.COM

ELECTRAICAL ENGINEER

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CONTACT: ROBERT SCHUSTER

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ROGERS, MN 55374
PHONE No.: 763.270.6316
EMAIL: RMS@DTE-LS.COM

INTERIOR DESIGNER

NAME: URBANWORKS ARCHITECTURE
CONTACT: SARAH DAVIS

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MINNEAPOLIS, MN, 55401
PHONE No.: 612.455.3100
EMAIL: SDAVIS@URBAN-WORKS.COM

TITLE SHEET

G001

URBANWORKS

CONSULTANT

PRELIMINARY

NOT FOR
CONSTRUCTION

LAND USE APPLICATION

07.01.2020

REVISIONS

KEY PLAN

DATE: 7/1/2020
PROJECT #: 19-0035
PHASE: LAND USE & PDR
DRAWN BY: DP/SL
CHECKED BY: DP/SL



EXISTING BROADWAY PIZZA BUILDING FROM NORTH



EXISTING BROADWAY PIZZA BUILDING FROM NORTHWEST CORNER



EXISTING VIEW OF ADJACENT PARK BOARD H.Q. TO THE NORTH



EXISTING VIEW OF SITE FROM NORTHEAST @ WEST RIVER ROAD



EXISTING VIEW OF ADJACENT BUILDINGS TO THE NORTHWEST



EXISTING VIEW OF SITE FROM SOUTHEAST @ WEST BROADWAY & WEST RIVER ROAD

FOR CITY PERMIT USE



2025 West River Rd

2025 W River Road, Minneapolis, MN 55411

URBANWORKS

© URBANWORKS ARCHITECTURE LLC 2018
901 NORTH THIRD STREET, SUITE 145, MINNEAPOLIS, MN 55401

CONSULTANT

PRELIMINARY
**NOT FOR
CONSTRUCTION**

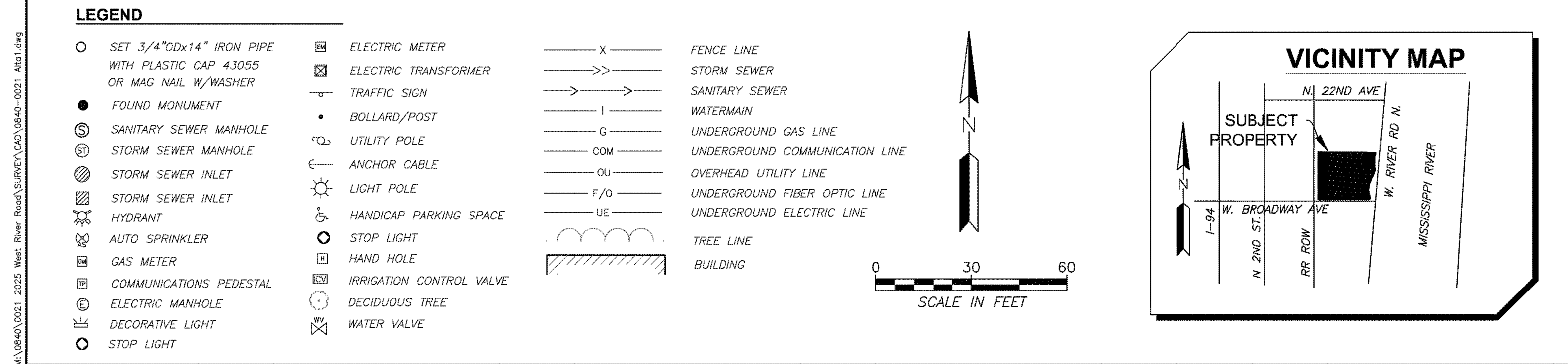
LAND USE APPLICATION
07.01.2020

REVISIONS

DATE 7/1/2020
PROJECT # 19-0035
PHASE LAND USE & POR
DRAWN BY DP/SL
CHECKED BY DP/SL

EXISTING
CONTEXT
PHOTOGRAPHS

AL103



The following description was provided in First American Title Insurance Company No. NCS-965354-MPLS REV#1: Delete Exception 9, BII, which has an effective date of May 31, 2019 at 8:00 A.M.

Lots 3 and 4, Block 2, North Washington Industrial Center, Hennepin County, Minnesota.

Property is Abstract and Torrens;
Certificate of Title No. 1064169.

1. Property corner markers were placed or found as shown
2. Property addresses: 2025 West River Road N., Minneapolis, MN.
3. Flood zone classification: this property is located in Zone X according to FEMA Flood Insurance Rate Map Number 27053C0378F which has an effective date of 11/4/2016.
4. Gross land area: 105,846 sq. ft. (2.43 acres)
5. Elevations and surface contours are shown hereon and are relative to the NAVD88 vertical datum
- 6a. Zoning Information: There were no zoning reports or letters provided to the surveyor.
- 7a. Exterior building dimensions shown are the exterior facade at chest height. underground footings and foundations and roof lines/eaves may extend outside the building lines shown.
8. The surveyor has made their best effort to depict all above ground visible features hereon.
9. There are 144 standard and 6 handicap parking spaces on the subject property.
10. Utility lines shown hereon are based on field markings and maps provided to us as a result of a Gopher State One Call private utility locate (ticket number 192121135). the surveyor cannot guarantee that all utilities were marked or that the markings/maps are accurate.
13. Names of owners of adjoining lands were obtained from the Hennepin County GIS.
16. There were no observable signs of recent construction or earthmoving on the subject property at the time of survey.

Items corresponding to Schedule B Section 2 as provided in First American Title Insurance Company No. NCS-965354-MPLS which has an effective date of May 31, 2019 at 8:00 A.M.

Items 1, 5-8, 9, and 12-17 are not survey related and are not addressed herein.

Item 2. Discrepancies, conflicts in boundary lines, shortage in area, encroachments, or any other facts which a correct survey would disclose, and which are not shown by the Public Records. - ANY SUCH ISSUES OBSERVED BY THE SURVEYOR BASED ON THE TITLE COMMITMENT ARE SHOWN HEREON.

Item 3. Any facts, rights, interests, or claims which are not shown by the Public Records but which could be ascertained by an inspection of the Land or by making inquiry of persons in possession thereof. - ANY SUCH ITEMS OBSERVED BY THE SURVEYOR ARE SHOWN HEREON. NO INQUIRIES OF OWNERS OR OCCUPANTS WERE MADE.

Item 4. Easements, or claims of easement, not shown by the Public Records. - ONLY THOSE EASEMENTS PROVIDED IN THE TITLE COMMITMENT ARE SHOWN HEREON.

Item 10. Easement in favor of Northern States Power Company, a Minnesota corporation, together with the rights incidental thereto, as described and contained in the Underground Easement dated July 2, 1971, recorded July 7, 1971, as Abstract Document No. 3893462, and as reserved in the Quit Claim Deed dated September 3, 1971, recorded December 22, 1971, as Abstract Document No. 3923359. - SAID EASEMENT IS SHOWN HEREON.

Item 11. Easement and right-of-way, together with the rights incidental thereto, in favor of KBL Cablestystems of Minneapolis, Inc., and KBL Cablestystems of the Southwest, Inc.; Nortel Cable Associates, I.P., through its general partners Nortel Cable Corporation, Minnesota Cable Properties, Inc., and Amzak Cable Midwest, Inc., all doing business as Paragon Cable, as described and contained in the Easement for Cable/Telecommunications Services dated May 3, 1995, recorded June 26, 1995, as Abstract Document No. 6442094. THIS EASEMENT IS BLANKET IN NATURE AND COVERS ALL OF THE SUBJECT PROPERTY SHOWN HEREON.

The following possible encroachment notes are intended to draw the users attention only, the surveyor does not guarantee that the items noted below are in fact encroachments or that all possible encroachments are shown herein.

① STORM SEWER LINE INDICATED BY CITY MAPPING MAY RUN ALONG NORTHERLY PROPERTY BOUNDARY; NO DOCUMENTATION OF AN EASEMENT FOR THIS LINE WAS PROVIDED

② SANITARY SEWER SERVICE RUNS THROUGH ADJACENT PROPERTY; NO DOCUMENTATION OF AN EASEMENT FOR THIS LINE WAS PROVIDED

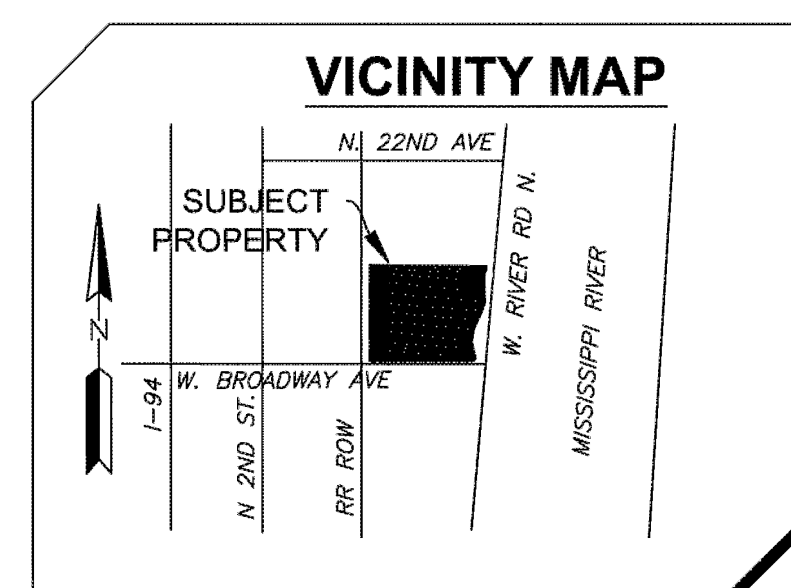
To: Schafer Richardson, SRPB Strategic Housing LLC, and First American Title Insurance Company: This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2016 Minimum Standard Detail Requirements for ALTA/NSPS Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1, 2, 3, 5, 6a, 7a, 8, 9, 11, 13 and 16 of Table A thereof.

Date of fieldwork: 8/12/2019

Chris Ambourn
Chris Ambourn, MN. License 43055

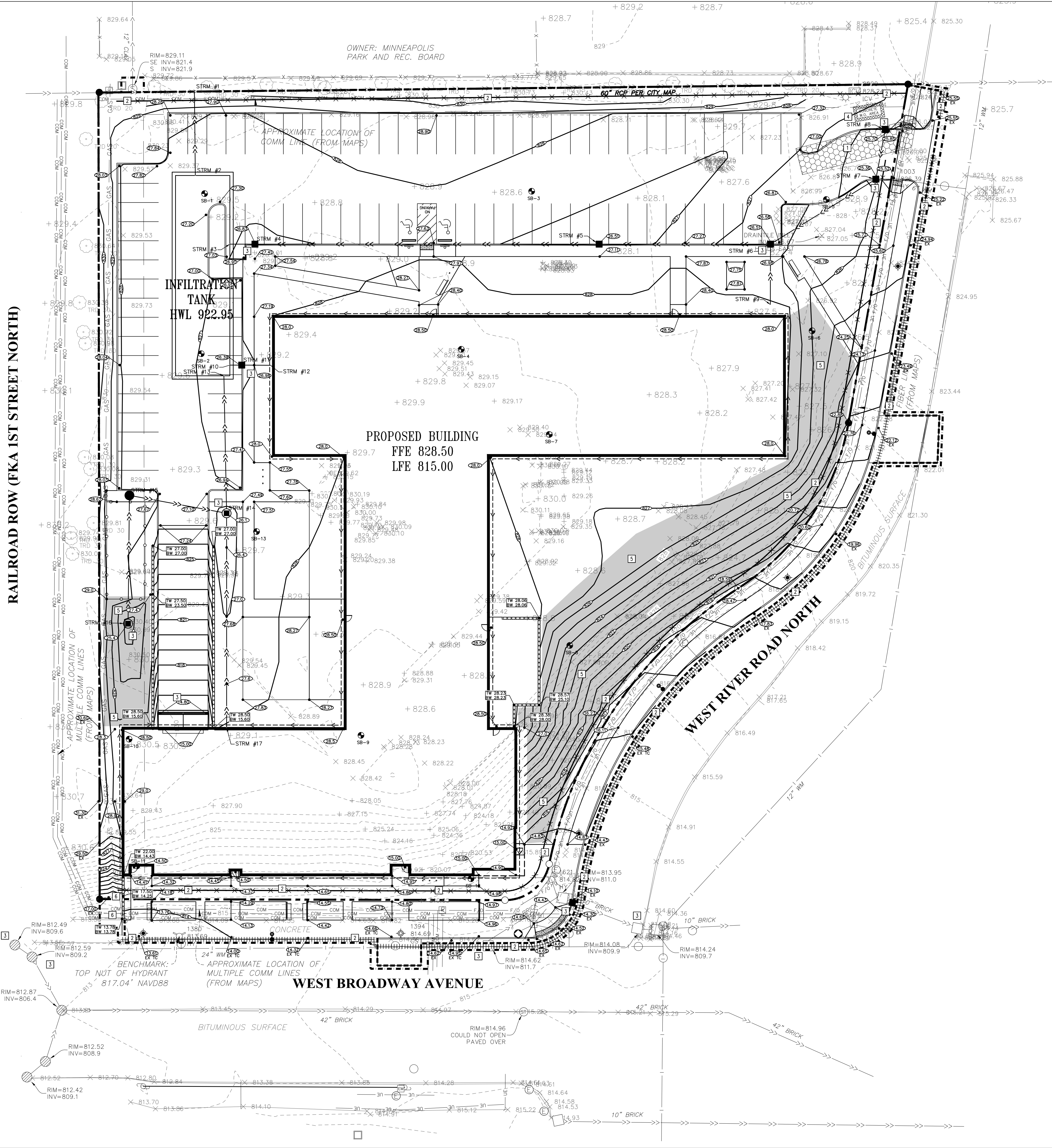
REVISED 11/21/19... ADDED PRIVATE UTILITIES

1. Bearings shown hereon are based on the Hennepin County Coordinate System relative to the NAD83(11) control adjustment.
2. Elevations and contours shown hereon were established with GPS and are relative to the NAVD88 vertical datum.



 Responsive partner. Exceptional outcomes. 1802 WOODLAKE DRIVE WOODBURY, MN 55125 Ph: 651-395-5212	CLIENT NAME SCHAFFER RICHARDSON	PROJECT TITLE ALTA/NPS LAND TITLE SURVEY											
	PROJECT NO. 0840-0021	<table style="width: 100%; border-collapse: collapse;"> <tr> <td style="width: 10%;">DWN</td> <td style="width: 10%;">BY</td> <td style="width: 10%;">CHK'D</td> <td style="width: 10%;">APP'D</td> <td style="width: 10%;">DWG. DATE</td> <td style="width: 10%;">SEE CERT</td> </tr> <tr> <td>CNA</td> <td>XXX</td> <td>XXX</td> <td>XXX</td> <td>SCALE</td> <td>1" = 30'</td> </tr> </table> SHEET NO. 1 OF 10	DWN	BY	CHK'D	APP'D	DWG. DATE	SEE CERT	CNA	XXX	XXX	XXX	SCALE
DWN	BY	CHK'D	APP'D	DWG. DATE	SEE CERT								
CNA	XXX	XXX	XXX	SCALE	1" = 30'								

RAILROAD ROW (FKA 1ST STREET NORTH)



1 GRADING, DRAINAGE, AND EROSION CONTROL PLAN
C200 1" = 20'

ABBREVIATIONS	
BLDG	Building
BM	Benchmark
BW	Bottom of Wall
CB	Catch Basin
CONC	Concrete
ELEV	Elevation
EX	Existing
FFE	Finished Floor Elevation
INV	Invert
LFE	Lower Floor Elevation
MAX	Maximum
MH	Manhole
MFC	Minimum
PVC	Polyvinyl Chloride
RCP	Reinforced Concrete Pipe
TW	Top of Wall
W.O.	Washout

NOTE: REFER TO SHEET C100 FOR MINNEAPOLIS STANDARD EROSION CONTROL NOTES - (JANUARY 7, 2011)

NOTE: STORM SEWER INLETS NOT SHOWN ON PLAN MAY RECEIVE RUNOFF FROM CONSTRUCTION ACTIVITIES. INSTALL INLET SEDIMENT PROTECTION PER DETAIL 4/C500 ON ALL STORM INLETS THAT MAY RECEIVE RUNOFF.

WALKWAY NOTES:

1. GRADING CONTRACTOR IS TO COORDINATE WITH PAVING CONTRACTOR SO THAT ALL STEPS AND LANDINGS ARE SLOPED PER CODE.
2. ALL SIDEWALK LONGITUDINAL AND TRANSVERSE SLOPES ARE TO BE PER CODE.

APPROXIMATE DISTURBED AREA IS 2.70 ACRES

RETAINING WALL NOTES:

1. ALL TOP AND BOTTOM ELEVATIONS CORRESPOND TO THE RESPECTIVE GRADE ELEVATIONS ON EACH SIDE OF THE WALL.
2. THE BOTTOM ELEVATION IS THE ELEVATION OF THE LOW-GRADE SIDE OF THE WALL, NOT THE TOP ELEVATION OF THE BURIED BLOCK COURSE.
3. ALL RETAINING WALLS SHALL HAVE PROTECTIVE FENCING AT THE TOP WHERE THE VERTICAL HEIGHT EXCEEDS 30 INCHES. REFER TO ARCHITECTURAL AND LANDSCAPE PLANS AND SPECIFICATIONS.
4. MODULAR BLOCK RETAINING WALL SHALL BE DESIGNED AND CERTIFIED BY A LICENSED PROFESSIONAL ENGINEER. DESIGN CALCULATIONS AND SHOP DRAWINGS SHALL BE SUBMITTED TO THE OWNER AND ARCHITECT AT LEAST THREE WEEKS PRIOR TO CONSTRUCTION OF WALL.

PROPOSED PLAN SYMBOLS	
CONSTRUCTION LIMITS	---
SILTATION FENCE	XXXXXX
SEDIMENT CONTROL LOG	
PROPERTY LINE	---
SAWCUT LINE (APPROX.)	---
PROPOSED CONTOUR	---
DRAIN TILE	---
STORM SEWER	---
RETAINING WALL	---
CATCH BASIN	---
MANHOLE	---
EROSION CONTROL BLANKET (TEMPORARY)	---
ROCK CONSTRUCTION ENTRANCE	---
DRAINAGE FLOW ARROW	---
RETAINING WALL SPOT ELEVATION	---
SPOT ELEVATION	---
SOIL BORING	---
CONCRETE WASHOUT AREA	---

*NOTE: CONSTRUCTION LIMITS ARE ANTICIPATED TO BE PROPERTY LINE UNLESS OTHERWISE SHOWN.

KEYED NOTES

KEYED NOTES ARE DENOTED BY [N] ON PLAN.

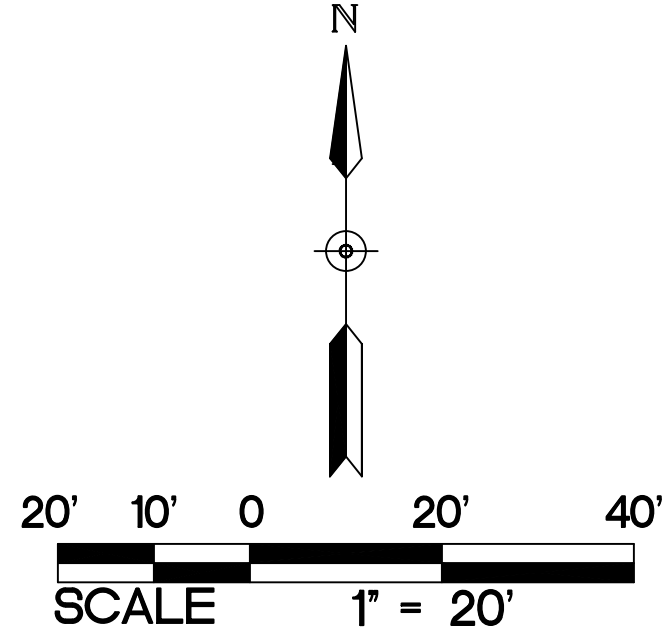
1. INSTALL CONSTRUCTION ENTRANCE. REFER TO DETAIL 1/C500.
2. INSTALL PERIMETER EROSION CONTROL. REFER TO DETAILS 2/C500 AND 3/C500.
3. INSTALL INLET SEDIMENT PROTECTION. REFER TO DETAIL 4/C500.
4. APPROXIMATE LOCATION OF TEMPORARY CONTAINED CONCRETE WASH OUT BIN. REFER TO THE MINNESOTA'S NPDES/SOS GENERAL STORMWATER PERMIT FOR CONSTRUCTION ACTIVITY FOR MORE DETAILS. SELF-CONTAINED CONCRETE WASHOUTS ON CONCRETE DELIVERY TRUCKS IS AN ACCEPTABLE ALTERNATIVE TO ON-SITE CONTAINMENT.
5. INSTALL MN/DOT 3885 CATEGORY 3 TEMPORARY STRAW FIBER EROSION CONTROL BLANKET.
6. CONTRACTOR SHALL COORDINATE RETAINING WALL CONSTRUCTION WITH GAS LINE AND COMMUNICATION LINE OWNERS PRIOR TO START OF CONSTRUCTION.

EROSION CONTROL NOTES:

1. ALL EROSION CONTROL FACILITIES SHALL BE INSTALLED PRIOR TO ANY SITE GRADING OPERATIONS. THE CITY ENGINEERING DEPARTMENT MUST BE NOTIFIED UPON COMPLETION OF THE INSTALLATION OF THE REQUIRED EROSION CONTROL FACILITIES AND PRIOR TO ANY GRADING OPERATION BEING COMMENCED. THE CONTRACTOR IS RESPONSIBLE TO SCHEDULE A PRE-CONSTRUCTION GRADING MEETING ON-SITE WITH THE CITY. IF DAMAGED OR REMOVED DURING CONSTRUCTION, ALL EROSION CONTROL FACILITIES SHALL BE RESTORED AND IN PLACE AT THE END OF EACH DAY.
2. ANY EROSION CONTROL FACILITIES DEEMED NECESSARY BY THE CITY, BEFORE, DURING OR AFTER THE GRADING ACTIVITIES, SHALL BE INSTALLED AT THEIR REQUEST.
3. NO DEVIATIONS SHALL BE MADE FROM THE ELEVATIONS SHOWN ON THE APPROVED GRADING PLAN, WITHOUT PRIOR APPROVAL FROM THE CIVIL ENGINEER.
4. FOR SITES GREATER THAN 1.0 ACRE, AS REQUIRED BY THE MPCA PERMIT REQUIREMENTS, THE PERMIT APPLICANT MUST KEEP AN EROSION CONTROL INSPECTION LOG. INSPECTION MUST BE MADE ONCE EVERY SEVEN DAYS AND WITHIN 24 HOURS AFTER EVERY RAIN EVENT. THE INSPECTION RECORD MUST BE MADE AVAILABLE TO THE CITY WITHIN 24 HOURS OF REQUEST.
5. FLOWS FROM DRAINAGE CHANNELS OR PIPES (TEMPORARY OR PERMANENT) SHALL BE ROUTED TO SEDIMENTATION BASINS OR APPROPRIATE ENERGY DISSIPATORS TO PREVENT TRANSPORT OF SEDIMENT TO OUTFLOW TO LATERAL CONVEYORS AND TO PREVENT EROSION AND SEDIMENTATION WHEN RUNOFF FLOWS INTO THESE CONVEYORS.
6. SITE ACCESS ROADS SHALL BE GRADED OR OTHERWISE PROTECTED WITH SILT FENCES, DIVERSION CHANNELS, OR DIKES AND PIPES TO PREVENT SEDIMENT FROM EXITING THE SITE VIA THE ACCESS ROADS. SITE-ACCESS ROADS/DRIVEWAYS SHALL BE SURFACED WITH CRUSHED ROCK WHERE THEY ADJOIN EXISTING PAVED ROADWAYS.
7. SOILS TRACKED FROM THE SITE BY MOTOR VEHICLES OR EQUIPMENT SHALL BE CLEANED DAILY FROM PAVED ROADWAY SURFACES, OR MORE FREQUENTLY IF REQUESTED BY CITY, THROUGHOUT THE DURATION OF CONSTRUCTION.
8. DUST CONTROL MEASURES SHALL BE PERFORMED PERIODICALLY WHEN CONDITIONS REQUIRE AND/OR AS DIRECTED BY THE CITY.
9. ALL EROSION CONTROL MEASURES SHALL BE USED AND MAINTAINED FOR THE DURATION OF SITE CONSTRUCTION. IF CONSTRUCTION OPERATIONS OR NATURAL EVENTS DAMAGE OR INTERFERE WITH THESE EROSION CONTROL MEASURES, THEY SHALL BE RESTORED TO SERVE THEIR INTENDED FUNCTION AT THE END OF EACH DAY OR AS SOON AS FIELD CONDITIONS ALLOW ACCESS.
10. ALL AREAS DISTURBED DURING CONSTRUCTION SHALL BE RESTORED AS SOON AS POSSIBLE. ANY AREAS WHICH HAVE BEEN FINISHED GRADED OR AREAS THAT HAVE BEEN DISTURBED AND FOR WHICH GRADING OR SITE BUILDING CONSTRUCTION OPERATIONS ARE NOT ACTIVELY UNDERWAY SHALL BE SEEDED AND MULCHED AS SET FORTH IN THE FOLLOWING PARAGRAPHS WITHIN 7 DAYS:
 - A. ALL SEEDED AREAS SHALL BE EITHER MULCHED AND DISC-ANCHORED OR COVERED BY FIBROUS BLANKETS TO PROTECT SEEDS AND LIMIT EROSION. TEMPORARY STRAW MULCH SHALL BE DISC-ANCHORED AND APPLIED AT A UNIFORM RATE OF NOT LESS THAN TWO TONS PER ACRE AND NOT LESS THAN 80% COVERAGE.
 - B. IF THE GRADED AREA IS ANTICIPATED TO BE RE-DISTURBED/DEVELOPED WITHIN SIX MONTHS, PROVIDE A TEMPORARY VEGETATIVE COVER CONSISTING OF MINNESOTA DEPARTMENT OF TRANSPORTATION (MNDOT) SEED MIXTURE 21-111 (OATS), OR 21-112 (WINTER WHEAT), AT A RATE OF 100 POUNDS PER ACRE.
 - C. IF GRADED AREA WILL NOT BE DEVELOPED FOR A PERIOD GREATER THAN SIX MONTHS, PROVIDE A SEMI-PERMANENT VEGETATIVE COVER OF SEED MIXTURE MNDOT 22-112 AT A RATE OF 40 POUNDS PER ACRE.
 - D. GRADING BONDS OR THE EQUIVALENT SECURITIES SHALL BE RETAINED UNTIL TURF HAS GERMINATED AND SURVIVED A 60-DAY GROWING PERIOD.
 - E. ALL AREAS THAT WILL NOT BE MOWED OR MAINTAINED AS PART OF THE ULTIMATE DESIGN WILL BE PERMANENTLY RESTORED USING SEED MIXTURE MNDOT 25-141 AT A RATE OF 59 POUNDS PER ACRE.
 - F. UNLESS SPECIFIED ELSEWHERE WITHIN THE CONSTRUCTION DOCUMENTS (I.E. ARCHITECTURAL SITE PLAN OR LANDSCAPE PLAN), PERMANENT TURF RESTORATION SHALL CONSIST OF MN/DOT SEED MIXTURE 25-131 (COMMERCIAL TURF GRASS) AT A RATE OF 220 POUNDS PER ACRE.
 - G. WHENEVER OTHER EROSION AND SEDIMENT CONTROL PRACTICES ARE INADEQUATE, TEMPORARY ON-SITE SEDIMENT BASINS THAT CONFORM TO THE CRITERIA FOR ON-SITE DETENTION BASINS SHALL BE PROVIDED.
 - H. MULCH, HYDROMULCH, AND TACKIFIERS MAY NOT BE USED FOR STABILIZATION IN SWALES OR DRAINAGE DITCHES UNLESS THE LONGITUDINAL SLOPE IS LESS THAN 2 PERCENT.
 - I. RUNOFF SHALL BE PREVENTED FROM ENTERING ALL STORM SEWER CATCH BASINS PROVIDING THEY ARE NOT NEEDED DURING CONSTRUCTION. WHERE STORM SEWER CATCH BASINS ARE NECESSARY FOR SITE DRAINAGE DURING CONSTRUCTION, A SILT FENCE OR SEDIMENT PROTECTION DEVICES AS DETAILED SHALL BE INSTALLED AND MAINTAINED AROUND ALL CATCH BASINS UNTIL THE TRIBUTARY AREA TO THE CATCH BASIN IS RESTORED.
11. GRADING ACTIVITIES PROPOSED TO BEGIN AFTER OCTOBER 15 WILL REQUIRE AN APPROVED PHASING SCHEDULE. THE AREA OF LAND THAT THE CITY WILL ALLOW TO BE DISTURBED AT THIS TIME OF YEAR WILL BE SEVERELY LIMITED. THE CITY WILL ALSO REQUIRE ADDITIONAL EROSION CONTROL DEVICES, I.E., TEMPORARY SEDIMENT BASINS, DORMANT SEEDING AND HIGH RATES OF APPLICATION OF BOTH SEED AND MULCH.
12. TO MINIMIZE EROSION, ALL 3:1 SLOPES SHALL BE COVERED WITH A MN/DOT 3885 CATEGORY 3 STRAW EROSION CONTROL BLANKETS OR STAKED SOD.
13. ACCUMULATION OF ALL SEDIMENT OCCURRING IN STORM SEWERS AND DITCHES SHALL BE REMOVED PRIOR TO, DURING AND AFTER COMPLETION OF GRADING ACTIVITIES.
14. EROSION CONTROL ITEMS AND DEVICES SHALL BE REMOVED ONLY AFTER THE AREA HAS RECEIVED FINAL STABILIZATION OR AS DIRECTED BY THE CITY.

GRADING NOTES:

1. THE CONTRACTOR SHALL VISIT THE SITE, REVIEW ALL CONSTRUCTION DOCUMENTS AND FIELD VERIFY THE EXISTING CONDITIONS PRIOR TO BIDDING. NO ADDITIONAL COMPENSATION WILL BE GIVEN FOR WORK THAT COULD HAVE BEEN IDENTIFIED BY A SITE VISIT OR CONSTRUCTION DOCUMENT REVIEW.
2. THE BACKGROUND INFORMATION WAS PREPARED BY WENCK ASSOCIATES, (651) 395-5212.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN THE LOCATION OF ALL EXISTING UTILITIES. THE CONTRACTOR SHALL VERIFY THE LOCATION, ELEVATION AND MARK ALL EXISTING UTILITIES 48 HOURS BEFORE CONSTRUCTION STARTS. THE ENGINEER, ARCHITECT OR OWNER DOES NOT GUARANTEE THAT ALL THE UTILITIES ARE MAPPED, OR IF MAPPED, ARE SHOWN CORRECTLY. CONTACT GOMER ONE AT 651-654-0002 FOR FIELD LOCATING EXISTING UTILITIES. CONTACT UTILITY OWNER IF DAMAGE OCCURS DUE TO CONSTRUCTION.
4. PROTECT ALL EXISTING STRUCTURES AND UTILITIES WHICH ARE NOT SCHEDULED FOR REMOVAL.
5. NOTIFY CITY BUILDING INSPECTOR BEFORE TRENCHING AND EXCAVATION WORK COMMENCES. THE CONTRACTOR SHALL OBTAIN ALL APPLICABLE PERMITS PRIOR TO START OF CONSTRUCTION.
6. ALL SPOT ELEVATIONS SHOWN AS 28.45, FOR EXAMPLE, ARE TO BE UNDERSTOOD TO MEAN 28.45.
7. ALL SPOT ELEVATIONS ALONG THE CURB-LINE INDICATE THE ELEVATION OF THE GUTTER, UNLESS NOTED OTHERWISE.
8. NO LANDSCAPED SLOPES ARE TO EXCEED 3:1 (3 FEET HORIZONTAL TO 1 FOOT VERTICAL) UNLESS NOTED OTHERWISE.
9. ACCESSIBLE PARKING AREAS SHALL NOT HAVE SLOPES IN ANY DIRECTION THAT EXCEED 2%.
10. PROVIDE POSITIVE DRAINAGE FROM BUILDINGS AT ALL TIMES.
11. UPON COMPLETION OF THE GRADING AND UTILITY WORK, THE CONTRACTOR SHALL CERTIFY THAT ALL GRADING AND UTILITY WORK WAS PERFORMED IN ACCORDANCE WITH THE APPROVED GRADING AND UTILITY PERMITS. AN AS-BUILT GRADING AND UTILITY PLAN SHALL BE PERFORMED BY A REGISTERED LAND SURVEYOR HIRED BY THE CONTRACTOR. SURVEY SHALL BE PROVIDED TO CIVIL ENGINEER.
12. PRIOR TO ISSUANCE OF BUILDING PERMITS, ALL NECESSARY EROSION CONTROL DEVICES MUST BE IN PLACE AND FUNCTIONING. THE CITY WILL INSPECT THE SITE TO DETERMINE ITS SUITABILITY FOR BUILDING ACTIVITIES. IF THE PUBLIC UTILITIES HAVE NOT BEEN INSTALLED AT THIS POINT, IT MAY BE NECESSARY TO WITHHOLD BUILDING PERMITS FOR VARIOUS LOTS TO ALLOW THE CONTRACTOR ADEQUATE SPACE TO PERFORM THIS WORK.
13. ALL DEBRIS CREATED IN THE PROCESS OF CLEARING AND GRADING THE SITE SHALL BE REMOVED FROM THE SITE. THIS INCLUDES TREES AND SHRUBS. UNDER NO CIRCUMSTANCES SHALL THIS TYPE OF MATERIAL BE BURIED OR BURNED ON THE SITE.
14. THE CONTRACTOR MAY STRIP AND SALVAGE TOPSOIL FOR POTENTIAL RE-SPREADING ON THE SITE, IF APPROVED BY THE LANDSCAPE ARCHITECT AND/OR SPECIFICATIONS. SIX INCHES OF TOPSOIL - AFTER COMPACTION - SHALL BE RE-SPREAD PRIOR TO SEEDING AND MULCHING. EXCESS TOPSOIL MAY BE REMOVED FROM THE SITE PROVIDING THERE IS ADEQUATE TOPSOIL REMAINING TO PROPERLY FINISH THE SITE AS NOTED ABOVE. THE TOPSOIL STRIPPING AND RE-SPREADING SHALL BE DONE IN ACCORDANCE TO, AND NOTED ON, THE APPROVED GRADING PLAN AND SPECIFICATIONS. THE CONTRACTOR SHALL REFER TO THE LANDSCAPE DRAWINGS AND SPECIFICATIONS FOR ANY SPECIAL TOPSOIL OR PLANTING REQUIREMENTS.
15. ALL GRADING OPERATIONS SHALL BE CONDUCTED IN A MANNER TO MINIMIZE THE POTENTIAL FOR SITE EROSION. EROSION CONTROL MEASURES SHALL BE INSTALLED TO PREVENT SEDIMENT FROM RUNNING OFF ONTO ADJACENT PROPERTIES. ANY DAMAGE TO ADJACENT PROPERTIES MUST BE CORRECTED AND RESTORED AS SOON AS PERMISSION IS GRANTED FROM THE ADJACENT PROPERTY OWNER(S).
16. IF CONSTRUCTION OF THE SITE WORK PROCEEDS THROUGH THE WINTER MONTHS, ANY DISTURBED AREAS OUTSIDE THE BUILDING FOOTPRINTS ARE TO BE MINIMALLY STABILIZED PRIOR TO MARCH 1, AS FOLLOWS: AREAS PLANNED TO RECEIVE PAVEMENTS ARE TO HAVE CLASS 5 BASE INSTALLED; ALL OTHER DISTURBED AREAS ARE TO BE SEEDED, STRAW MULCH PLACED, AND DISC-ANCHORED.
17. WINTER MULCHING:
 - 17.A. SNOW MULCHING SHALL BE DEFINED AS MULCH MATERIAL SPREAD OVER THE TOP OF SNOW SO THAT THE MULCH SELTS THROUGH THE SNOW AND STICKS TO THE EXPOSED SOILS.
 - 17.B. FROZEN GROUND MULCHING SHALL BE DEFINED AS MULCH MATERIAL SPREAD OVER FROZEN GROUND. MULCH MATERIALS THAT DO NOT REQUIRE DISC-ANCHORING INTO THE SOIL MAY BE PLACED WITHOUT MODIFICATION. MULCH MATERIALS THAT REQUIRE DISC-ANCHORING MAYBE ANCHORED WITH HYDRAULIC SOIL STABILIZERS OR MAY BE FROZEN TO THE SOIL BY APPLYING WATER, AT A RATE OF 2000 GALLONS PER ACRE, OVER THE MULCH AS A SUBSTITUTION FOR DISC-ANCHORING.
18. RETAINING WALLS AND APPROPRIATE SAFETY FENCING ALONG THE TOP OF WALLS ARE TO BE DESIGNED AND CERTIFIED BY A REGISTERED PROFESSIONAL ENGINEER. SUBMIT RETAINING WALL SHOP DRAWINGS TO PROJECT TEAM PRIOR TO CONSTRUCTION.
19. THE CONTRACTOR SHALL LIMIT THE DISTURBED AREA AS MUCH AS POSSIBLE.



NOTE: CONTRACTOR, PROPERTY OWNER, OR RESPONSIBLE PARTY SHALL CONTACT MINNEAPOLIS SURFACE WATERS AND SEWERS 48 HOURS PRIOR TO ANY EXCAVATION OR CONSTRUCTION RELATED TO OR IN THE LOCATION OF THE PROPOSED STORMWATER MANAGEMENT BMP. CONTACT PAUL GHELLEN, 612-673-2406 OR PAUL.GHELLEN@MINNEAPOLIS.MN.GOV.

UPON THE PROJECT'S COMPLETION THE GENERAL CONTRACTOR, PROPERTY OWNER, OR RESPONSIBLE PARTY SHALL PROVIDE TO THE DEPARTMENT OF PUBLIC WORKS A FINAL STORMWATER MANAGEMENT REPORT INCLUDING RECORD DRAWINGS. THIS REPORT WILL SERVE AS A MEANS OF VERIFICATION THAT THE INTENT OF THE APPROVED STORMWATER MANAGEMENT DESIGN HAS BEEN MET. THIS FINAL REPORT SHALL SUBSTANTIATE THAT ALL ASPECTS OF THE ORIGINAL DESIGN HAVE BEEN ADEQUATELY PROVIDED FOR BY THE CONSTRUCTION OF THE PROJECT.

NOTE: ALL SIDEWALK, CURB AND GUTTER, DRIVEWAY ENTRANCES, AND ALLEYS ARE TO BE CONSTRUCTED PER:

STANDARD SUPPLEMENTAL SPECIFICATIONS FOR CONSTRUCTION OF PUBLIC INFRASTRUCTURE IN THE CITY OF MINNEAPOLIS 2011 EDITION

BY
DEPARTMENT OF PUBLIC WORKS
MINNEAPOLIS, MINNESOTA
FEBRUARY 1, 2011

NOTE: NO WORK IS TO TAKE PLACE WITHIN RAILROAD ROW WITHOUT PRIOR APPROVAL FROM THE RAILROAD OWNER.

Schafer
Richardson
2025 West River Parkway
2025 W River Road, Minneapolis, MN 55411

URBANWORKS

CONSULTANT

BKBM
ENGINEERS
6100 Earle Brown Drive, Suite 700
Minneapolis, MN 55430
Phone: (612) 843-0400
Fax: (612) 843-0421
www.bkbm.com

PRELIMINARY

NOT FOR
CONSTRUCTION

LAND USE & PDR
APPLICATION
02.28.2020

REVISIONS

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the state of Minnesota.

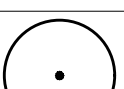








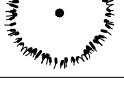


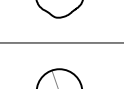
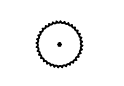
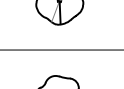
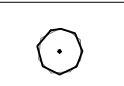
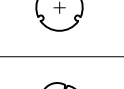
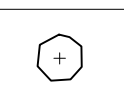
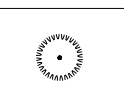
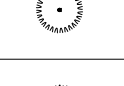
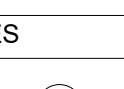
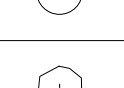

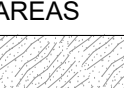
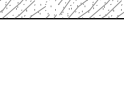




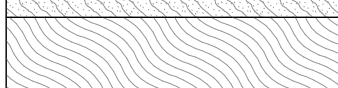


Date: 07/01/2020 U.S. No. 46674

DATE 07/01/2020
PROJECT # 19-0035
PHASE LAND USE & PDR
DRAWN BY WH
CHECKED BY NPA

GRADING,
DRAINAGE, AND
EROSION
CONTROL PLAN

C200

OVERALL REFERENCE NOTES SCHEDULE							
SYMBOL	EDGING						
DESCRIPTION	QTY	DETAIL	MANUFACTURER	PRODUCT/MODEL	COLOR/FINISH	COMMENTS	
ED-01	STEEL EDGING	659 LF	6/L500	RYERSON	3/16" GALV. STEEL	BLACK	4"X 3/16" WITH 12" STAKES - RYERSON OR APPROVED EQUAL
SYMBOL	FENCE & GUARDRAIL						
DESCRIPTION	QTY	DETAIL	MANUFACTURER	PRODUCT/MODEL	COLOR/FINISH	COMMENTS	
FE-01	EXTERIOR FENCE	214 LF	2/L501	OMEGA FENCE SYSTEMS	OMEGA ARCHITECTURAL	BLACK	5' HEIGHT WITH FROST FOOTINGS
SYMBOL	LIGHTING						
DESCRIPTION	QTY	DETAIL	MANUFACTURER	PRODUCT/MODEL	COLOR/FINISH	COMMENTS	
LT-01	STREET LIGHT	4	N/A	TBD	MATCH TO CITY STANDARD	MATCH TO CITY STANDARD	OWNER PURCHASED, CITY INSTALLED
LT-02	PEDESTIAN LIGHT	5	N/A	TBD	MATCH TO CITY STANDARD	MATCH TO CITY STANDARD	OWNER PROVIDED, CITY INSTALLED
LT-03	POLE LIGHT	5	N/A	LIGMAN LIGHTING	MARTINI STREET LIGHT	BLACK	16' HEIGHT POLE
SYMBOL	MINERAL MULCH						
DESCRIPTION	QTY	DETAIL	MANUFACTURER	PRODUCT/MODEL	COLOR/FINISH	COMMENTS	
MM-01	ROCK MULCH	1,163 SF	N/A	N/A	DRESSER GRAY TRAP ROCK	NATURAL	3/4" DIA. 3" DEPTH OVER LANDSCAPE FABRIC
MM-02	CRUSHED STONE	903 SF	3/L501	HEDBERG AGGREGATES	DRESSER GRAY TRAP ROCK	GREY	3" DEPTH - 3/16" MINUS OVER 6" CLASS V BASE
SYMBOL	PAVING						
DESCRIPTION	QTY	DETAIL	MANUFACTURER	PRODUCT/MODEL	COLOR/FINISH	COMMENTS	
P-01	CONCRETE PAVING	10,437 SF	SEE CIVIL	N/A	STANDARD CONCRETE	STANDARD / BROOM FINISH	SEE CIVIL FOR PAVING PROFILE
P-02	SPECIALTY PAVING	1,940 SF	SEE CIVIL	ORJUN STONE			
P-03	PLAY SURFACE	1,910 SF	N/A	N/A	HARDWOOD MULCH	NATURAL	DEPTH TO BE DETERMINED BY PLAYGROUND MANUFACTUER
SYMBOL	SITE FURNITURE						
DESCRIPTION	QTY	DETAIL	MANUFACTURER	PRODUCT/MODEL	COLOR/FINISH	COMMENTS	
SF-01	SITE BENCH - TYPE A	6	4/L501	LANDSCAPE FORMS	BANCAL BENCH	IPE / METAL	88" BACKED, SURFACE MOUNT PER MANUFACTURER'S DIRECTION
SF-02	BIKE RACK	10	5/L501	FORMS+SURFACES	CAPITOL BIKE RACK	DARK GREY / METALLIC	EMBEDDED INTO CONCRETE PER MANUFACTURER'S DIRECTION

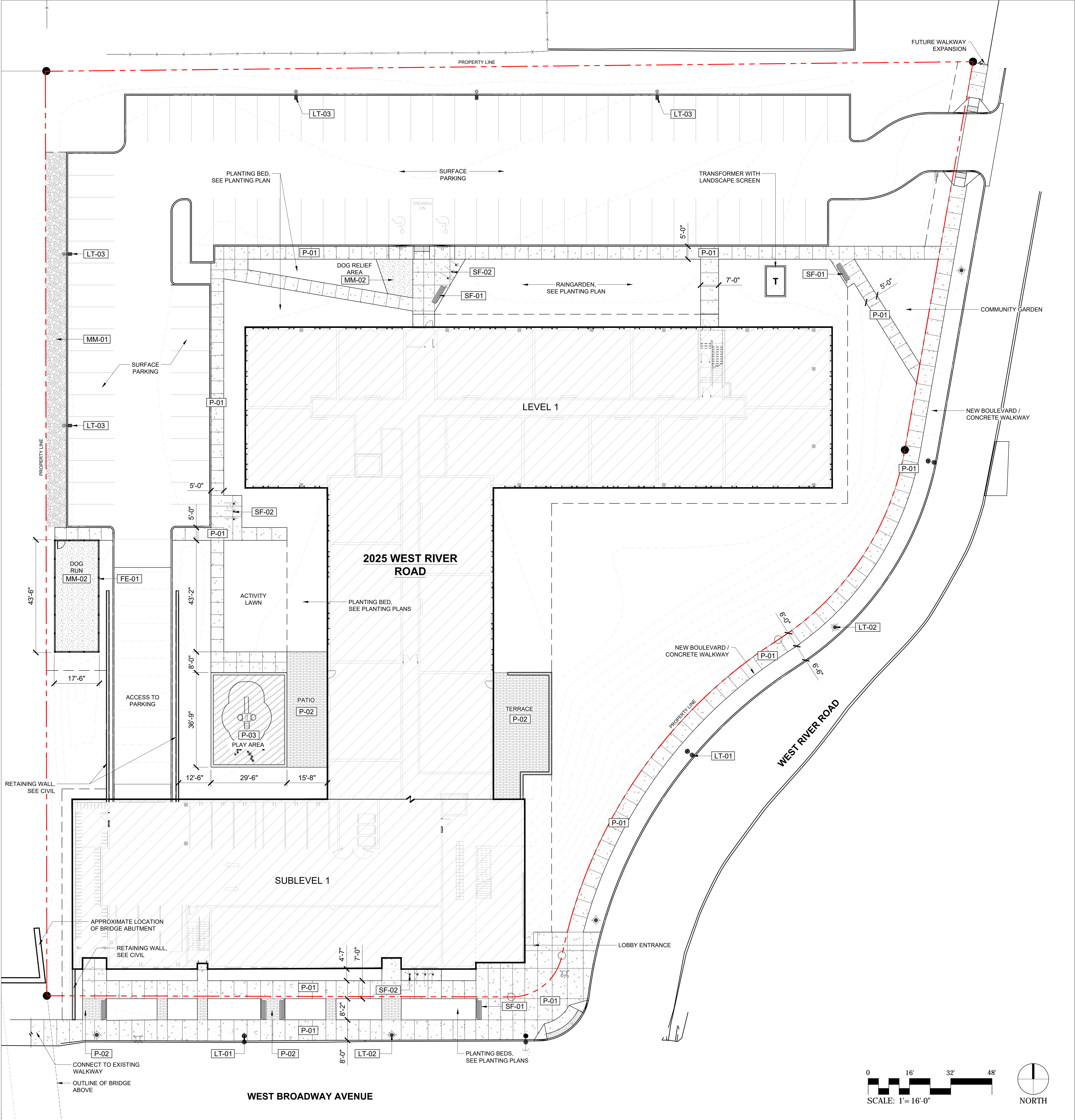
OVERALL PLANT SCHEDULE						
TREES	CODE	QTY	BOTANICAL / COMMON NAME	SIZE	CONT.	NOTES
	AC-P	4	ACER PLATANOIDES 'POND' TM / EMERALD LUSTRE MAPLE	3" CAL.	B&B	NATURAL FORM, SINGLE LEADER
	AM-G	9	AMELANCHIER X GRANDIFLORA 'AUTUMN BRILLIANCE' / 'AUTUMN BRILLIANCE' SERVICEBERRY	8" HEIGHT CLUMP	B&B	CLUMP FORM, 3 STEM MINIMUM
	BE-P	8	BETULA POPULIFOLIA 'WHITESPIRE' / WHITESPIRE BIRCH	2.5" CAL.	B&B	NATURAL FORM, SINGLE LEADER
	GI-B	6	GINKGO BILOBA 'AUTUMN GOLD' TM / MAIDENHAIR TREE	2.5" CAL.	B&B	TRUNK FREE OF BRANCHES 6'-7' FROM BASE
	GL-T	8	GLEDITSIA TRIACANTHOS VAR. INERMIS 'DRAVES' / STREET KEEPER HONEY LOCUST	3" CAL.	B&B	NATURAL FORM, SINGLE LEADER
	GY-D	7	GYMNOCLADUS DIOICA 'ESPRESSO' / KENTUCKY COFFEETREE	2.5" CAL.	B&B	TRUNK FREE OF BRANCHES 6'-7' FROM BASE
	QU-B	11	QUERCUS BICOLOR / SWAMP WHITE OAK	3" CAL.	B&B	TRUNK FREE OF BRANCHES 6'-7' FROM BASE
	QU-E	9	QUERCUS ELLIPSOIDALIS / NORTHERN PIN OAK	3" CAL.	B&B	TRUNK FREE OF BRANCHES 6'-7' FROM BASE
	SO-D	6	SORBUS DECORA / SHOWY MOUNTAIN ASH	2.5" CAL.	B&B	TRUNK FREE OF BRANCHES 6'-7' FROM BASE
EVERGREEN TREES	CODE	QTY	BOTANICAL / COMMON NAME	SIZE	CONT.	NOTES
	PI-N	8	PINUS NIGRA / AUSTRIAN BLACK PINE	10' HT.	B&B	ROOT PRUNED STOCK - CAN BE B&B OR SPADE
	TH-O	25	THUJA OCCIDENTALIS 'TECHNY' / TECHNY ARBORVITAE	8' HT.	B&B	NATURAL, FULL FORM
SHRUBS	CODE	QTY	BOTANICAL / COMMON NAME	SIZE	CONT.	NOTES
	AR-M	75	ARONIA MELANOCARPA 'UCONNAM165' / LOW SCAPE MOUND CHOKEBERRY	#2	CONT.	SPACE 2'-0" O.C.
	CO-R	173	CORNUS RACEMOSA 'MUSZAM' / MUSKINGUM DOGWOOD	#5	CONT.	SPACE 4'-0" O.C.
	CO-S	26	CORNUS SERICEA 'ALLEMAN'S COMPACT' / DWARF RED TWIG DOGWOOD	#10	CONT.	SPACE 4'-0" O.C.
	DI-L	117	DIERVILLA LONICERA / DWARF BUSH HONEYSUCKLE	#5	CONT.	SPACE 3'-0" O.C.
	HY-P	63	HYDRANGEA PANICULATA 'JANE' / LITTLE LIME HYDRANGEA	#3	CONT.	SPACE 4'-0" O.C.
	PH-O	30	PHYSOCARPUS OPULIFOLIUS 'DONNA MAY' / DONNA MAY NINEBARK	#2	CONT.	SPACE 3'-6" O.C.
	RH-A	144	RHUS AROMATICA 'GRO-LOW' / GRO-LOW FRAGRANT SUMAC	#5	CONT.	SPACE 4'-0" O.C.
	SO-S	120	SORBARIA SORBIFOLIA 'SEM' / SEM ASH LEAF SPIREA	#2	CONT.	SPACE 3'-0" O.C.
	SP-B	141	SPIRAEA X BUMALDA 'ANTHONY WATERER' / ANTHONY WATERER SPIRAEA	#5	CONT.	SPACE 4'-0" O.C.
	VI-O	74	VIBURNUM OPULUS 'NANUM' / DWARF EUROPEAN VIBURNUM	#2	CONT.	SPACE 2'-6" O.C.
EVERGREEN SHRUBS	CODE	QTY	BOTANICAL / COMMON NAME	SIZE	CONT.	NOTES
	JU-S	31	JUNIPERUS SQAMATA 'BLUE STAR' / BLUE STAR JUNIPER	#5	CONT.	SPACE 3'-0" O.C.
	TA-N	50	TAXUS CUSPIDATA 'NOVA' / NOVA YEW	#5	CONT.	SPACE 3'-0" O.C.
	TH-H	32	THUJA OCCIDENTALIS 'HOLMSTRUP' / HOLMSTRUP CEDAR	5' HEIGHT	B&B	SPACE 3'-0" O.C.
GRASSES	CODE	QTY	BOTANICAL / COMMON NAME	SIZE	CONT.	NOTES
	CA-A	80	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / FEATHER REED GRASS	#3	CONT.	SPACE 2'-6" O.C.
	SC-S	108	SCHIZACHYRIUM SCOPARIUM 'BLUE HEAVEN' / BLUE HEAVEN LITTLE BLUESTEM	#3	CONT.	SPACE 2'-0" O.C.
ANNUALS/PERENNIALS	CODE	QTY	BOTANICAL / COMMON NAME	SIZE	CONT.	NOTES
	PN	30	PEROVSKIA ATRIPLICIFOLIA 'BLUE JEAN BABY' / BLUE JEAN BABY RUSSIAN SAGE	#1	CONT.	SPACE 2'-0" O.C.
SHRUB AREAS	CODE	QTY	BOTANICAL / COMMON NAME	SIZE	CONT.	NOTES
		2,079 SF	RAINGARDEN MIX			
		55	ALLIUM STELLATUM / PRAIRIE ONION	#1	CONT.	SPACE 24" O.C.
		82	IRIS VERSICOLOR / BLUE FLAG	#1	CONT.	SPACE 24" O.C.
		82	LIATRIS SPICATA 'KOBOLD' / SPIKE GAYFEATHER	#1	CONT.	SPACE 24" O.C.
		217	PANICUM VIRGATUM / SWITCH GRASS	#1	CONT.	SPACE 24" O.C.
		109	RUDBECKIA HIRTA / BLACK-EYED SUSAN	#1	CONT.	SPACE 24" O.C.
GROUND COVERS	CODE	QTY	BOTANICAL / COMMON NAME	SIZE	CONT.	NOTES
	SM-01	10,126 SF	SEED MIX-01 / MESIC SHORT GRASS PRAIRIE MIX	N/A	SEED	B.O.D. SHOOTING STAR NATIVE SEEDS
	SM-02	1,601 SF	SEED MIX-02 / SLOPE STABILIZATION MIX	N/A	SEED	B.O.D. SHOOTING STAR NATIVE SEEDS
	T-SEED	3,954 SF	TURF SEED	N/A	SEED	SEE SPECIFICATION SECTION 329200
	T-SOD	5,255 SF	TURF SOD	N/A	ROLL	SEE SPECIFICATION SECTION 329200

REFERENCE NOTES SCHEDULE

EDGING			
SYMBOL	DESCRIPTION	QTY	DETAIL
ED-01	STEEL EDGING	659 LF	6/L500
FENCE & GUARDRAIL			
SYMBOL	DESCRIPTION	QTY	DETAIL
FE-01	EXTERIOR FENCE	214 LF	2/L501
LIGHTING			
SYMBOL	DESCRIPTION	QTY	DETAIL
LT-01	STREET LIGHT	4	N/A
LT-02	PEDESTIAN LIGHT	5	N/A
LT-03	POLE LIGHT	5	N/A
MINERAL MULCH			
SYMBOL	DESCRIPTION	QTY	DETAIL
MM-01	ROCK MULCH	1,163 SF	N/A
MM-02	CRUSHED STONE	903 SF	3/L501
PAVING			
SYMBOL	DESCRIPTION	QTY	DETAIL
P-01	CONCRETE PAVING	10,437 SF	SEE CIVIL
P-02	SPECIALTY PAVING	1,940 SF	SEE CIVIL
P-03	PLAY SURFACE	1,910 SF	N/A
SITE FURNITURE			
SYMBOL	DESCRIPTION	QTY	DETAIL
SF-01	SITE BENCH - TYPE A	6	4/L501
SF-02	BIKE RACK	10	5/L501

SITE INFORMATION:

SITE AREA:	105,846 SF
BUILDING FOOTPRINT:	33,351 SF (32%)
LANDSCAPE / PERVIOUS AREA:	35,684 SF (33%)
HARDSCAPE / IMPERVIOUS AREA:	36,811 SF (35%)



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PLANT SCHEDULE

TREES	CODE	BOTANICAL / COMMON NAME
	AC-P	ACER PLATANOIDES 'POND' TM / EMERALD LUSTRE MAPLE
	AM-G	AMELANCHIER X GRANDIFLORA 'AUTUMN BRILLIANCE' / 'AUTUMN BRILLIANCE' SERVICEBERRY
	BE-P	BETULA POPULIFOLIA 'WHITESPIRE' / WHITESPIRE BIRCH
	GI-B	GINKGO BILOBA 'AUTUMN GOLD' TM / MAIDENHAIR TREE
	GL-T	GLEDITSIA TRIACANTHOS VAR. INERMIS 'DRAVES' / STREET KEEPER HONEY LOCUST
	GY-D	GYMNOCLADUS DIOICA 'ESPRESSO' / KENTUCKY COFFEETREE
	QU-B	QUERCUS BICOLOR / SWAMP WHITE OAK
	QU-E	QUERCUS ELLIPSOIDALIS / NORTHERN PIN OAK
	SO-D	SORBUS DECORA / SHOWY MOUNTAIN ASH
EVERGREEN TREES	CODE	BOTANICAL / COMMON NAME
	PI-N	PINUS NIGRA / AUSTRIAN BLACK PINE
	TH-O	THUJA OCCIDENTALIS 'TECHNY' / TECHNY ARBORVITAE
SHRUBS	CODE	BOTANICAL / COMMON NAME
	AR-M	ARONIA MELANOCARPA 'UCONNAM165' / LOW SCAPE MOUND CHOKEBERRY
	CO-R	CORNUS RACEMOSA 'MUSZAM' / MUSKINGUM DOGWOOD
	CO-S	CORNUS SERICEA 'ALLEMAN'S COMPACT' / DWARF RED TWIG DOGWOOD
	DI-L	DIERVILLA LONICERA / DWARF BUSH HONEYSUCKLE
	HY-P	HYDRANGEA PANICULATA 'JANE' / LITTLE LIME HYDRANGEA
	PH-O	PHYSOCARPUS OPULIFOLIUS 'DONNA MAY' / DONNA MAY NINEBARK
	RH-A	RHUS AROMATICA 'GRO-LOW' / GRO-LOW FRAGRANT SUMAC
	SO-S	SORBARIA SORBIFOLIA 'SEM' / SEM ASH LEAF SPIREA
	SP-B	SPIRAEA X BUMALDA 'ANTHONY WATERER' / ANTHONY WATERER SPIRAEA
	VI-O	VIBURNUM OPULUS 'NANUM' / DWARF EUROPEAN VIBURNUM
EVERGREEN SHRUBS	CODE	BOTANICAL / COMMON NAME
	JU-S	JUNIPERUS SQAMATA 'BLUE STAR' / BLUE STAR JUNIPER
	TA-N	TAXUS CUSPIDATA 'NOVA' / NOVA YEW
	TH-H	THUJA OCCIDENTALIS 'HOLMSTRUP' / HOLMSTRUP CEDAR
GRASSES	CODE	BOTANICAL / COMMON NAME
	CA-A	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / FEATHER REED GRASS
	SC-S	SCHIZACHYRIUM SCOPARIUM 'BLUE HEAVEN' / BLUE HEAVEN LITTLE BLUESTEM
ANNUALS/PERENNIALS	CODE	BOTANICAL / COMMON NAME
	PN	PEROVSKIA ATRIPLICIFOLIA 'BLUE JEAN BABY' / BLUE JEAN BABY RUSSIAN SAGE
SHRUB AREAS	CODE	BOTANICAL / COMMON NAME
		RAINGARDEN MIX
		ALLIUM STELLATUM / PRAIRIE ONION
		IRIS VERSICOLOR / BLUE FLAG
		LIATRIS SPICATA 'KOBOLD' / SPIKE GAYFEATHER
		PANICUM VIRGATUM / SWITCH GRASS
		RUDBECKIA HIRTA / BLACK-EYED SUSAN
GROUND COVERS	CODE	BOTANICAL / COMMON NAME
	SM-01	SEED MIX-01 / MESIC SHORT GRASS PRAIRIE MIX
	SM-02	SEED MIX-02 / SLOPE STABILIZATION MIX
	T-SEED	TURF SEED
	T-SOD	TURF SOD

MINIMUM LANDSCAPE REQUIREMENTS:

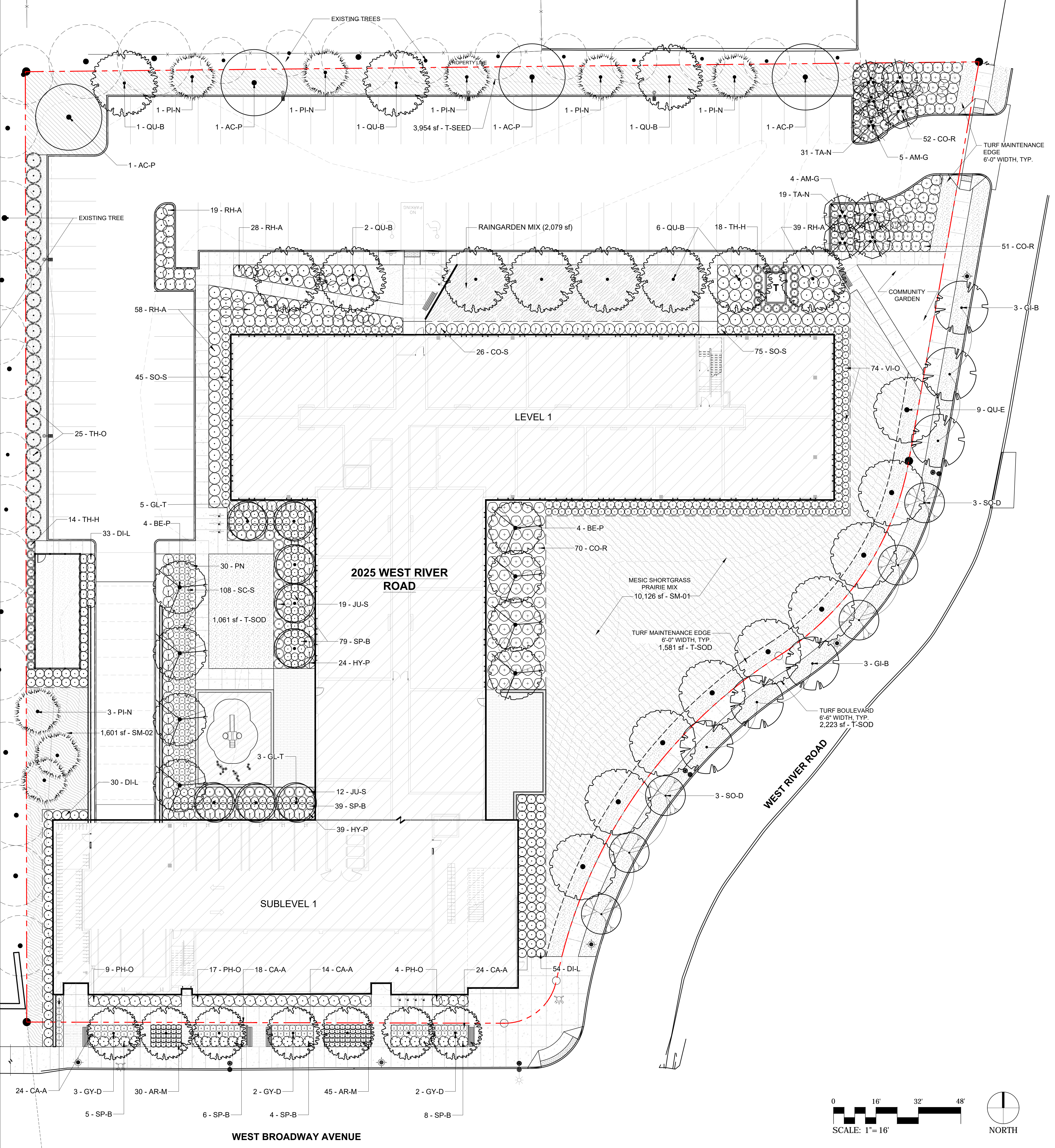
NOT LESS THAN TWENTY (20) PERCENT OF THE SITE NOT OCCUPIED BY BUILDINGS INCLUDING ALL REQUIRED LANDSCAPED YARDS SHALL BE LANDSCAPED AS FOLLOWS:

TOTAL AREA NOT OCCUPIED BY BUILDINGS: 72,495 SF

MINIMUM REQUIRED LANDSCAPE: 14,499 SF REQUIRED / 35,684 SF PROVIDED

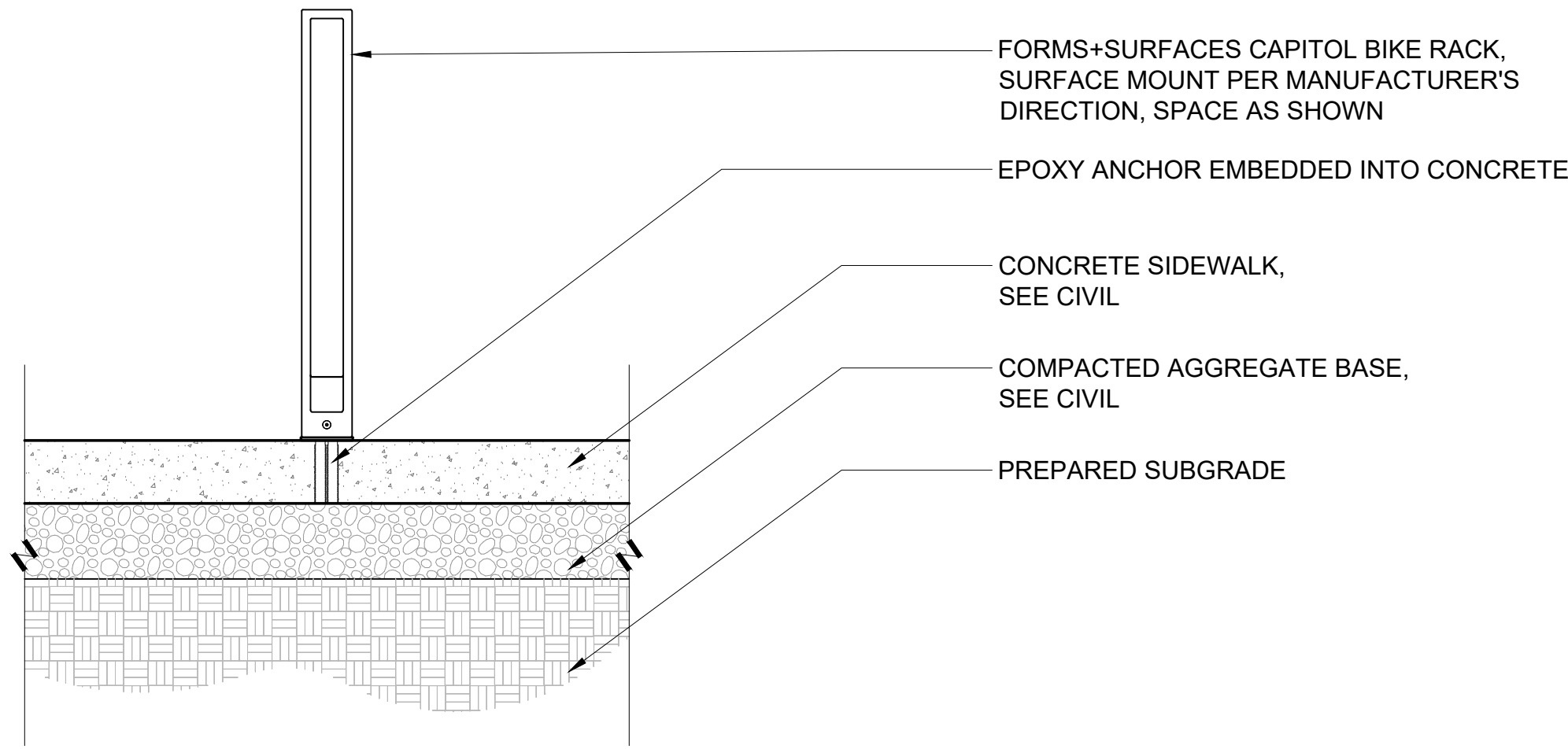
CANOPY TREES: 29 TREES REQUIRED / 82 PROVIDED
ONE (1) CANOPY TREE FOR EACH FIVE HUNDRED (500) SQUARE FEET

SHRUBS: 145 SHRUBS REQUIRED / 978 PROVIDED
ONE (1) SHRUB FOR EACH ONE HUNDRED (100) SQUARE FEET

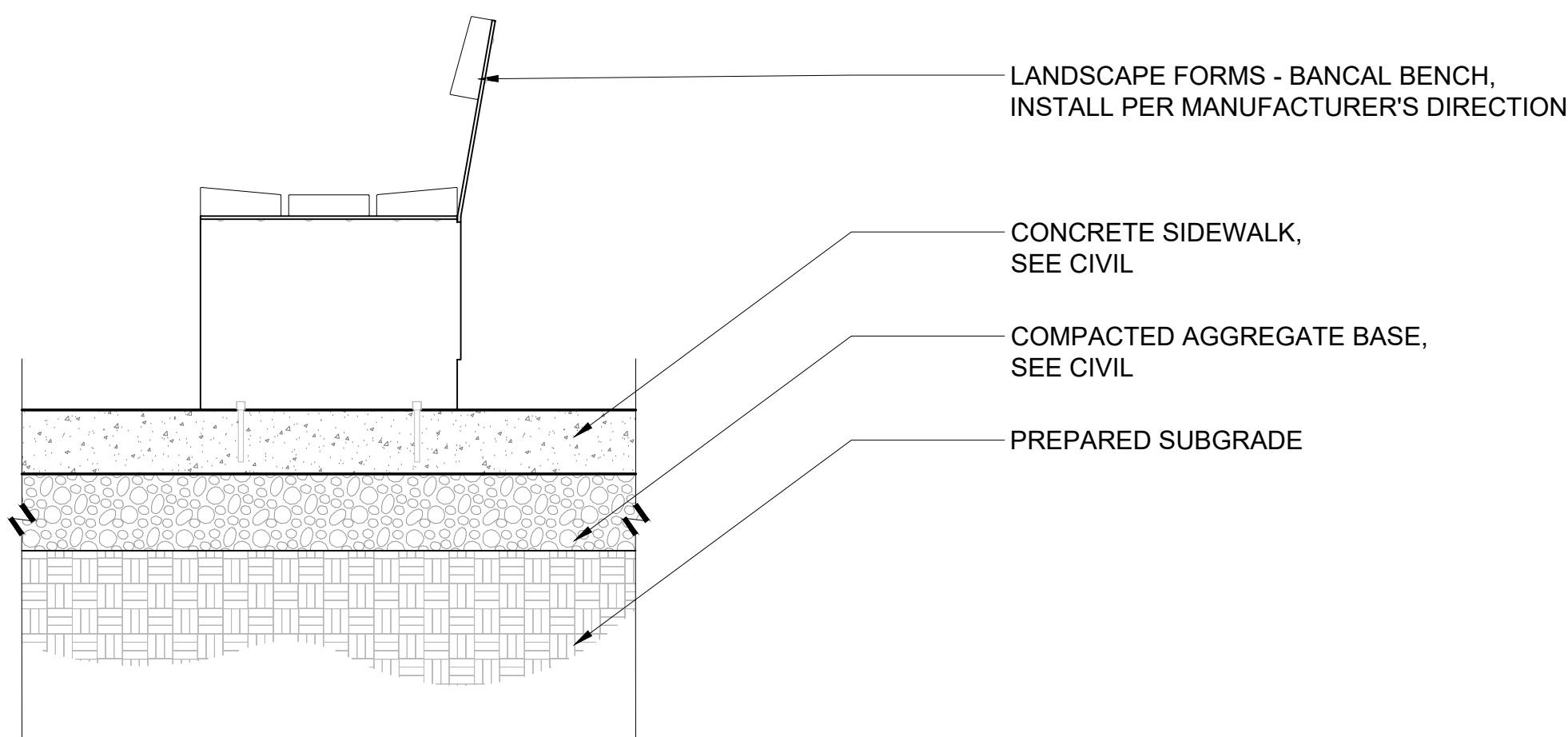


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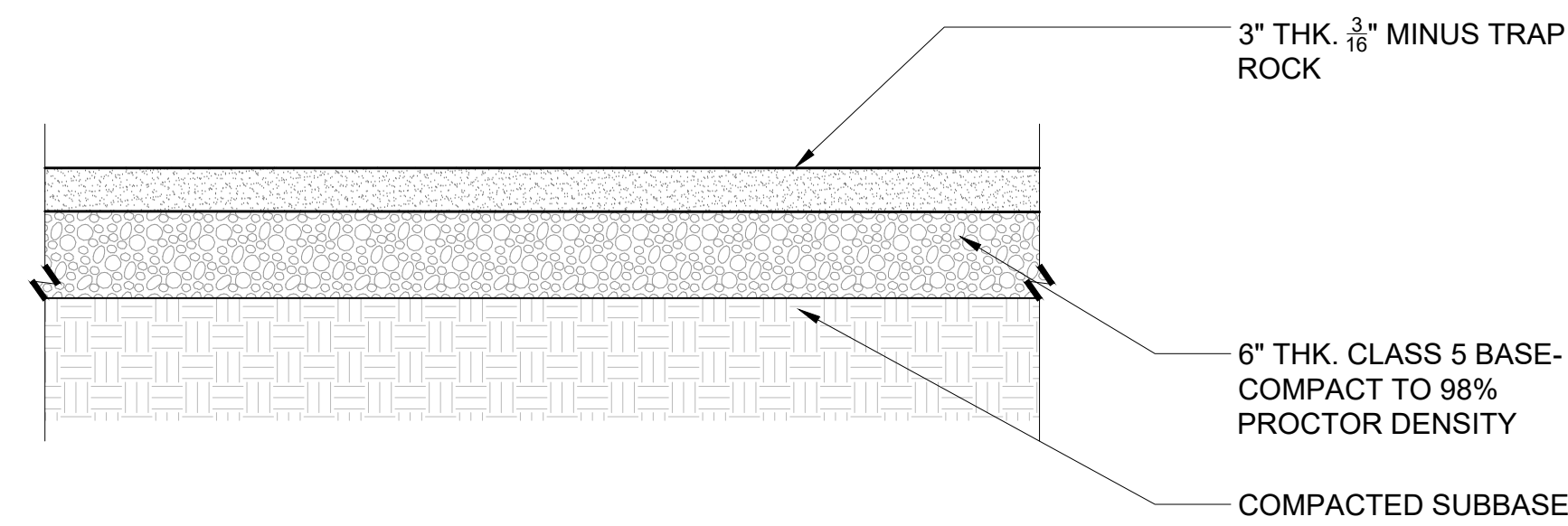
5 BIKE RACK - IN GROUND MOUNT
1" = 1'-0" P-20 104-11



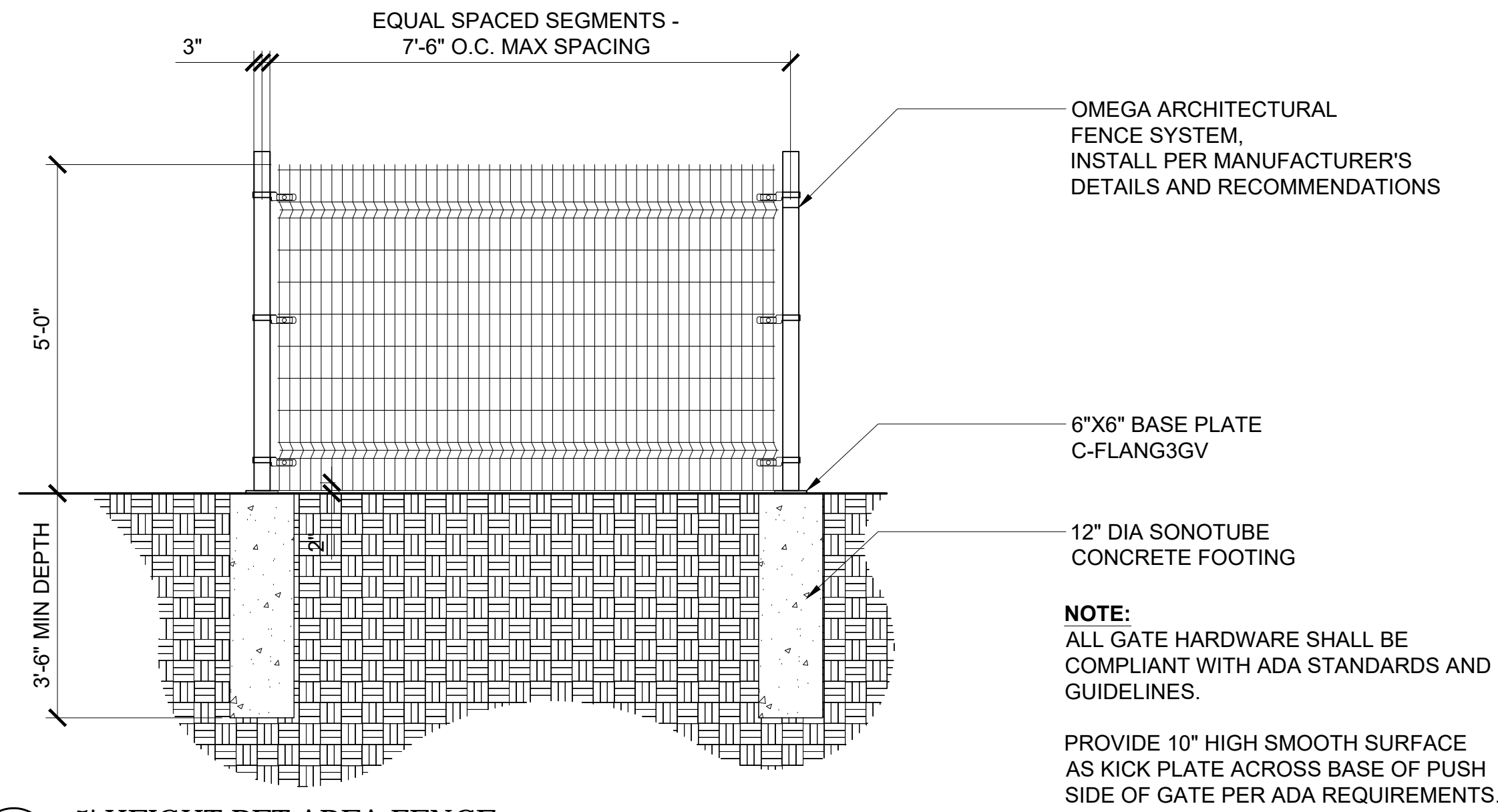
4 SITE BENCH - INGROUND MOUNT
1" = 1'-0" P-19.242-75



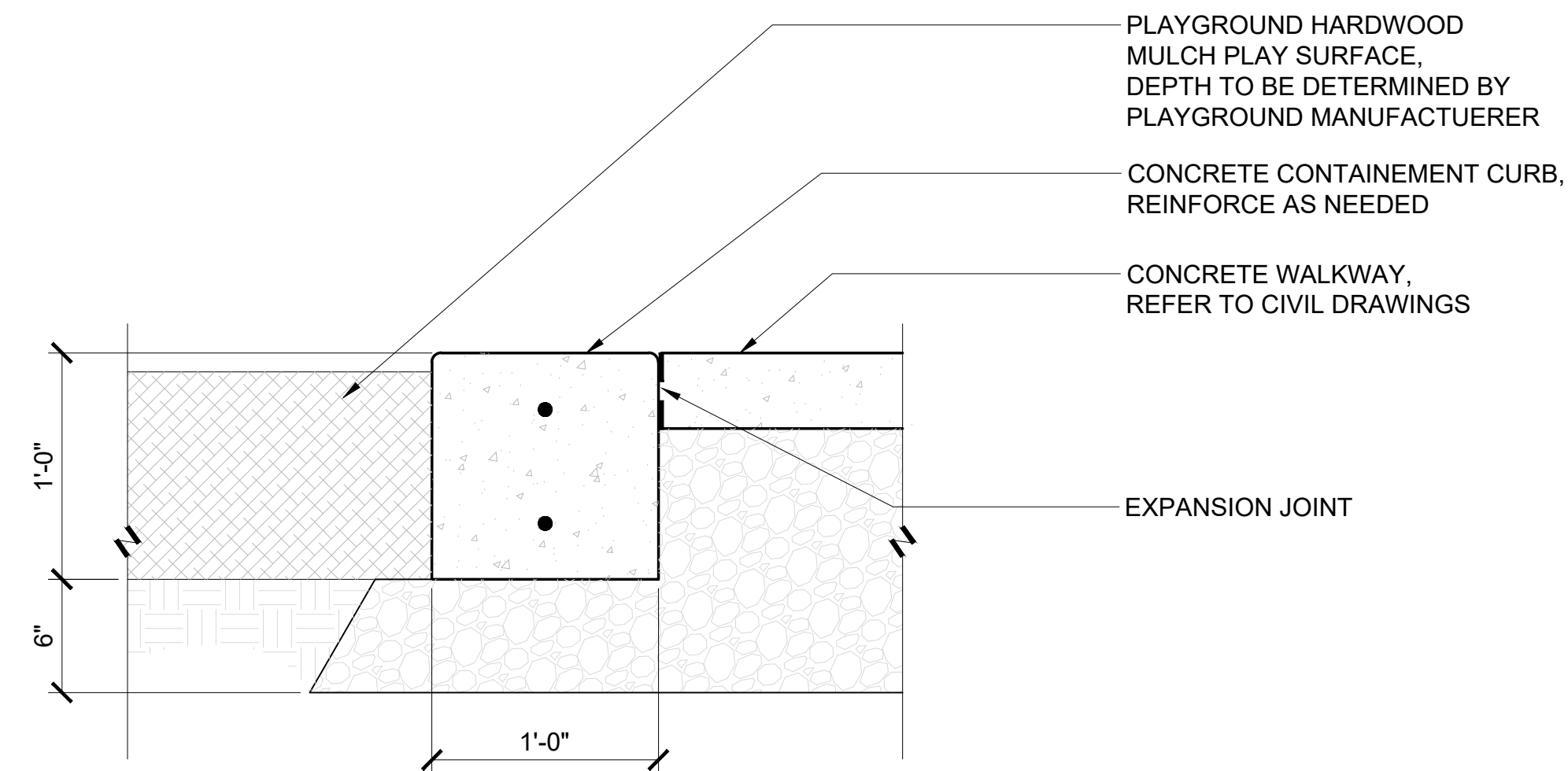
3 CRUSHED STONE PAVING
1" = 1'-0" P-20 104-10



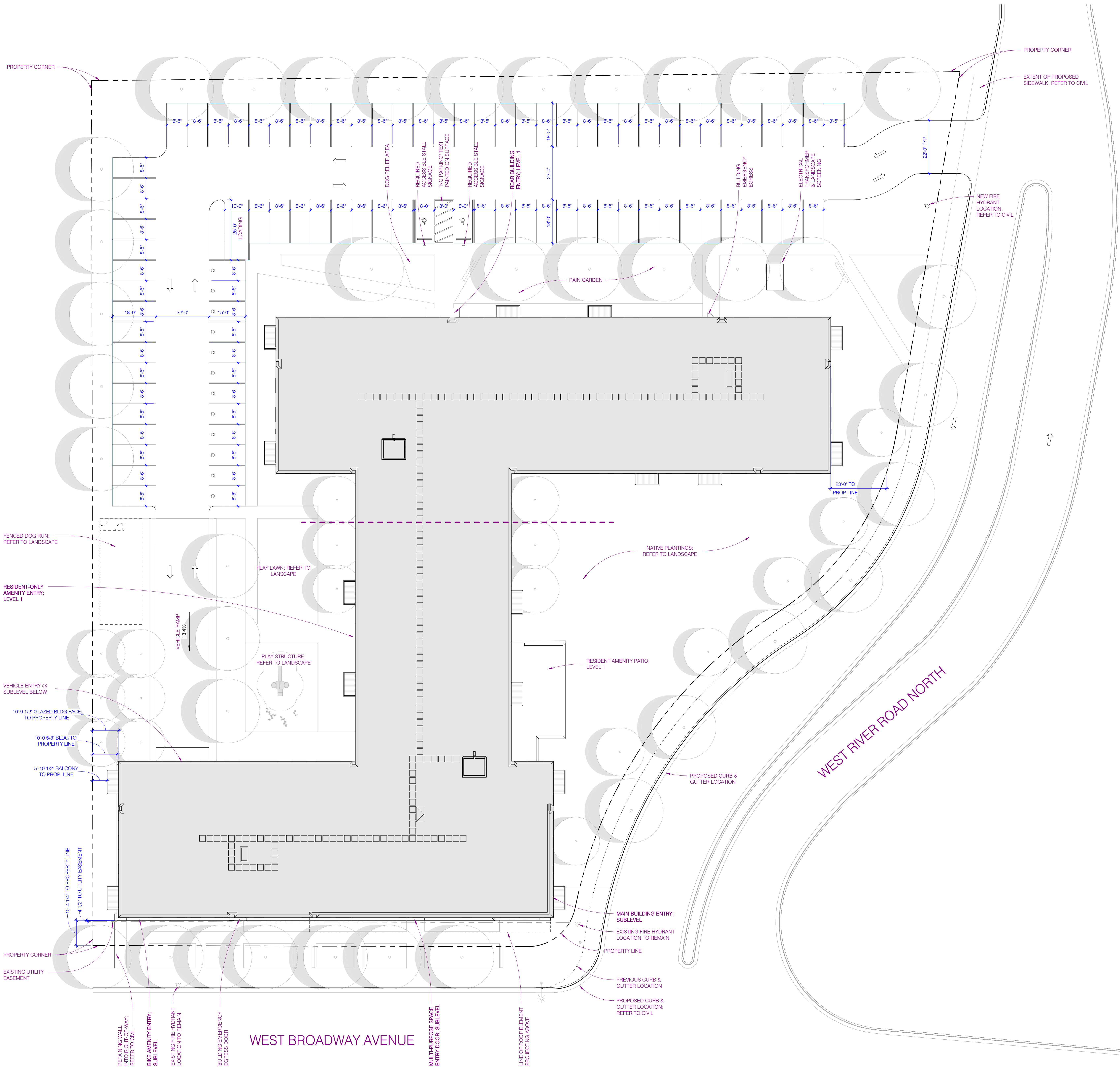
2 5' HEIGHT PET AREA FENCE
1/2" = 1'-0" P-20 104-09 P-20 104-08



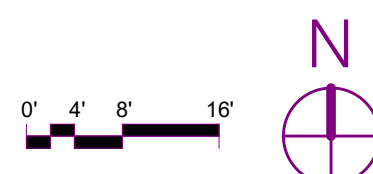
1 CONTAINMENT CURB AT PLAYGROUND
1 1/2" = 1'-0" P-20 104-08 P-20 104-09



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1 ARCHITECTURAL LANDSCAPE PLAN
AL101 | 1/16" = 1'-0"



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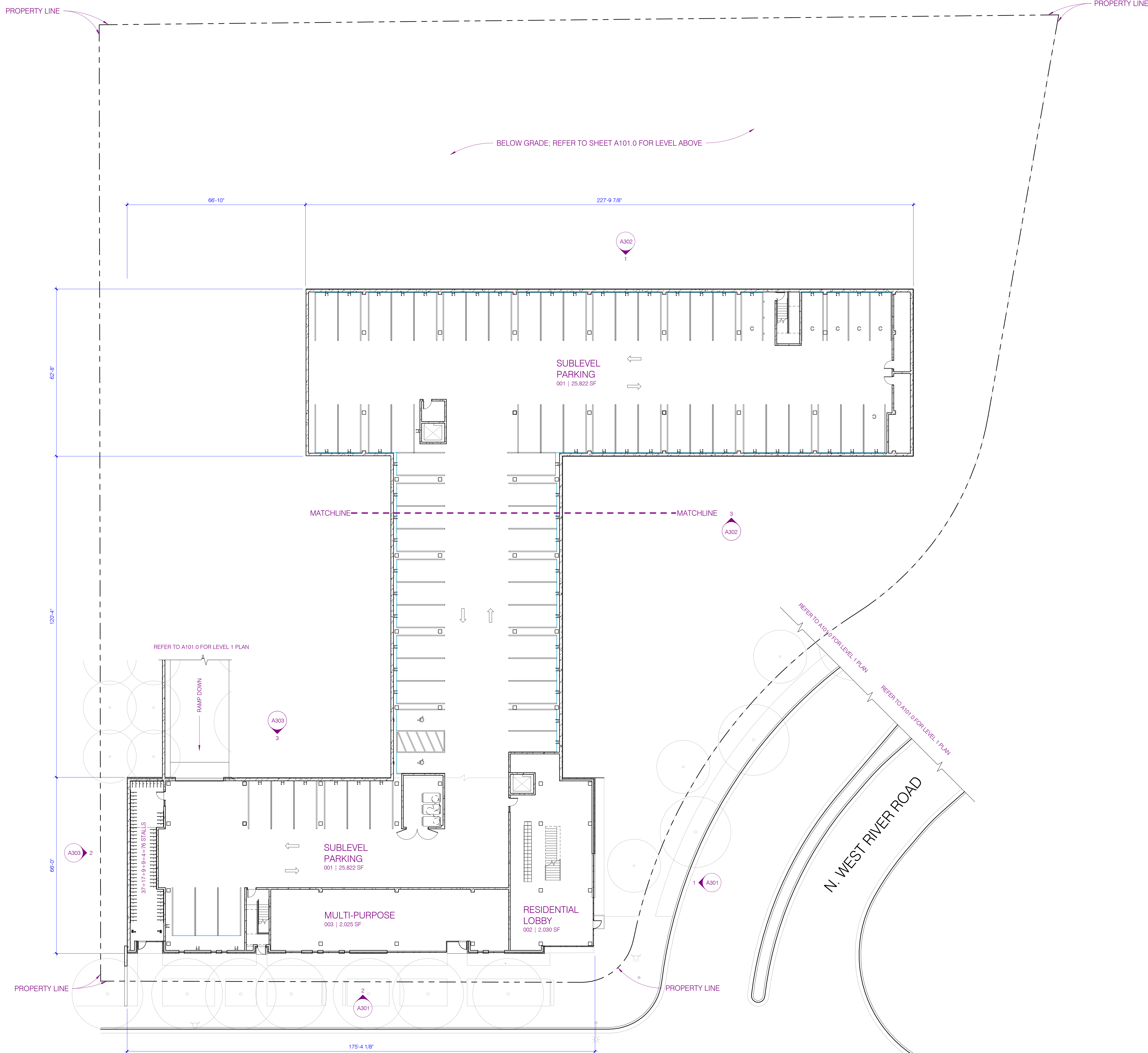
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ARCHITECTURAL
SITE PLAN

AL101

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1 A001.0 - SUBLEVEL 1 PLAN - OVERALL
A001.0 1/16" = 1'-0"

GENERAL PLAN NOTES:

1. DIMENSIONS AT EXTERIOR WALLS ARE TO OUTSIDE FACE OF SHEATHING
2. DIMENSIONS AT INTERIOR WALLS ARE TO FACE OF WALL UNLESS OTHERWISE NOTED
3. DIMENSIONS AT UNIT DEMISING ARE TO CENTERLINE OF WALLS
4. DIMENSIONS AT CMU ARE TO FACE OF CMU UNLESS OTHERWISE NOTED
5. DIMENSIONS AT CAST-IN-PLACE CONCRETE WALLS ARE TO FACE OF WALL UNLESS OTHERWISE NOTED
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10. ALL RESIDENTIAL CORRIDOR WALLS AT METAL STUD LOCATIONS ARE TYPE **B6**, 1 HR FIRE RATING, WITH ACOUSTIC RATINGS - UNLESS OTHERWISE NOTED
11. ALL RESIDENTIAL CORRIDOR WALLS AT WOOD STUD LOCATIONS ARE TYPE **W6**, 1 HR FIRE RATING, WITH ACOUSTIC RATING - UNLESS OTHERWISE NOTED
12. ALL UNITS ARE TYPE B ACCESSIBLE UNLESS OTHERWISE NOTED
13. MAINTAIN 8'-2" MIN. CLEAR HEIGHT AT PARKING AREAS INDICATED ON PLANS, ALL OTHER PARKING AREAS SHALL PROVIDE 7'-2" MIN. CLEAR HEIGHT
14. PROVIDE WALL BLOCKING AT KITCHEN WALLS AND ALL BATHROOM LOCATIONS INDICATED ON SHEET A000

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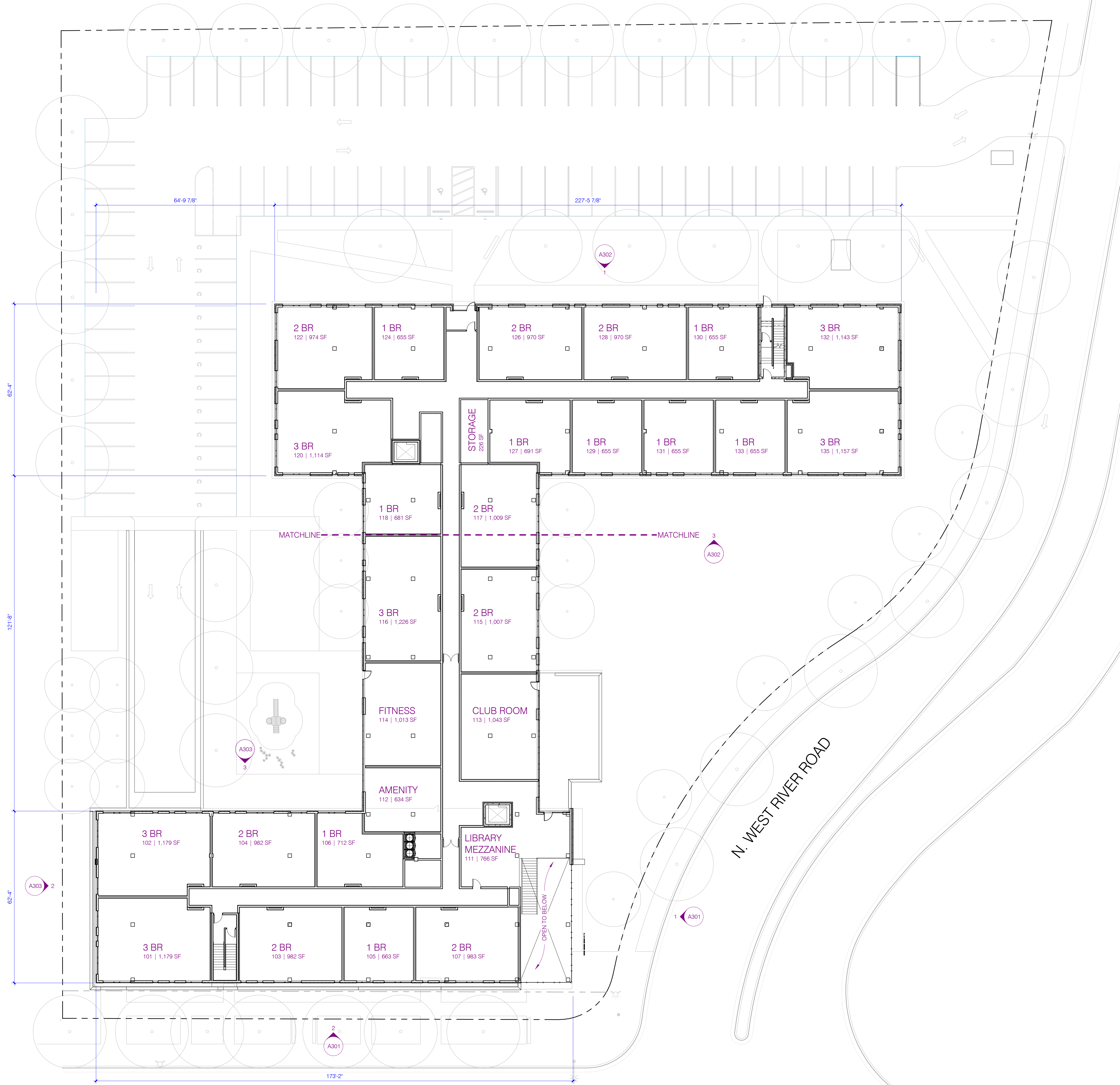
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SUBLEVEL PLAN - OVERALL

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GENERAL PLAN NOTES:

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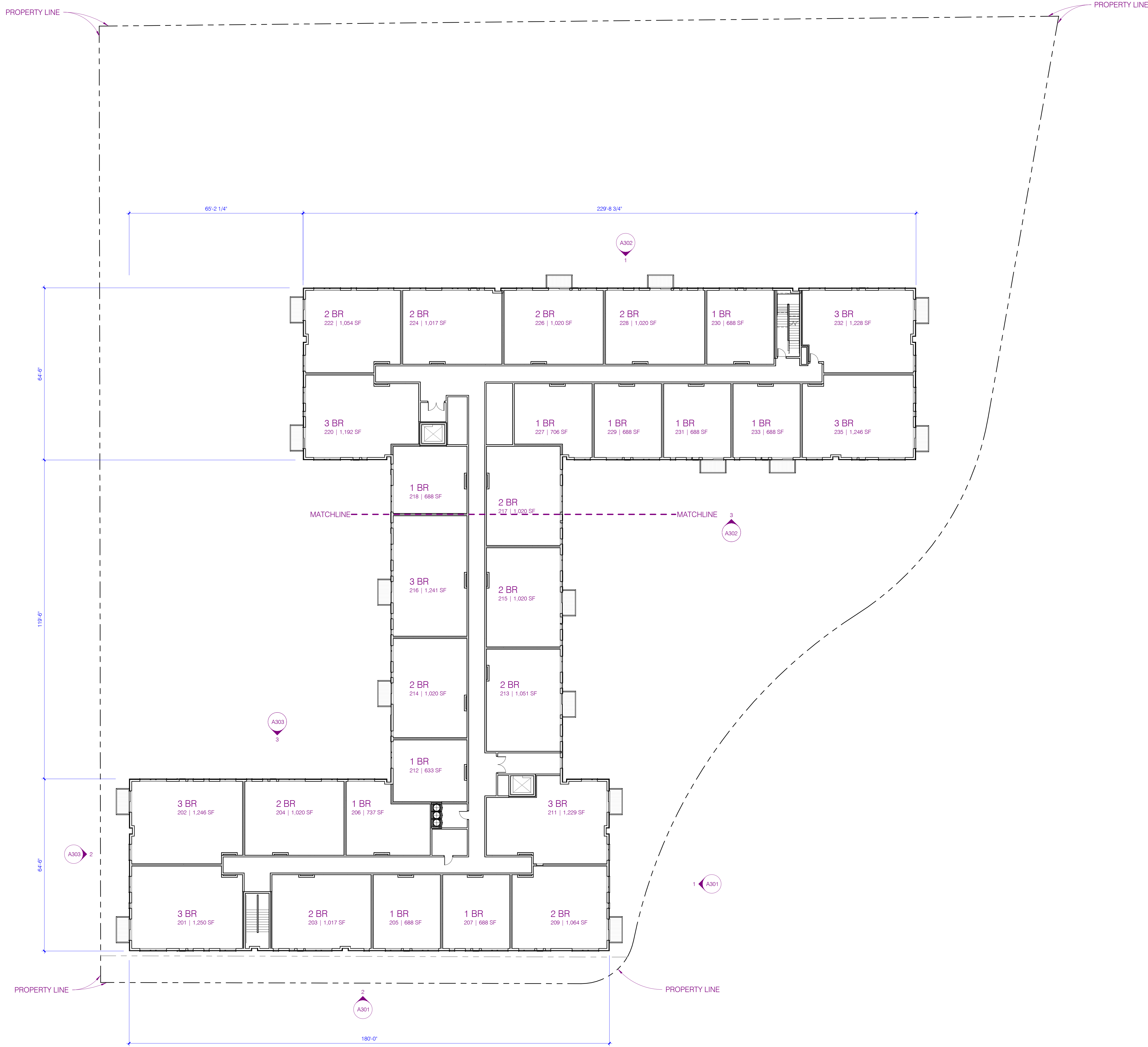
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**LEVEL 1 PLAN -
OVERALL**

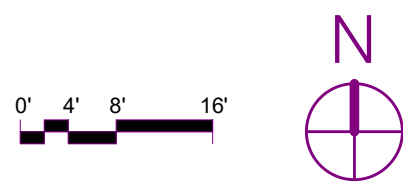
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1 A102.0 - LEVEL 2 PLAN - OVERALL
[A102.0] 1/16" = 1'-0"



GENERAL PLAN NOTES:

- DIMENSIONS AT EXTERIOR WALLS ARE TO OUTSIDE FACE OF SHEATHING
- DIMENSIONS AT INTERIOR WALLS ARE TO FACE OF WALL UNLESS OTHERWISE NOTED
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- PROVIDE WALL BLOCKING AT KITCHEN WALLS AND ALL BATHROOM LOCATIONS INDICATED ON SHEET A000

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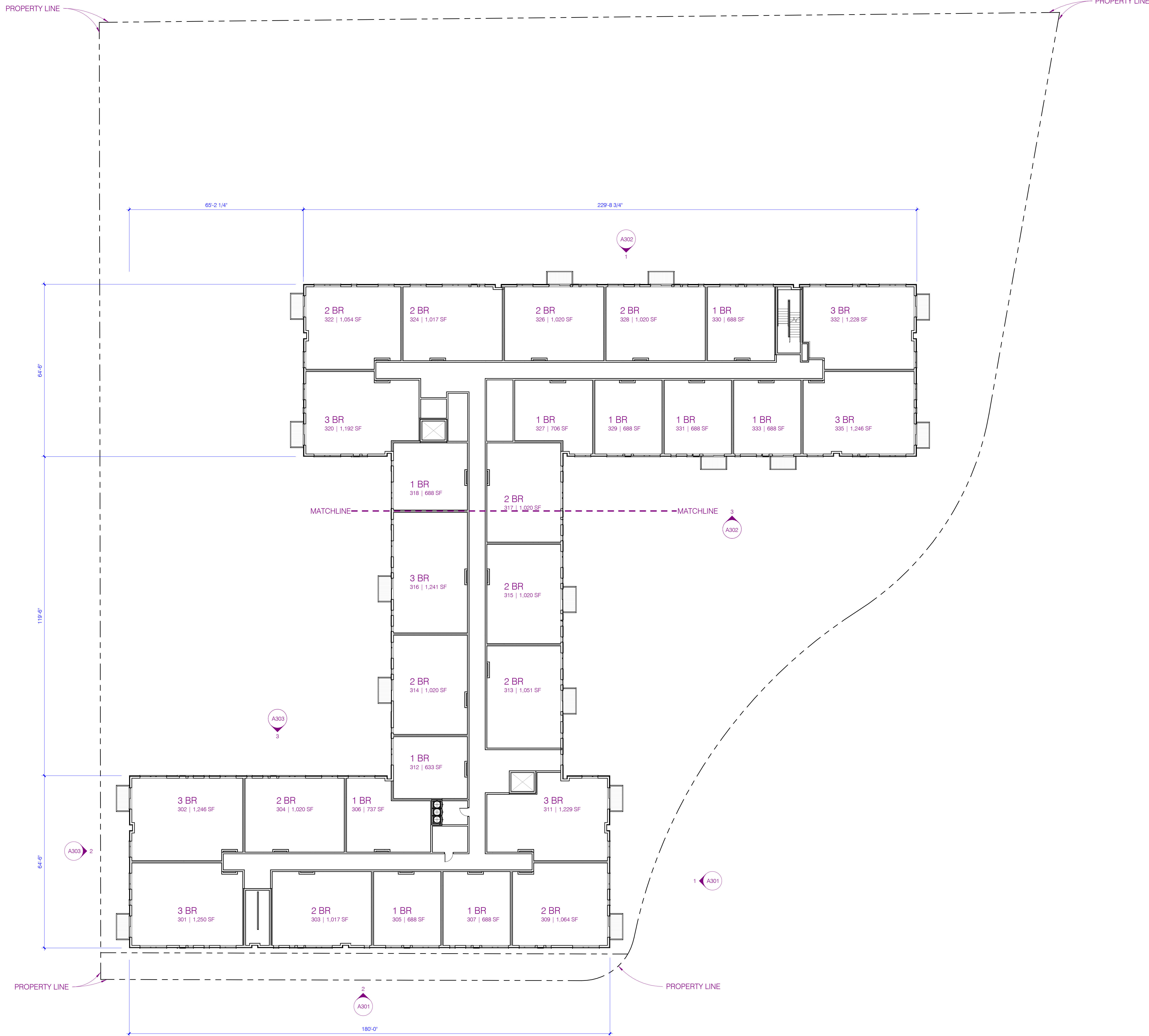
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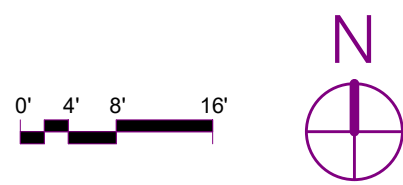
LEVEL 2 PLAN -
OVERALL

A102.0

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1 A103.0 - LEVEL 3 PLAN - OVERALL
1/16" = 1'-0"



GENERAL PLAN NOTES:

- DIMENSIONS AT EXTERIOR WALLS ARE TO OUTSIDE FACE OF SHEATHING
- DIMENSIONS AT INTERIOR WALLS ARE TO FACE OF WALL UNLESS OTHERWISE NOTED
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- PROVIDE WALL BLOCKING AT KITCHEN WALLS AND ALL BATHROOM LOCATIONS INDICATED ON SHEET A1000

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LEVEL 3 PLAN -
OVERALL

A103.0

1. DIMENSIONS AT EXTERIOR WALLS ARE TO OUTSIDE FACE OF SHEATHING
2. DIMENSIONS AT INTERIOR WALLS ARE TO FACE OF WALL UNLESS OTHERWISE NOTED
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7. NO PENETRATIONS OR OPENINGS ALLOWED IN SHAF WALLS UNLESS THEY EXPRESSLY SEVE THE SHAF
8. ALL RESIDENTIAL DEMISING WALLS AT METAL STUD LOCATIONS ARE TYPE **K6**, **K8**, **K6A**, **K8**, **K6A**, AND **K6B** 1 HR FIRE RATING, WITH ACUSTIC RATING
9. ALL RESIDENTIAL DEMISING WALLS AT WOOD STUD LOCATIONS ARE TYPE **V3**, **V6**, **V6A**, **V8**, **V6A**, AND **V8B** 1 HR FIRE RATING, WITH ACUSTIC RATING
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14. PROVIDE WALL BLOCKING AT KITCHEN WALLS AND ALL BATHROOM LOCATIONS INDICATED ON SHEET AR000

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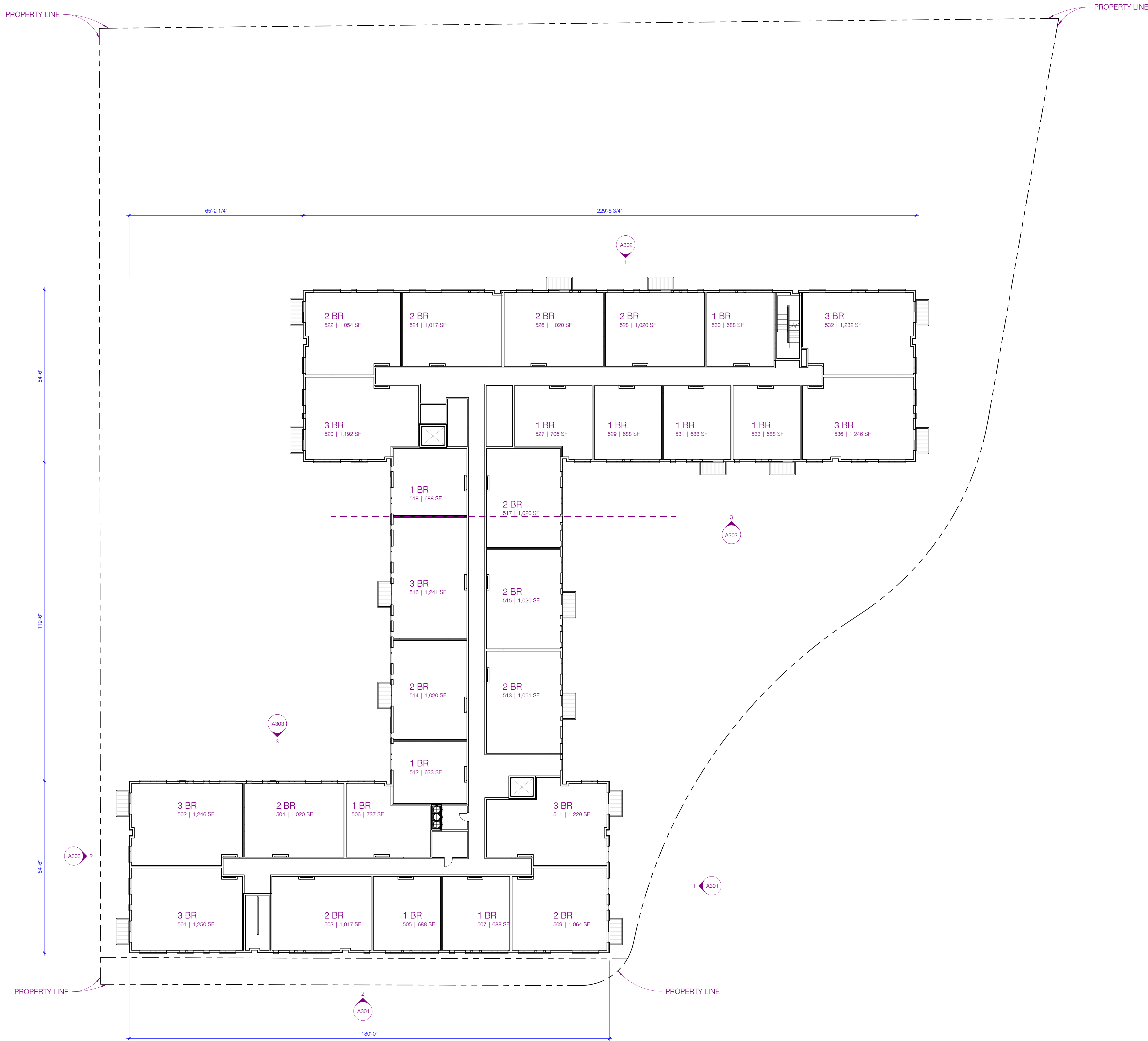
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LEVEL 4 PLAN - OVERALL

A104.0

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1 A105.0 - LEVEL 5 PLAN - OVERALL
[A105.0] 1/16" = 1'-0"

GENERAL PLAN NOTES:

1. DIMENSIONS AT EXTERIOR WALLS ARE TO OUTSIDE FACE OF SHEATHING
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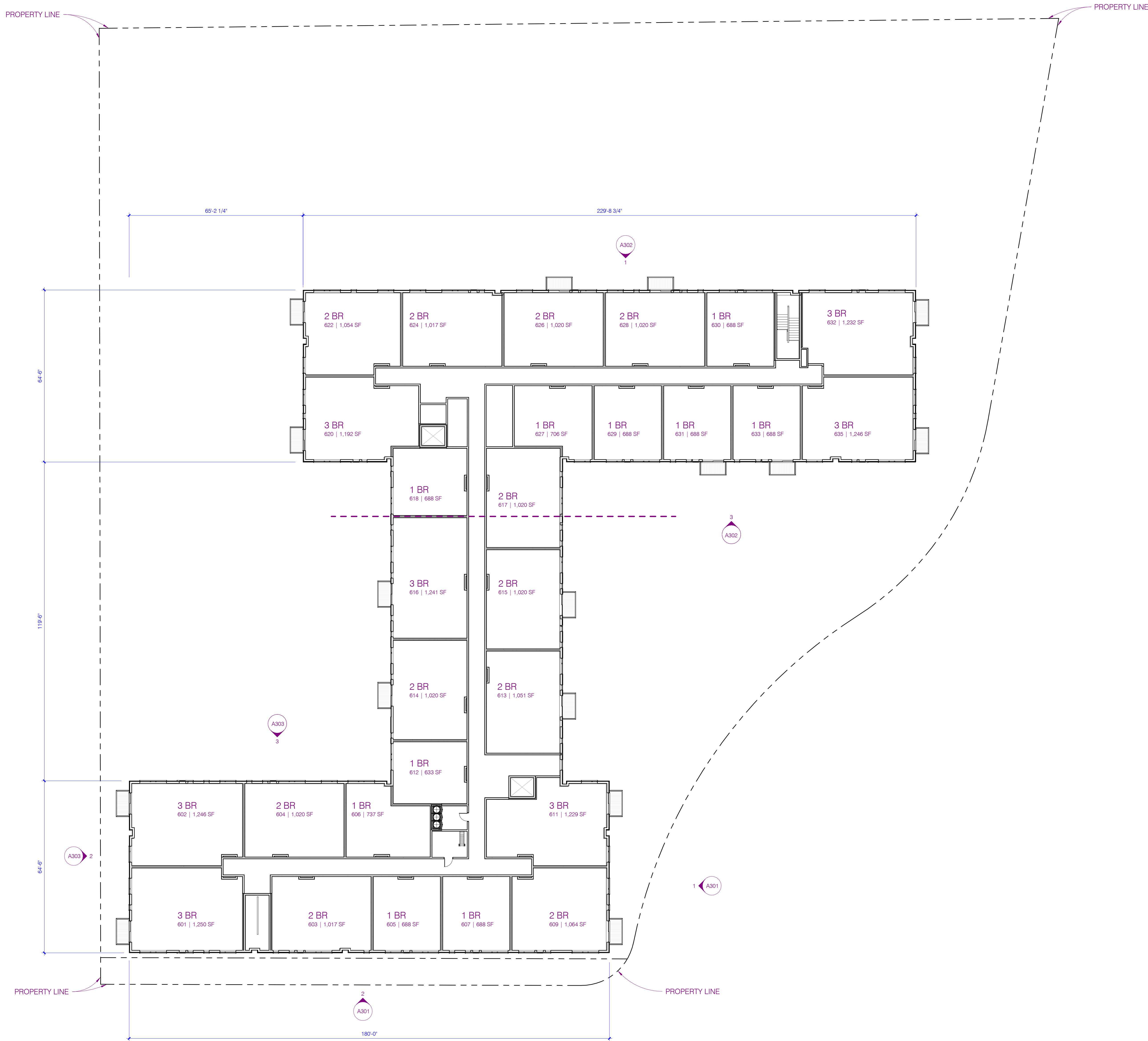
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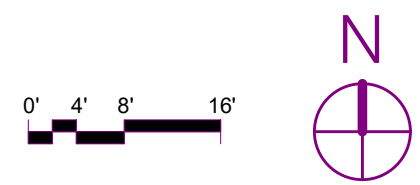
**LEVEL 5 PLAN -
OVERALL**

A105.0

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1 A106.0 - LEVEL 6 PLAN - OVERALL
A106.0 1/16" = 1'-0"



GENERAL PLAN NOTES:

- DIMENSIONS AT EXTERIOR WALLS ARE TO OUTSIDE FACE OF SHEATHING
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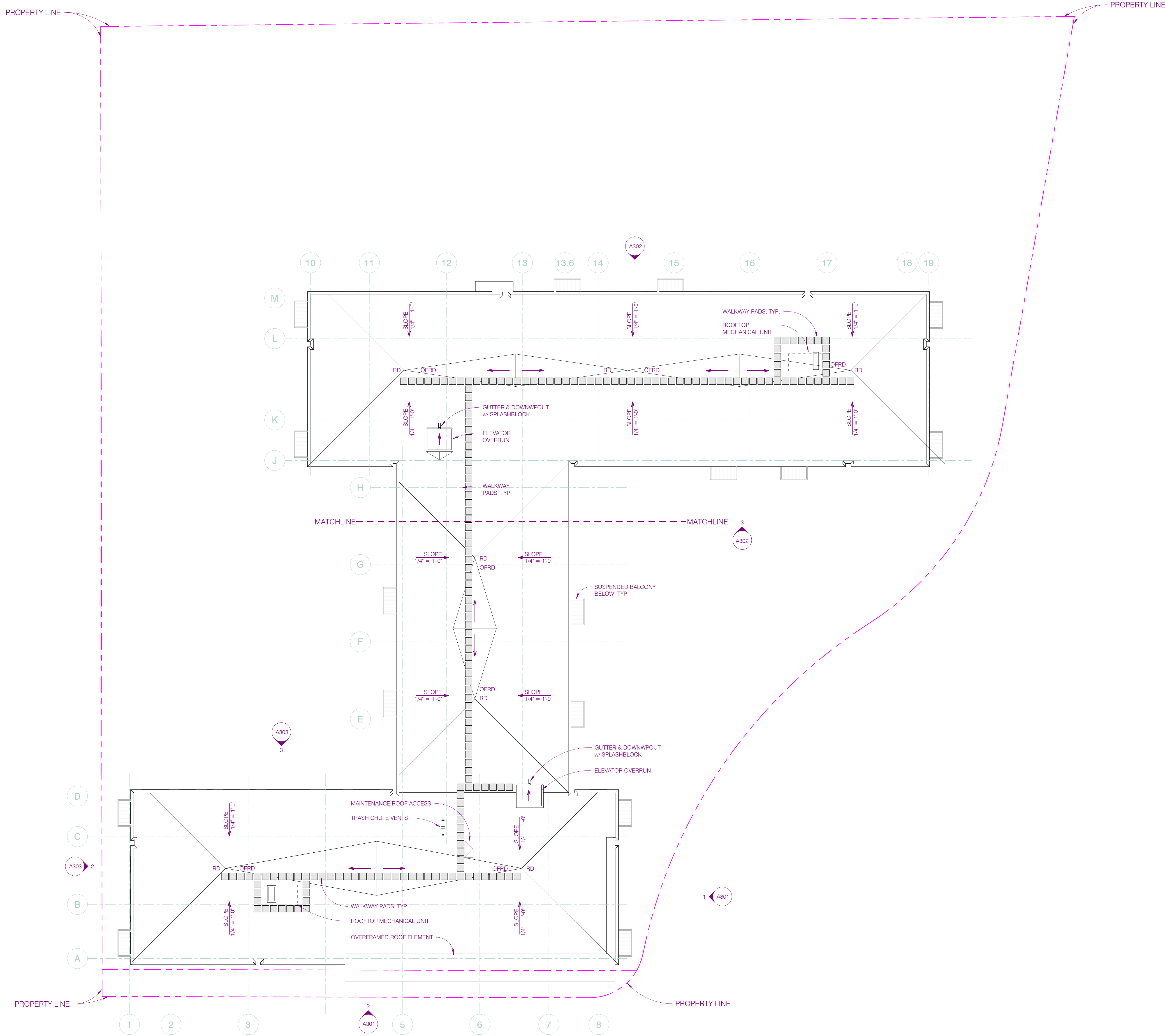
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**LEVEL 6 PLAN -
OVERALL**

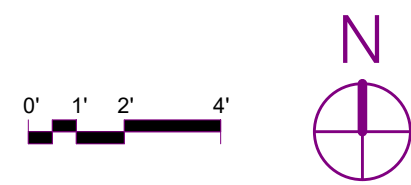
A106.0

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1 A201.0 - ROOF PLAN - OVERALL
A201.0 1/16" = 1'-0"



- GENERAL ROOF NOTES:
1. PROVIDE WALKING PADS FROM ROOF ACCESS POINT TO AND AROUND ALL MECHANICAL EQUIPMENT.
 2. GUARD RAILING REQUIRED AT ALL LOCATIONS WHERE ROOF TOP EQUIPMENT IS WITHIN 10'-0" FROM EDGE OF ROOF.
 3. PROVIDE WINDOW WASHING ANCHORS PER THE 2015 MINNESOTA BUILDING CODE SECTION 3112.
 4. ALL ROOF DRAINS TO DRAIN INTERNALLY THROUGH BUILDING TO STORM SEWER.
 5. ALL OVER FLOW ROOF DRAINS TO DAYLIGHT IN LANDSCAPE AREA.
 6. VERIFY FINAL WINDOW WASHING ANCHOR LAYOUT w/ ANCHOR SUPPLIER/MANUFACTURER.

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KEYNOTES:



2025 West River Rd

2025 W River Road, Minneapolis, MN 55411

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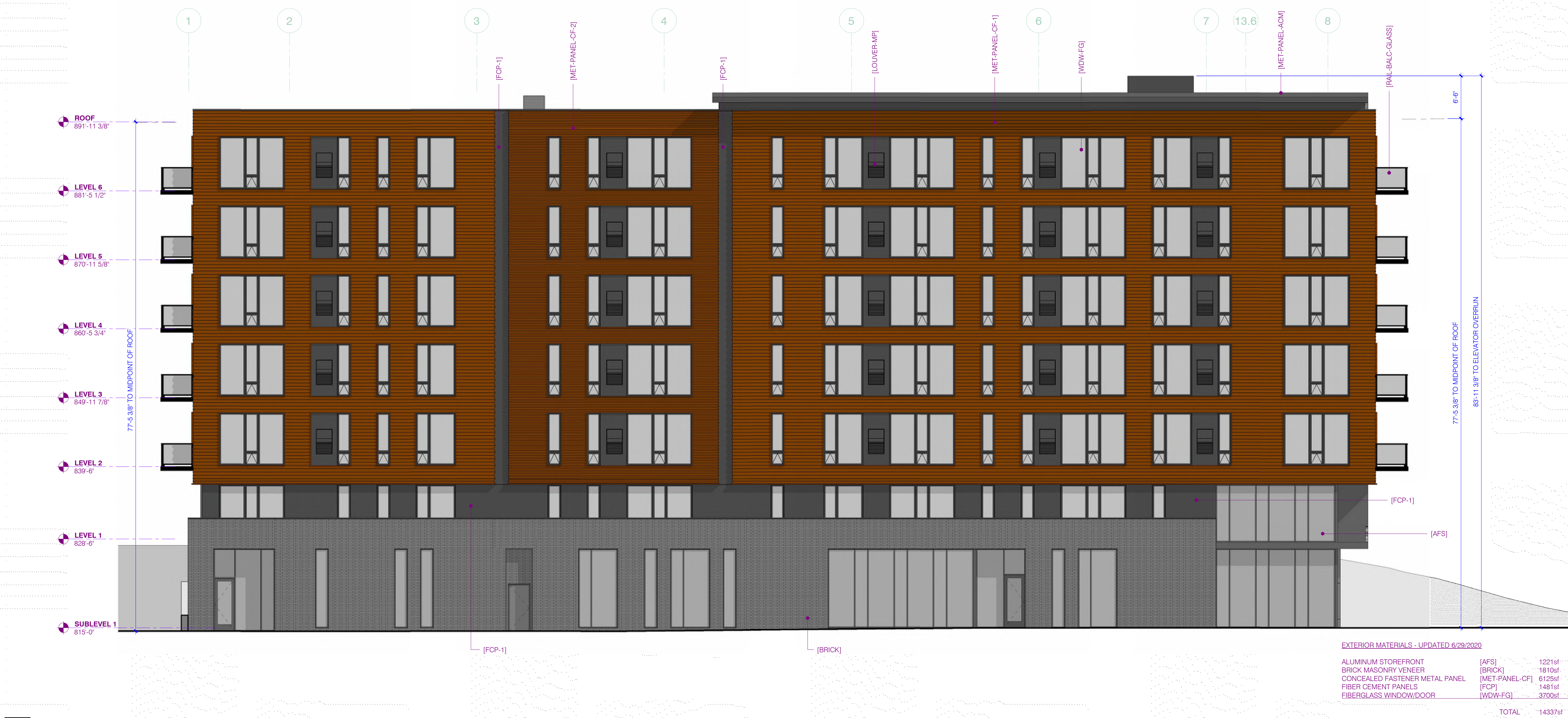
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ROOF PLAN - OVERALL

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2 EXTERIOR ELEVATION - SOUTH (BROADWAY)
A301 1/8" = 1'-0"

EXTERIOR MATERIALS - UPDATED 6/29/2020

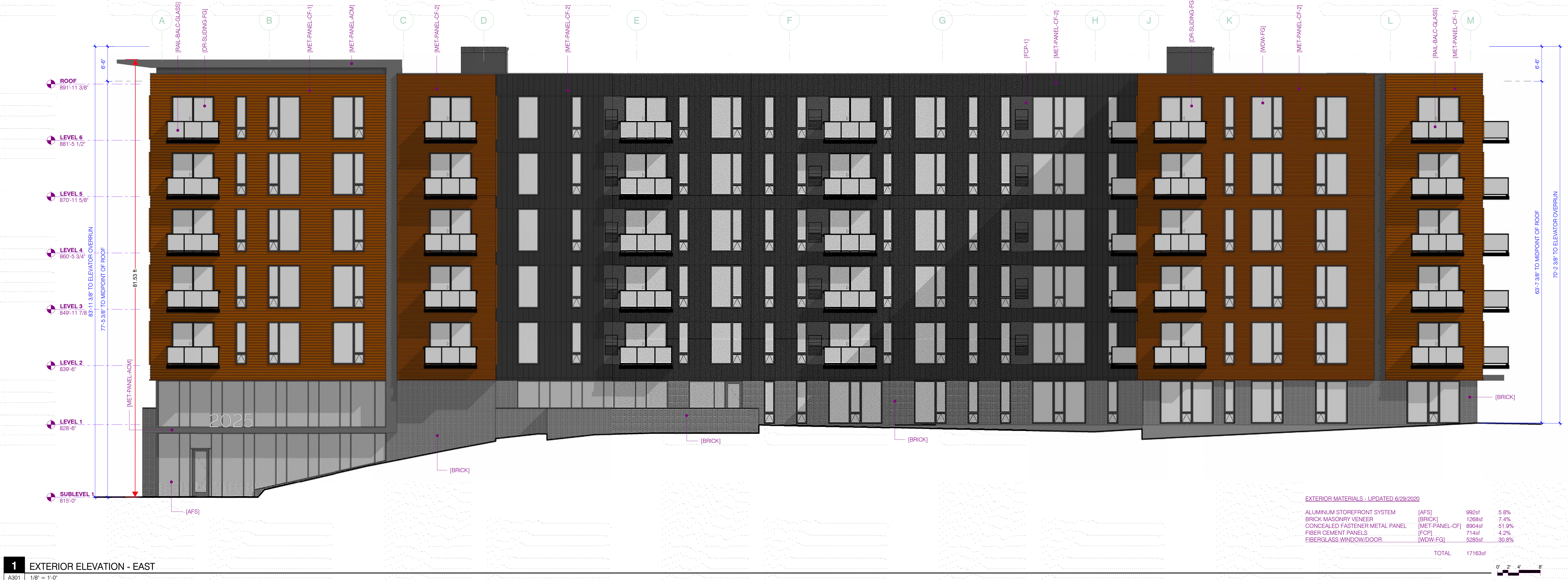
ALUMINUM STOREFRONT	[AFS]	1221sf	7.9%
BRICK MASONRY VENEER	[BRICK]	1810sf	12.6%
CONCEALED FASTENER METAL PANEL	[MET-PANEL-CF]	6125sf	42.9%
FIBER CEMENT PANELS	[FCP]	1481sf	8.9%
FIBERGLASS WINDOW/DOOR	[WDW-FG]	3700sf	27.7%
TOTAL		14337sf	

EXTERIOR NOTES:

1. REFER TO G005 AND G006 FOR ADDITIONAL EXTERIOR MATERIAL KEYNOTE INFORMATION
2. REFER TO SPECIFICATION FOR DETAILED EXTERIOR MATERIAL INFORMATION

KEYNOTES:

- [AFS] (08 40 00) ALUMINUM-FRAMED ENTRANCES, STOREFRONTS, AND CURTAIN WALLS
- [BRICK] (04 20 01) BRICK MASONRY VENEER
- [DR-SLIDING-FG] (08 16 73) FIBERGLASS SLIDING DOORS
- [FCP-1] (07 40 40) FIBER CEMENT PANELS, TYPE 1
- [LOUVER-MP] (23 80 00) LOUVER FOR PACKAGED THROUGH-THE-WALL HVAC UNIT, EXTRUDED ALUMINUM, CUSTOM COLOR SELECTED BY ARCHITECT
- [MET-PANEL-ACM] (07 42 13.23) METAL COMPOSITE MATERIAL WALL PANELS
- [MET-PANEL-CF-1] (07 42 13) CONCEALED FASTENER METAL WALL PANEL, TYPE 1
- [MET-PANEL-CF-2] (07 42 13) CONCEALED FASTENER METAL WALL PANEL, TYPE 2
- [RAIL-BALC-GLASS] (05 73 00) ALUMINUM BALCONY, DECK, AND GLASS RAILING
- [WDW-FG] (08 54 13) FIBERGLASS WINDOW, COLOR: BLACK



1 EXTERIOR ELEVATION - EAST
A301 1/8" = 1'-0"

EXTERIOR MATERIALS - UPDATED 6/29/2020

ALUMINUM STOREFRONT SYSTEM	[AFS]	992sf	5.8%
BRICK MASONRY VENEER	[BRICK]	1268sf	7.4%
CONCEALED FASTENER METAL PANEL	[MET-PANEL-CF]	8904sf	51.9%
FIBER CEMENT PANELS	[FCP]	714sf	4.2%
FIBERGLASS WINDOW/DOOR	[WDW-FG]	5285sf	30.8%
TOTAL		17163sf	

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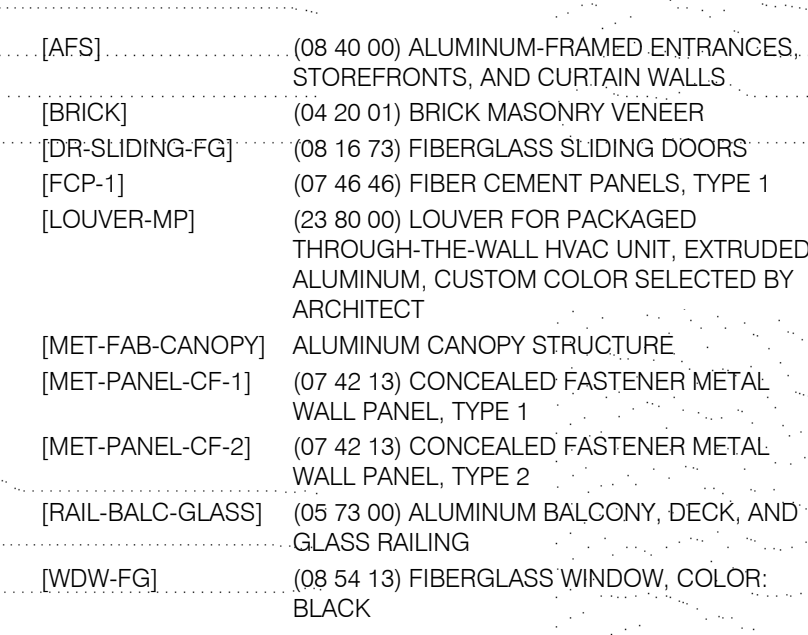
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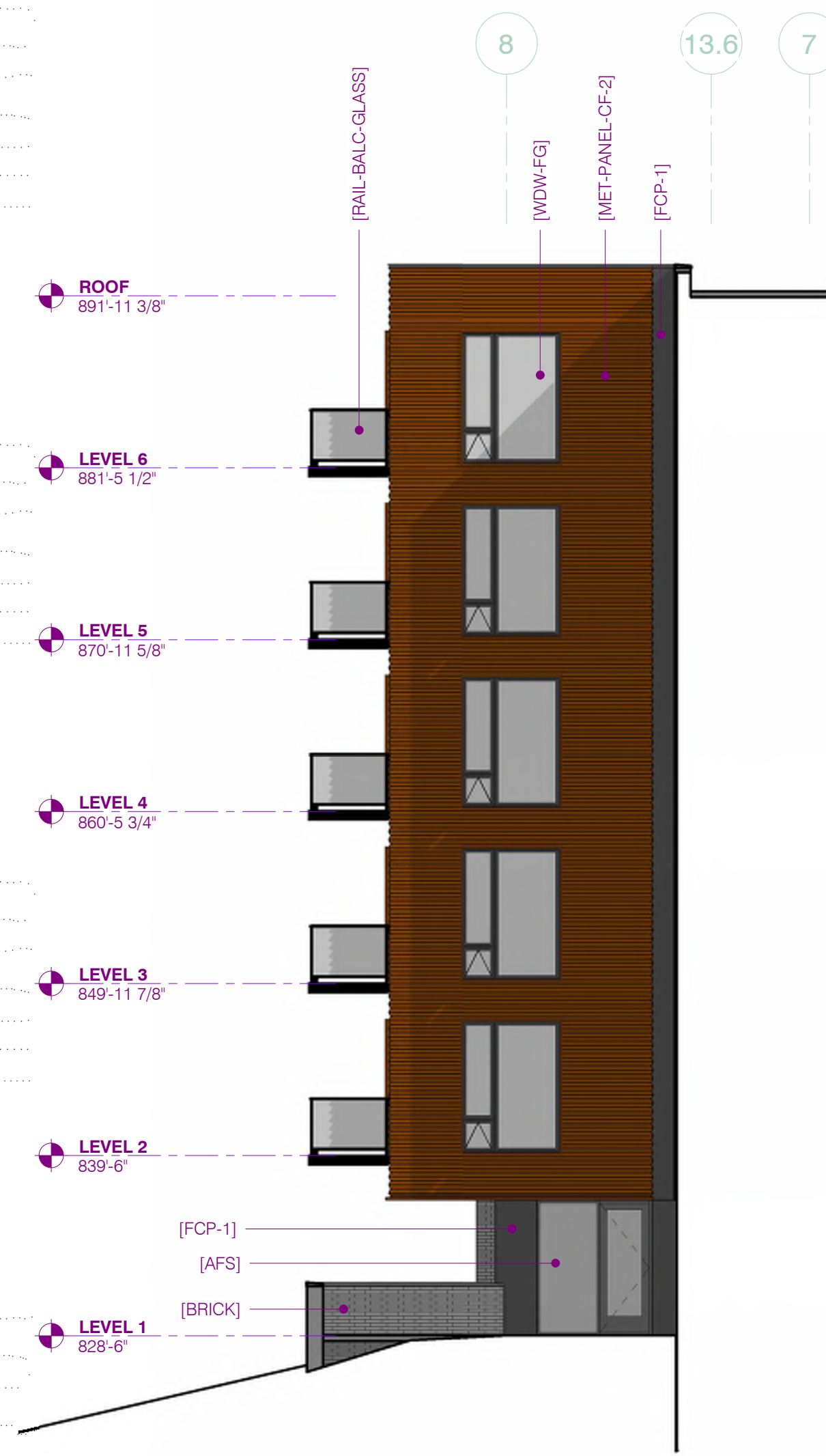
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BUILDING
ELEVATIONS



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EXTERIOR MATERIALS - UPDATED 6/29/2020

ALUMINUM STOREFRONT	[AFS]	58sf	5.1%
BRICK MASONRY VENEER	[BRICK]	50sf	4.4%
CONCEALED FASTENER METAL PANEL	[MET-PANEL-CF]	756sf	66.5%
FIBER CEMENT PANELS	[FCP]	32sf	2.9%
FIBERGLASS WINDOW/DOOR	[WDW-FG]	240sf	21.1%
TOTAL		1130sf	

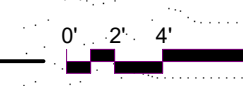
1 EXTERIOR ELEVATION - EAST COURTYARD - SOUTH SIDE
A303 1/8" = 1'-0"



EXTERIOR MATERIALS - UPDATED 6/29/2020

BRICK MASONRY VENEER	[BRICK]	373sf	5.5%
CONCEALED FASTENER METAL PANEL	[MET-PANEL-CF]	3173sf	47.2%
FIBER CEMENT PANELS	[FCP]	771sf	11.5%
FIBERGLASS WINDOW/DOOR	[WDW-FG]	2409sf	35.8%
TOTAL		6726sf	

3 EXTERIOR ELEVATION - WEST COURTYARD - SOUTH SIDE
A303 1/8" = 1'-0"



EXTERIOR MATERIALS - UPDATED 6/29/2020

ALUMINUM STOREFRONT	[AFS]	314sf	1.9%
BRICK MASONRY VENEER	[BRICK]	1067sf	7.8%
CONCEALED FASTENER METAL PANEL	[MET-PANEL-CF]	8552sf	52.2%
FIBER CEMENT PANELS	[FCP]	866sf	5.3%
FIBERGLASS WINDOW/DOOR	[WDW-FG]	5585sf	34.1%
TOTAL		16384sf	

2 EXTERIOR ELEVATION - WEST
A303 1/8" = 1'-0"

EXTERIOR NOTES:

- REFER TO G005 AND G006 FOR ADDITIONAL EXTERIOR MATERIAL KEYNOTE INFORMATION
- REFER TO SPECIFICATION FOR DETAILED EXTERIOR MATERIAL INFORMATION

KEYNOTES:

- [AFS] (08 40 00) ALUMINUM-FRAMED ENTRANCES, STOREFRONTS, AND CURTAIN WALLS
[BRICK] (04 20 01) BRICK MASONRY VENEER
[CONC-CAST] (03 30 00) CAST-IN-PLACE CONCRETE
[DR-SLIDING-FG] (08 16 73) FIBERGLASS SLIDING DOORS
[FCP-1] (07 46 46) FIBER CEMENT PANELS, TYPE 1
[MET-PANEL-CF-1] (07 42 13) CONCEALED FASTENER METAL WALL PANEL, TYPE 1
[MET-PANEL-CF-2] (07 42 13) CONCEALED FASTENER METAL WALL PANEL, TYPE 2
[RAIL-BALC-GLASS] (05 73 00) ALUMINUM BALCONY, DECK, AND GLASS RAILING
[WDW-FG] (08 54 13) FIBERGLASS WINDOW, COLOR: BLACK

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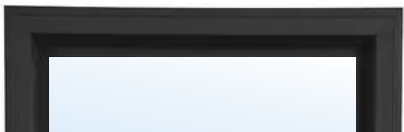
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BUILDING ELEVATIONS

A303

2025 W RIVER RD



A. [WDW-FG] FIBERGLASS WINDOW, BLACK



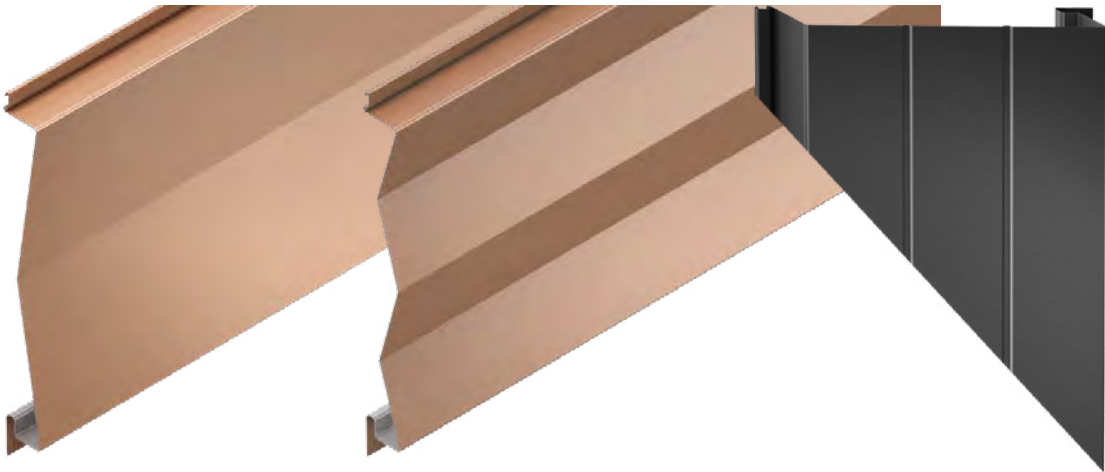
B. [AFS] ALUMINUM FRAMED STOREFRONT, BLACK ANODIZED



C. [MET-PANEL-ACM] METAL COMPOSITE MATERIAL WALL PANEL, BLACK; PREFINISHED METAL FLASHING AND LOUVERS



E. BRICK MASONRY VENEER



F. [MET-PANEL-CF-1] CONCEALED FASTENER METAL WALL PANEL, PROFILE 1

G. [MET-PANEL-CF-2] CONCEALED FASTENER METAL WALL PANEL, PROFILE 2

H. [MET-PANEL-CF-3] CONCEALED FASTENER METAL WALL PANEL, PROFILE 3



4 A351 - AXON FROM NW
A351



3 A351 - AXON FROM SW
A351



2 A351 - AXON FROM NE
A351



1 A351 - AXON FROM SE
A351

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BUILDING
AXONOMETRIC
VIEW

A351



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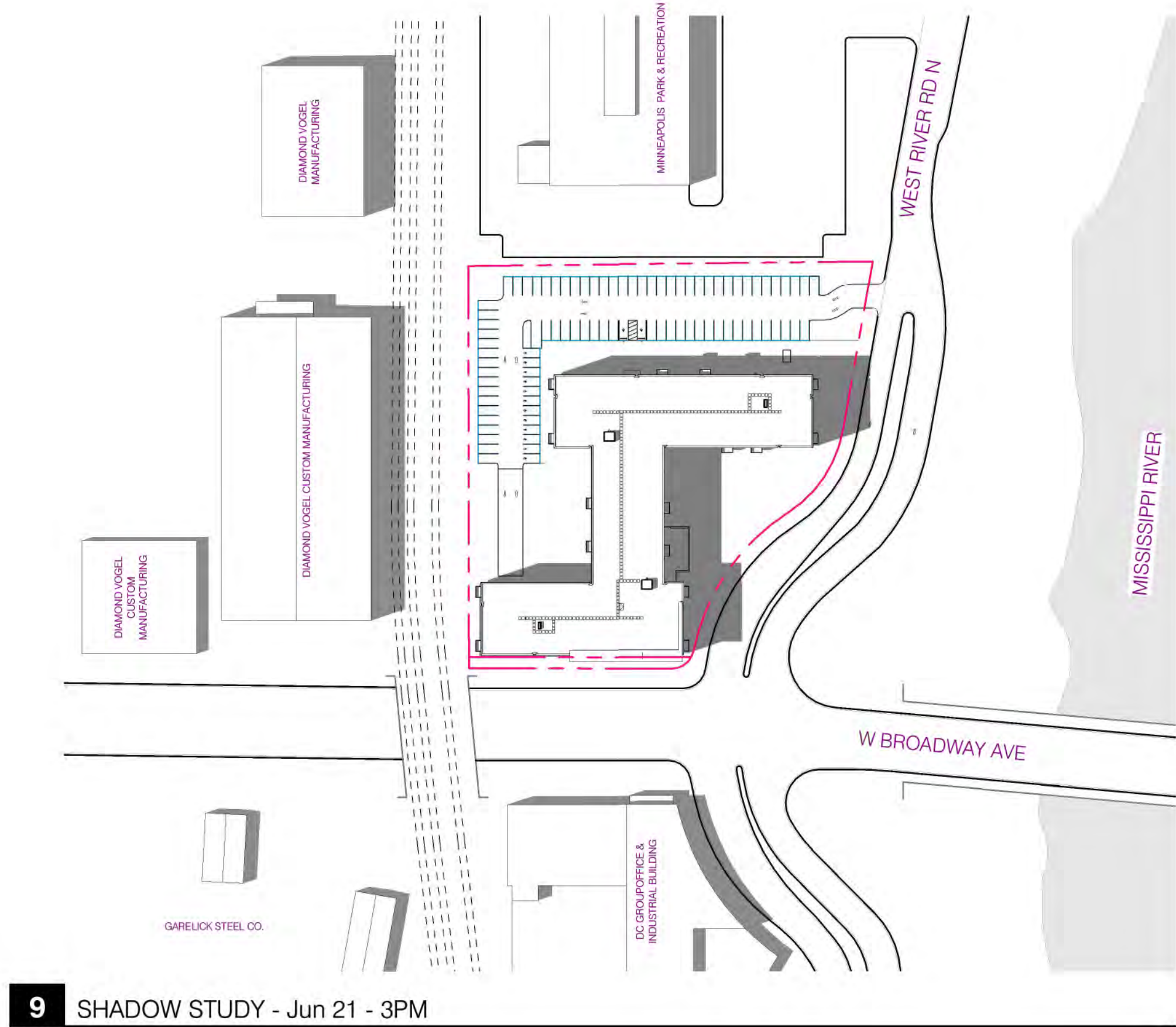
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DATE 7/1/2020
PROJECT # 19-0035
PHASE LAND USE & PDR
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CHECKED BY DPPL

BUILDING
PERSPECTIVE
VIEWS

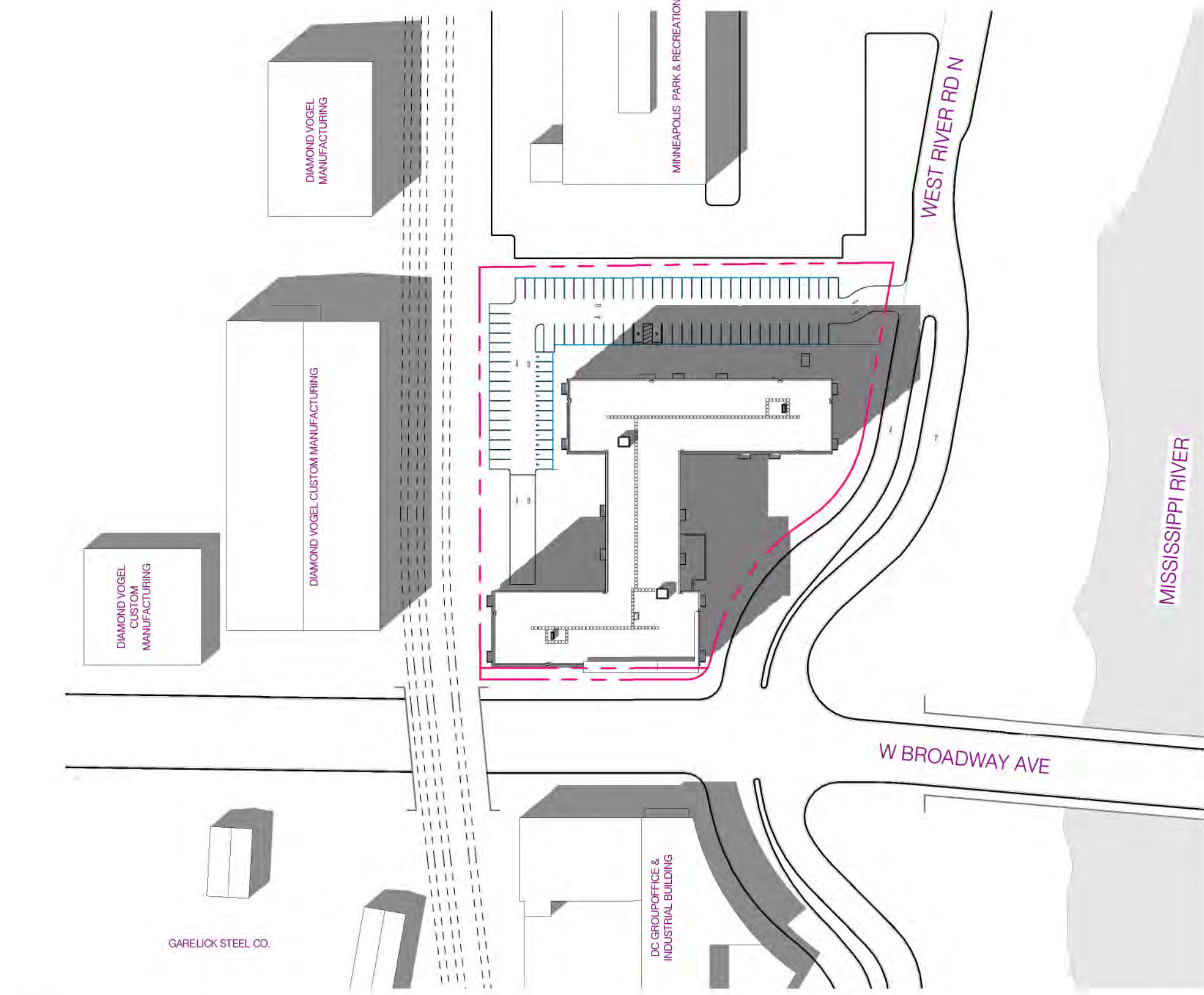
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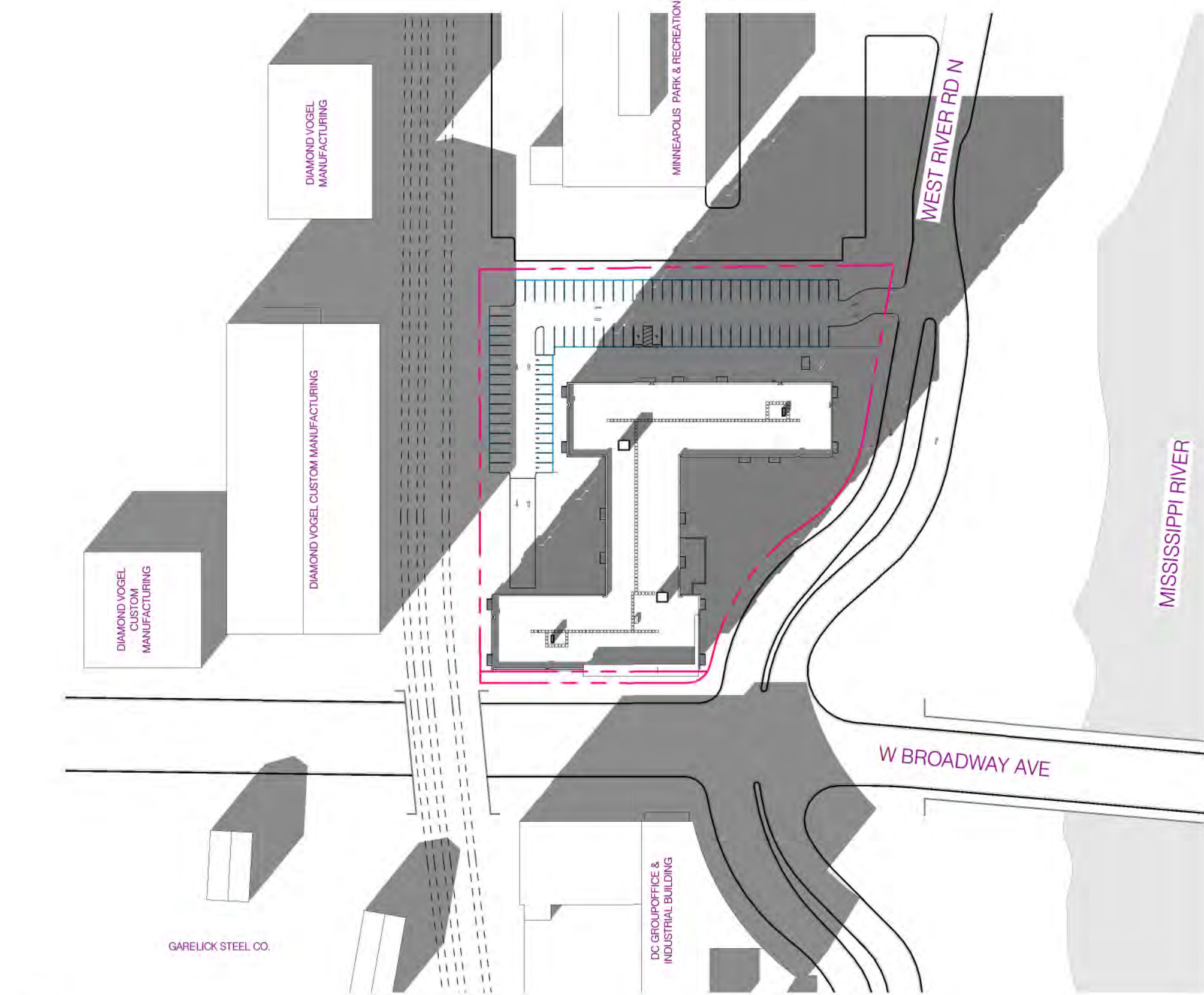
9 SHADOW STUDY - Jun 21 - 3PM

AL102 1" = 100'-0"



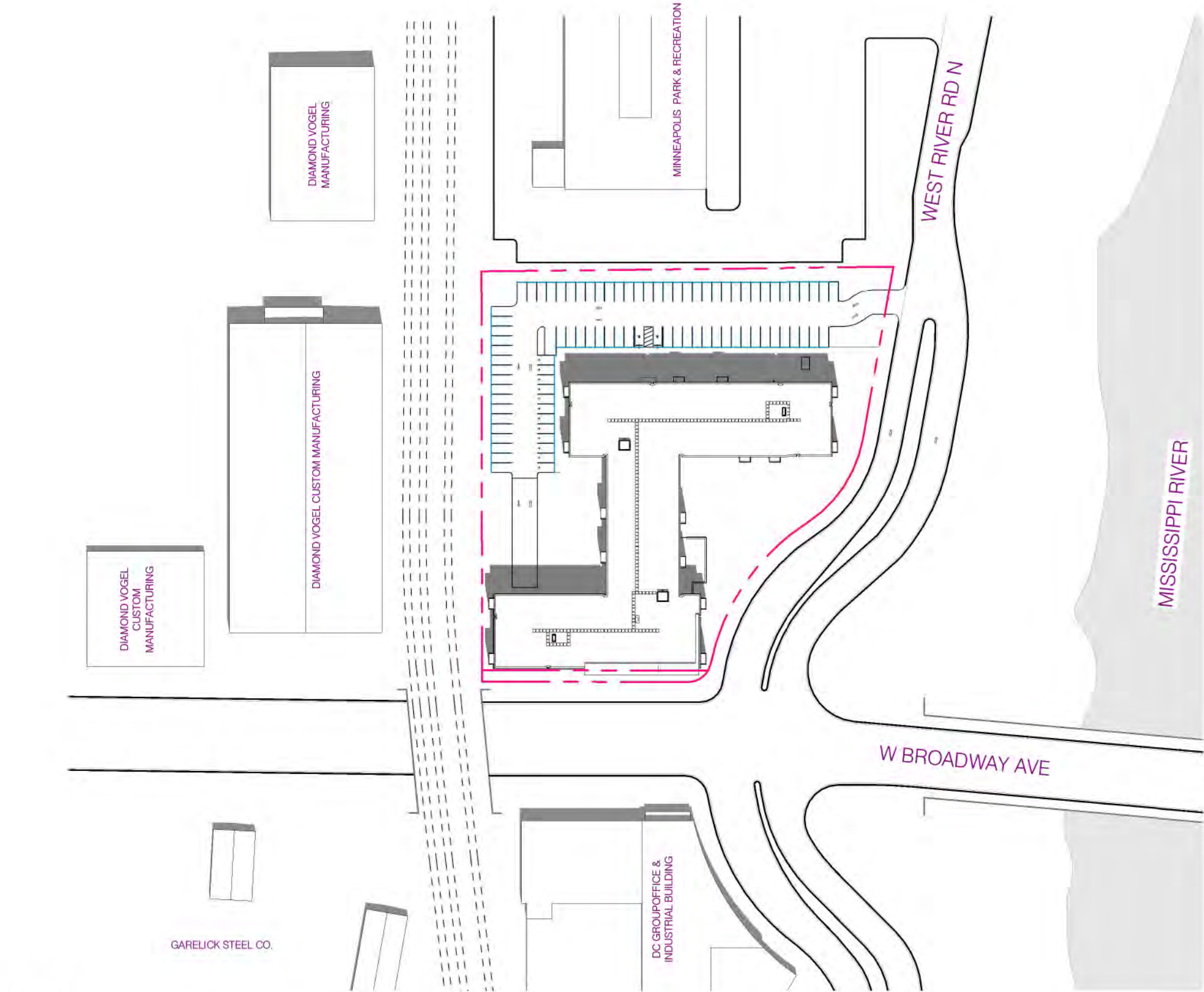
6 SHADOW STUDY - Mar/Sept 21 - 3PM

AL102 1" = 100'-0"



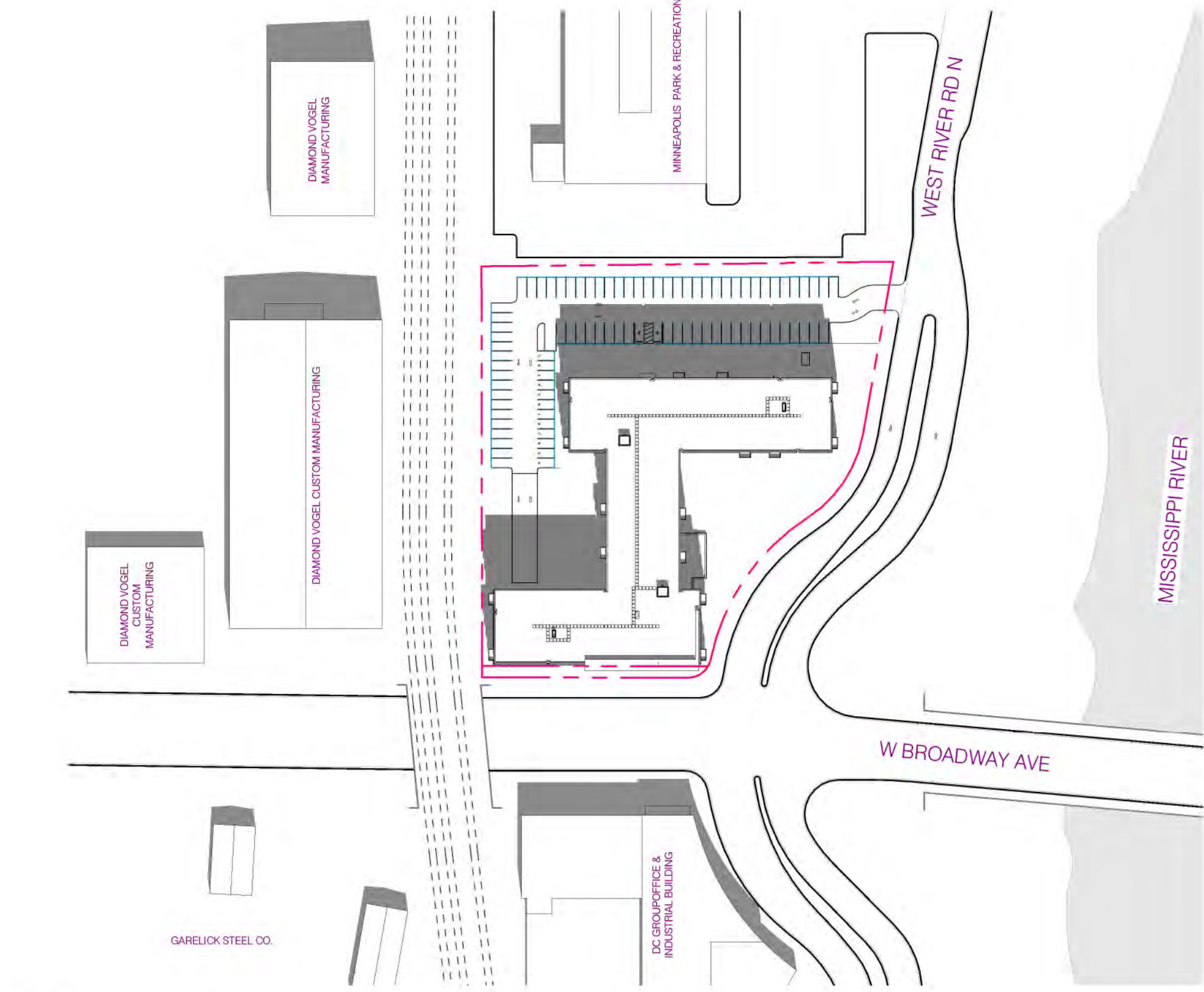
3 SHADOW STUDY - Dec 21 - 3PM

AL102 1" = 100'-0"



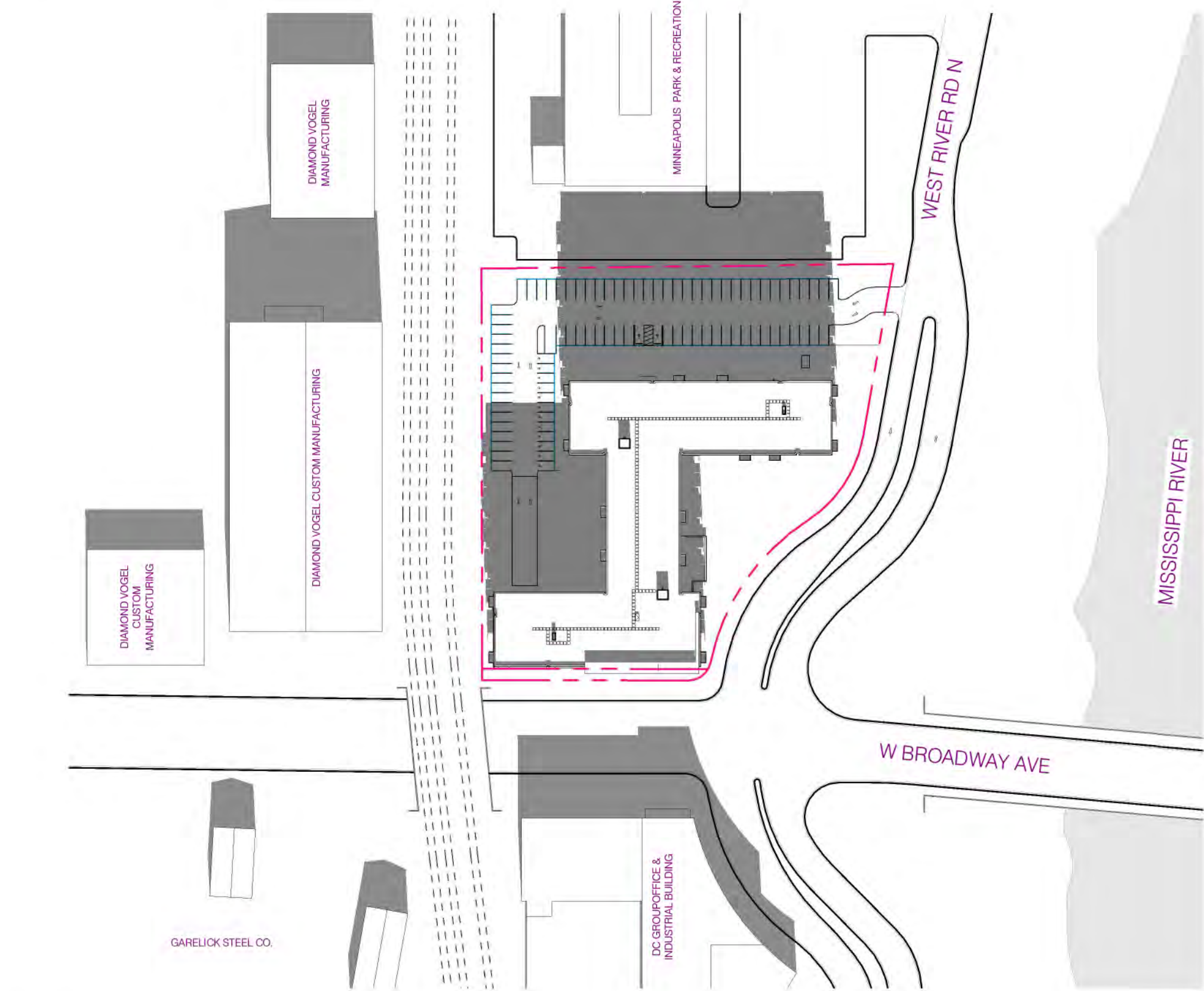
8 SHADOW STUDY - Jun 21 - 12PM

AL102 1" = 100'-0"



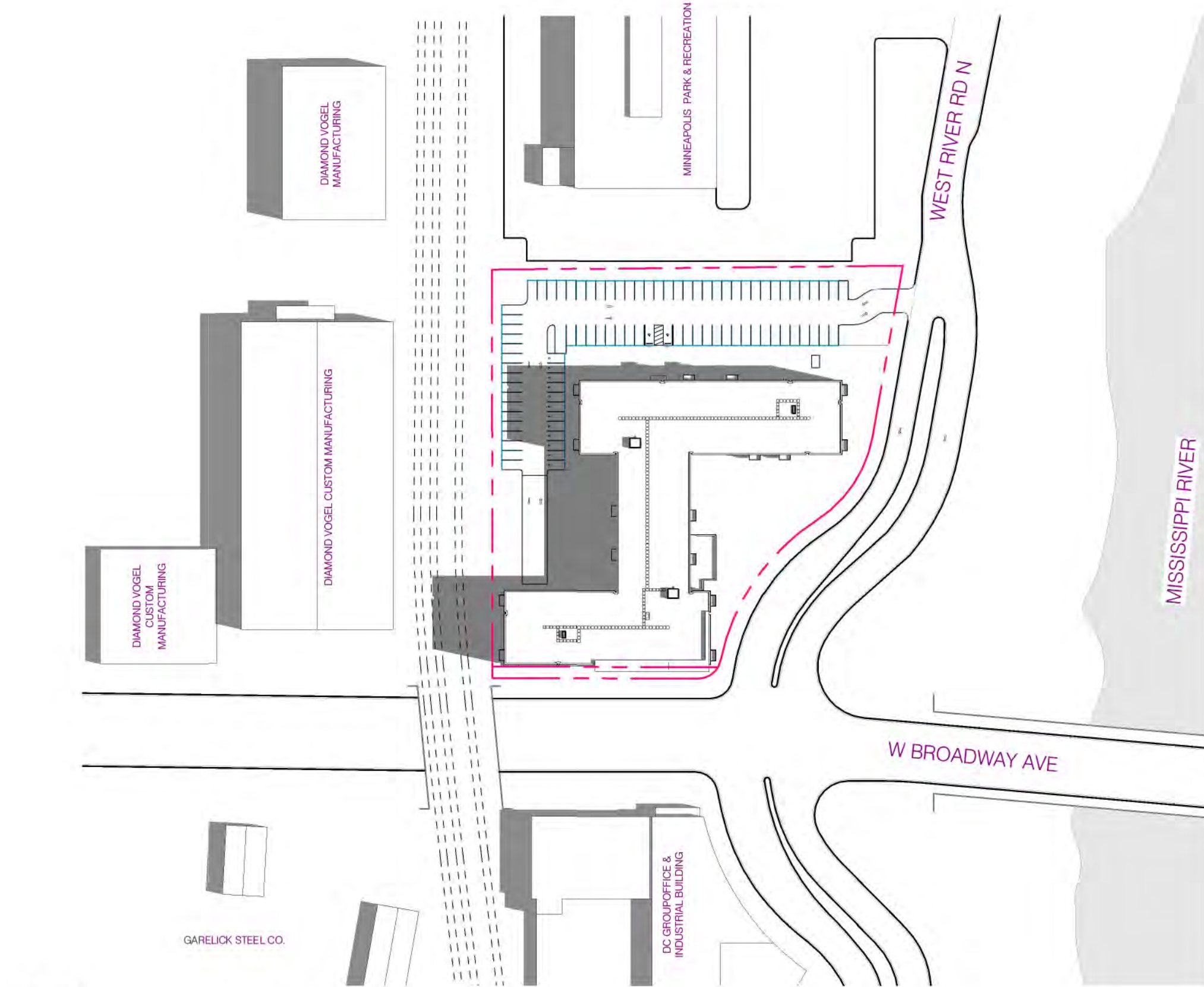
5 SHADOW STUDY - Mar/Sept 21 - 12PM

AL102 1" = 100'-0"



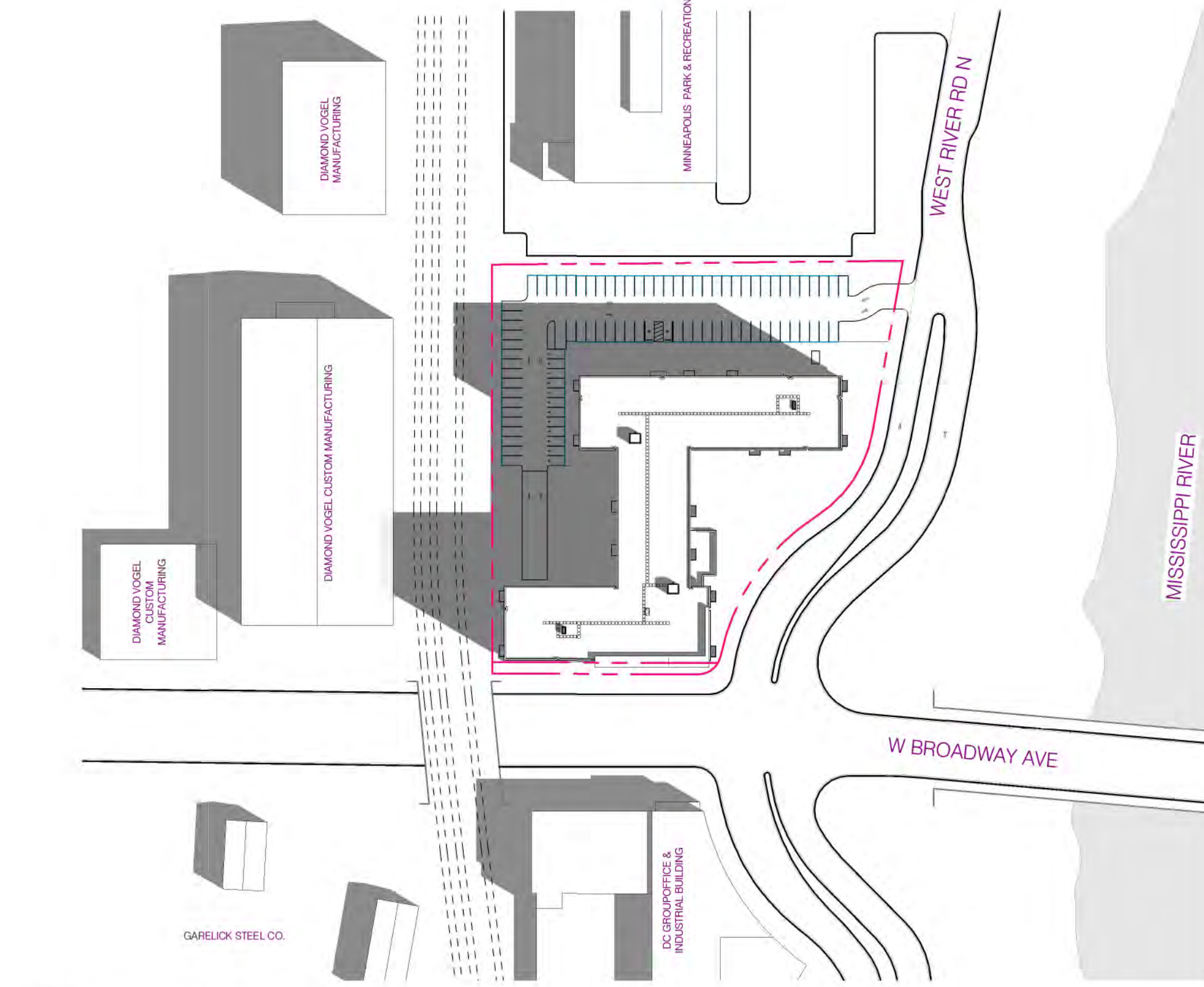
2 SHADOW STUDY - Dec 21 - 12PM

AL102 1" = 100'-0"



7 SHADOW STUDY - Jun 21 - 9AM

AL102 1" = 100'-0"



4 SHADOW STUDY - Mar/Sept 21 - 9AM

AL102 1" = 100'-0"



1 SHADOW STUDY - Dec 21 - 9AM

AL102 1" = 100'-0"

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PHASE LAND USE & POR
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SHADOW STUDY &
CONTEXT MAP

AL102

TRAVEL DEMAND MANAGEMENT PLAN

2025 West River Road
Minneapolis, MN

By _____ Dated: _____
Schafer Richardson
c/o Dominik Jensen
900 North Third Street
Minneapolis, MN 55401

MINNEAPOLIS COMMUNITY PLANNING AND ECONOMIC DEVELOPMENT DEPARTMENT:

By _____ Dated: _____
Steve Poor, CPED Development Services Director

MINNEAPOLIS PUBLIC WORKS DEPARTMENT:

By _____ Dated: _____
Steve Mosing, P.E., P.T.O.E., Traffic Operations Engineer

PREPARED BY:



I hereby certify that this report was prepared by me
or under my direct supervision, and that I am a duly
Licensed Professional Engineer under the laws of the
State of Minnesota.

By: _____
Jonah Finkelstein, PE, RSP
License No. 56120

Dated: 05/19/2020

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1. Introduction

a. Proposed Development

A six-story multi-family residential development plan is proposed at 2025 West River Road in Minneapolis, Minnesota. The proposed development includes 163 dwelling units and 163 parking stalls, split between a surface lot and a structured garage.

The purpose of this report is to address the transportation impacts of the completion of the development on air quality, parking and roadway infrastructure. The report will satisfy the City of Minneapolis' requirements for the development to have a Travel Demand Management Plan in place.

For those not familiar with the general concepts and terms associated with traffic engineering, *The Language of Traffic Engineering* guide is included in the Appendix

b. Study Objectives Summary

The study objectives are:

- i. Document existing conditions surrounding the development site.
- ii. Document the development's expected traffic generation.
- iii. Determine how the study intersections and roadways will operate in the future with and without the proposed development.
- iv. Recommend appropriate mitigation measures if poor operations are identified.
- v. Determine the vehicle parking demands of the development.
- vi. Determine bicycle parking demands of the development.
- vii. Develop strategies to encourage non-single occupancy vehicle modes of transportation with the goal of attaining at least 30% of all site trips being made by transit/pedestrian/bicycle.

For this traffic study, the study intersections closest to the proposed development and where the greatest impact is expected were chosen for review and include:

- W Broadway Avenue/West River Road
- West River Road/Site Access
- West River Road/22nd Avenue
- W Broadway Avenue/2nd Street
- W Broadway Avenue/Washington Avenue

The roadways corridors studied in this document include those surrounding the proposed site, which are:

- W Broadway Avenue
- West River Road
- N 22nd Avenue
- N Washington Avenue

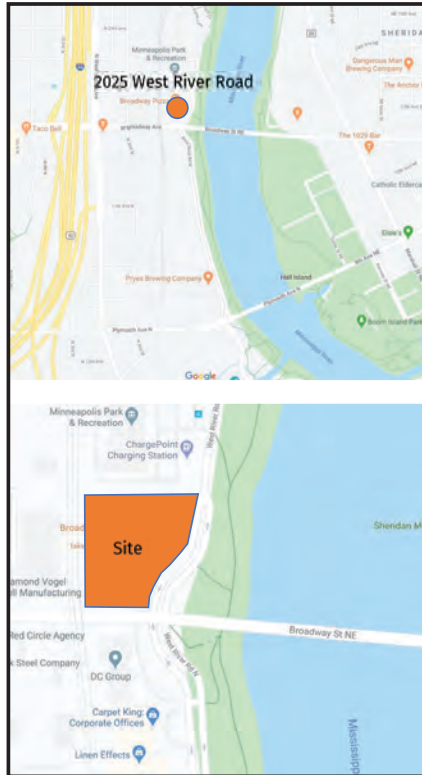
It should be noted that traffic expected from the proposed development will have minor impacts on other corridors and intersections beyond those studied here. Furthermore, this study does not account for the existing roadway conditions such as pavement quality or appropriate drainage.

2. Development Site

a. Site Location

The proposed development is located at 2025 West River Road on the northwest corner of the W Broadway Avenue/West River Road intersection in Minneapolis, MN. Figure 1 shows the development location.

Figure 1 – Project Location



b. Existing Site

The site is currently zoned C3A (Community Activity Center District) and is in the Mississippi River Critical Area and Pedestrian Oriented overlay district.

The existing single-story restaurant on site will be removed as part of this development. There is an existing surface lot at the site that will also be removed. This surface lot currently has access from West River Road.

c. Proposed Development

The proposed development includes 163 dwelling units.

Off-street parking will be provided with 163 total vehicle parking stalls. 74 stalls will be inside the structured parking facility with 89 stalls located in the surface parking lot.

There will be one vehicle access point to West River Road, located just north of the existing access on West River Road. This access is roughly 225 feet south of the Minneapolis Park & Recreation Board's Building access and 370 feet north of W Broadway Avenue/West River Road.

One bicycle storage room will be located within the parking garage.

A concept site plan is in the Appendix.

3. Existing Conditions

a. Corridor Characteristics

Table 1 shows the characteristics of the key roadway corridors around this site and within the study area.

Table 1 – Study Corridor Characteristics

Name	Designation ¹	Classification ²	Speed Limit	Lanes	Transit ³	Peds/ Bicycles
W Broadway Avenue	CSAH 66	A-Minor Augmentor	30 mph	4 undivided	1 bus route	Sidewalks both sides
West River Road	-	Local Road	30 mph	2 divided	None	Trail east sides
N 22 nd Avenue	-	Local Road	30 mph	2 undivided	None	Sidewalks southside
N Washington Avenue	CSAH 152	A-Minor Reliever South of Broadway, Major Collector North of Broadway	30 mph	4 divided	1 bus route	Sidewalks both sides

¹ CSAH = County State Aid Highway.

² Source: Minneapolis 2040 Comprehensive Plan.

³ Number of routes around the site.

On-street parking is currently provided on the east side of West River Road north of the site as well as on both sides of West River Road south of Broadway Avenue. On-street parking is also provided on both sides of N 22nd Avenue as well as on the east side of Washington Avenue north of Broadway Avenue.

b. Intersection Characteristics

The W Broadway Avenue/West River Road intersection is under signalized control. Both eastbound and westbound approaches operate with shared through/left and through/right lanes. The northbound approach provides a roughly 30-foot cross-section without lane markings. This causes some confusion in lane operations. Based on previous experience at the intersection, and the volumes present on each turning movement, this approach was assumed to operate with a shared through/left turn lane and dedicated right turn lane. The southbound approach provides dedicated turn lanes and a single through lane.

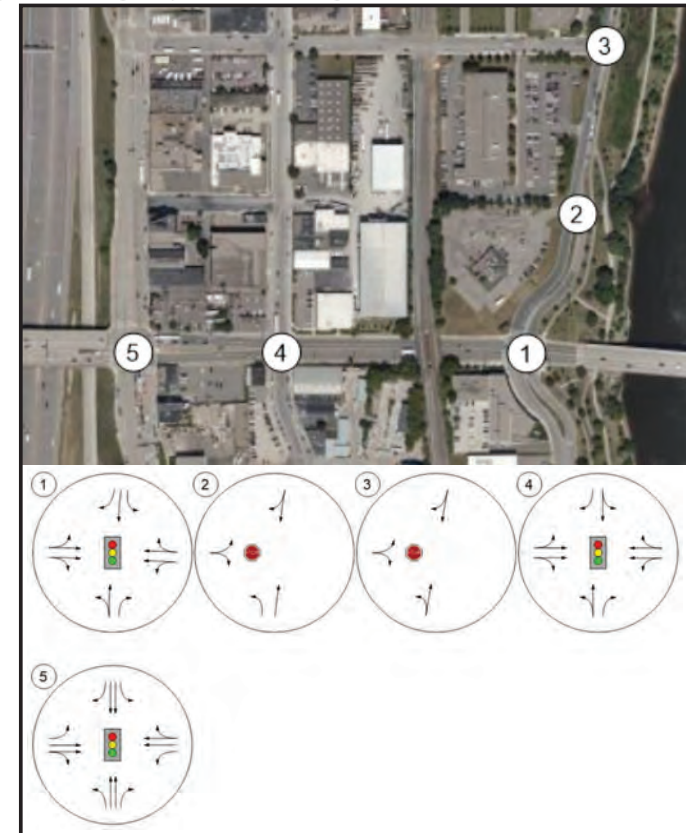
Broadway Avenue/2nd Street is under signalized control with shared through/left and through/right lanes along W Broadway avenue and shared through/left and dedicated right turn lanes along 2nd Street.

Broadway Avenue/Washington Avenue is under signalized control with dedicated right turn and left turn lanes along Washington Avenue and dedicated left turn lanes along Broadway Avenue.

The West River Road/Site Access and West River Road/22nd Avenue intersection are under side street stop control with West River Road remaining under free-flowing conditions.

The existing lane configurations and traffic control for the four existing intersections can be seen in Figure 2.

Figure 2 – Existing Study Intersection Configurations



c. Transit

This site has some nearby public transportation with bus stops at the intersection of W Broadway Avenue/N 2nd Street and south of N Washington Avenue/W Broadway Avenue. These locations provide service on Metro Transit Route 14 and 30. Details on these routes are:

- Route 4 – Robbinsdale to Richfield. Service every 15 to 20 minutes during the weekday peak periods.
- Route 30 – Minneapolis to Saint Paul. Service every 30 minutes during the weekday peak periods.

Walkscore.com, which gives ratings to addresses based on transit, bicycle, and pedestrian access, gives 2025 West River Road a score of 49/100 for transit.

d. Pedestrians/Bicycles

Sidewalks are available on both sides W Broadway Avenue, N 2nd Street, and N Washington Avenue; West River Road has a trail on the east side of the corridor; and N 22nd Avenue has a sidewalk on the south side of the street. With the large number of sidewalks in the area and the density of the surrounding area, there is good pedestrian access to the site. Walkscore.com gives 2025 West River Road a score of 56/100 for pedestrian access. This lower score is impacted to the proximity of walkable destinations and not the infrastructure available for walking.

The proposed development site is located directly next to the northern end of the West River Road trail system. This separated trail provides connectivity into downtown Minneapolis and Saint Paul. This trail provides access to various other bicycle facilities throughout its length. Walkscore.com gives the proposed site a 83/100 for the bicycle connectivity score.

e. Traffic Volumes

Due to impacts of the COVID-19 pandemic, and after discussions with the City, existing turning movement counts were not collected. This was decided due to the steep decrease in traffic due to the 'Stay-at-home' executive order. With businesses and schools closed and a majority of employees working from home, not only would traffic volumes be low, but the distribution of turning movement counts is likely to also vary from what would typically be expected.

Instead, previously collected turning movements counts and balancing between intersections was completed. The following data was used at each intersection:

- W Broadway Avenue/West River Road – TMC from 2011
- West River Road/Site Access – No TMC available
- West River Road/N 22nd Avenue – No TMC available
- W Broadway Avenue/N 2nd Street – TMC from 2013
- W Broadway Avenue/N Washington Avenue – TMC from 2013

Using a growth rate of 0.25% per year these turning movement counts were factored to the year 2020. The 0.25% increase was determined in discussion with the City. Review of available roadway volumes within the project area was also completed. These ADT counts showed a general decrease in roadway volumes from 2011 to the present. This suggests the 0.25% a year increase reflects a conservatively high estimate for the existing roadway volumes. Also, the turning movement counts on Broadway Avenue collected in 2013 were roughly 55% lower than the adjacent counts completed in 2011. This peaking condition is also observed in the ADT counts in the area suggesting external factors impacted and increased roadway volumes over this time. To account for this peak the turning movement counts from W Broadway Avenue/West River Road (2011) were balanced downward to more closely match the volumes at the surrounding intersections (2013) for the a.m. and p.m. peak periods.

The turning movements at West River Road/Site access were determined using the estimated existing condition entering and exiting volumes at W Broadway Avenue/West River Road, and a trip generation completed for the existing restaurant.

Finally, as a low percentage of vehicles are expected to travel north on West River Road to N 22nd Avenue, turning movements were not forecast here, instead the expected change in turning movement volumes due to site related traffic will be shown. This will help us determine if any impacts are expected at the intersection due to site related traffic and if additional analysis is needed once traffic patterns return to normal and data can be collected.

The current daily traffic volumes on roadways surrounding the site are based on the most recently collected ADT counts available through the City of Minneapolis. These counts, which were collected in 2017, were then factored to the 2020 scenario by applying the 0.25% per year increase to the 2017 ADT volumes. Figure 3 presents these estimated daily volumes.

Figure 3 – Existing Daily Traffic Volumes



4. Forecasted Traffic

a. Site Traffic Forecasting

A trip generation analysis was performed for the development site based on the methods published in the *Institute of Transportation Engineers (ITE) Trip Generation Manual, 10th Edition*. Trip generation rates are also based on local data collected by Spack Solutions.

The ITE manual compiles studies from across the country to provide a national average traffic for various land uses. Spack Solutions collects current average traffic volumes for various land uses in the Twin Cities regional area for use in our studies. Local data is considered more relevant than the ITE national data as it is generally newer and accounts for our area's specific characteristics and driving habits. Per the procedure in the *Trip Generation Manual*, local trip generation data is used when possible and supplemented with national ITE data when local data is not available.

The new trips generated by the proposed development are shown in Table 2. A detailed trip generation table showing the exact breakdowns is provided in the Appendix.

Table 2 – New Trip Generation

Land Use Code – Source ¹	Description & Size	Daily		AM Peak Hour		PM Peak Hour	
		In	Out	In	Out	In	Out
221 - ITE	Multi-Family Housing (mid rise) (163 units)	443	443	15	43	44	28
Local	Apartment (163 units)	358	358	8	43	47	23

¹ Local = Trip generation data collected by Spack Solutions.

As shown in Table 2, the locally collected trip generation is lower than that calculated by ITE, though the a.m. and p.m. peak periods do have relatively similar results. As ITE recommends using local data when available, the Spack Solutions collected rates were used for our analysis.

A trip distribution pattern was developed for the generated traffic going to and from the proposed development. This pattern is based on the existing traffic volumes, site access, and access to the regional transportation system. The general trip distribution pattern for this study is:

- 20% of the generated traffic to/from the west on Broadway Avenue.
- 20% of the generated traffic to/from the north on Washington Avenue, N 2nd Street, and West River Road.
- 25% of the generated traffic to/from the east on Broadway Avenue.

- iv. 35% percent of the generated traffic to/from the south on Washington Avenue, N 2nd Street, and West River Road.

Traffic generated by the site development was assigned to the area roadways per this distribution pattern.

b. Non-site Traffic Forecasting

The impacts of the proposed development are studied in the year 2022. To forecast future traffic volumes in the study area outside of the proposed development's traffic, a general growth in traffic are considered.

Looking at historic traffic volumes in the study area in the last decade, most daily traffic volumes have been decreasing with the only exception being West River Road south of W Broadway Avenue, which has seen growth rates fluctuation between increasing and decreasing. Based on this information, and discussion with the City, the 0.25% per year growth rate was again applied to the 2020 estimated volumes to create the 2022 no-build scenario volumes.

Applying this background growth rate to the existing traffic volumes established the 2022 No-Build forecasts. The estimated trips for the existing restaurant were left in for the No-Build Scenario.

c. Total Traffic

Traffic forecasts were developed for the 2022 Build scenarios by adding the traffic generated by the proposed development to the non-site forecast volumes. The existing restaurant related trips were removed from the study intersections and roadways for this analysis.

The average daily traffic volume forecasts along the study corridors developed through this process are shown in Figure 1 of the Appendix. The resultant 2022 peak hour forecasts are shown in Figure 2 through Figure 3 in the Appendix.

5. Traffic Analyses

a. Corridor Vehicular Analysis

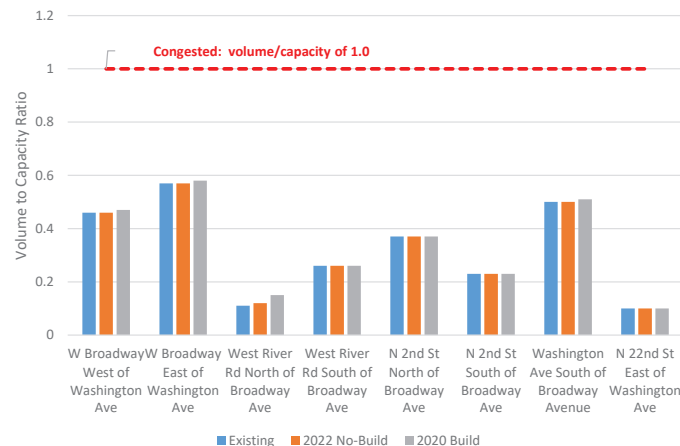
While many factors contribute to a road feeling congested, the two biggest factors are volume, how many vehicles are using the road, and capacity, how many vehicles the road can accommodate a day. Transportation professionals use these pieces of information to create a ratio of volume to capacity. For example, a road with a volume to capacity ratio of 1.0, where the traffic demand is nearly equal to the traffic supply, will feel congested to motorists.

Below is a rough guide to the daily traffic volumes different types of roads can accommodate based on Exhibits 16-16 and 12-39 of the *Highway Capacity Manual, 6th Edition*. If the Average Daily Traffic (ADT) volume on a roadway is below the threshold, then it is considered un-congested. If the daily volume falls within the range, the road is almost congested, and if the daily volume is over the threshold, the road is congested.

- 2-Lane (one in each direction with left turn lanes at busy intersections and coordinated signals), undivided street, are considered congested with a volume between 8,900 to 18,300 vehicles per day.
- 4-Lane, divided street (two in each direction with left turn lanes at busy intersections and coordinated signals), – 18,600 to 36,800 vehicles per day.

To provide an initial planning level screening, Chart 1 provides volume to capacity ratios of the study corridors using daily volumes during the existing, 2022 No-Build and 2022 Build scenarios.

Chart 1 – Study Corridor Volume to Capacity



As seen in Chart 1, the No-Build and Build conditions have similar results. This suggests little to no impact is expected due to this proposed redevelopment. All corridors are forecast to operate within their existing capacities with the addition of traffic from this development.

b. Intersection Vehicular Analysis

Individual intersections can perform poorly during peak periods while the overall roadway corridor is operating with an uncongested daily volume to capacity ratio lower than 1.0. Therefore, capacity analyses are performed for the study intersections to determine if they need improvements such as turn lanes or an upgrade in traffic control.

The existing and forecasted turning movement volumes along with the existing intersection configurations and traffic control were used to develop the average delay per intersection in each study scenario. The delay calculations were done in accordance with the *Highway Capacity Manual, 6th Edition* using the Vistro software package. The full calculations for each study scenario, including Level of Service (LOS) grades and queue lengths, are included in the Appendix. Also, included in the Appendix is a guide explaining the Level of Service grade concept.

Chart 2 (a.m. peak hour) and Chart 3 (p.m. peak hour) show the average peak hour delay per traffic signal-controlled intersection for each study scenario. The LOS D/E

boundary of 55 seconds of delay per vehicle is considered the threshold between acceptable and unacceptable traffic signal operation in Minnesota.

The signal timing was provided by the City of Minneapolis.

Chart 2 – A.M. Peak Hour Delays: Signal Controlled Intersections

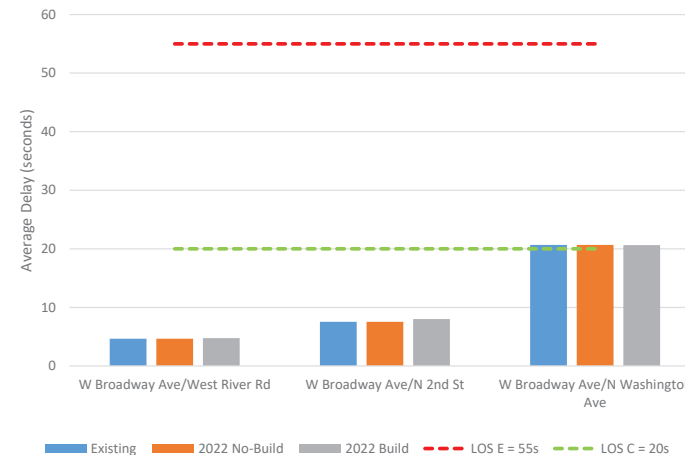


Chart 3 – P.M. Peak Hour Delays: Signal Controlled Intersections

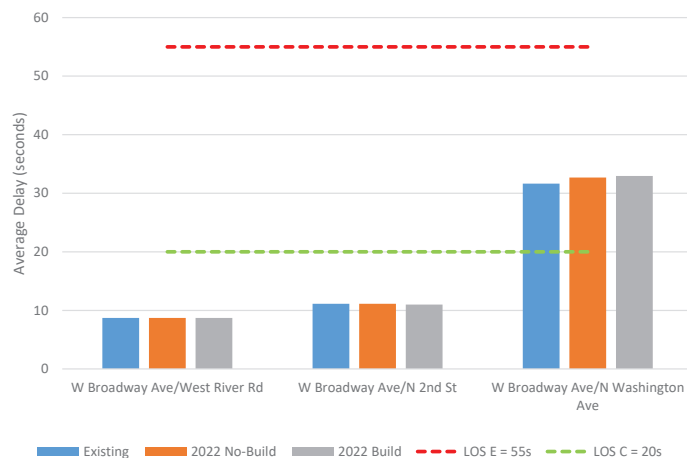


Chart 4 (a.m. peak hour) and Chart 5 (p.m. peak hour) show the 95th percentile queue lengths on the busiest stop sign controlled approach at intersections with side street stop sign control. Average delays are not calculated for intersections with side street stop sign control because the vast majority of vehicles going through the intersection are on the main roadway and have zero delay, which leads to low overall average delays. At side street stop sign controlled approaches to busy roadways, the average delay for all the vehicles on the approach often exceeds 60 seconds. This can be the case for a few vehicles waiting at the stop sign where improvements would not be justified for the low traffic volume. Based on our experience, improvements are not warranted at these types of intersections until the 95th percentile queue at a stop sign is in the five to ten vehicle range.

Chart 4 – A.M. Peak Hour Queues: Side Street Stop Sign Controlled Intersections

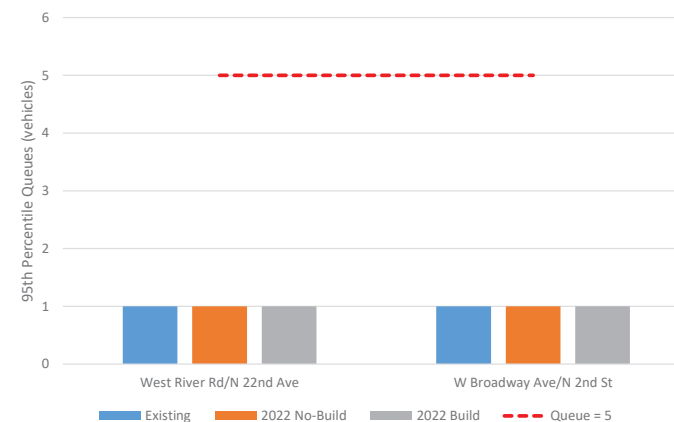
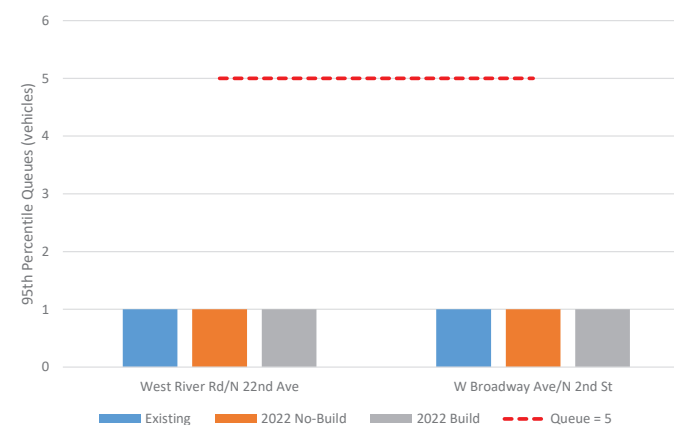


Chart 5 – P.M. Peak Hour Queues: Side Street Stop Sign Controlled Intersections



Per the above analyses, all study intersections and corridors are forecast to operate acceptably throughout the study scenarios. Given the existing, 2022 No-Build, and 2022

Build results are similar, little to no impact is expected due to the proposed development traffic. This is due to the low general background growth rate in the study area and relatively low net new trips associated with the removal of the existing restaurant and addition of the proposed residential development.

It is worth mentioning that queueing along Broadway Avenue is experienced, and the westbound 95th percentile queue at W Broadway Avenue/N Washington Avenue does stretch to the Broadway Avenue/N 2nd Street intersection in the existing condition. This existing queue is lengthened by roughly two vehicles in the build scenarios. This additional queueing is minimal and does not raise any specific concerns. Updated signal timing, which should occur every three to five years should help provide improved traffic flow along the corridor.

As previously mentioned, the wide cross sections and lack of pavement markings on West River Road at W Broadway Avenue can cause some confusion. To help ensure all vehicles understand the designated operations pavement markings should be updated and signage installed to clearly notify the following configurations:

- Northbound – Shared through/left lane, right turn lane
- Southbound – Designated left, through, and right turn lanes

Finally, additional review was given to the intersection of West River Road/N 22nd Avenue as existing volumes were not generated for that intersection. Based on the trip generation and distribution of the proposed residential development, six a.m. peak hour trips and eight p.m. peak hour trips are expected to route through this intersection. This magnitude of traffic does not raise any concerns and little to no impact to traffic operations is expected at this intersection.

6. Parking Analysis

a. Automobile Parking Forecasting & Analysis

Parking to be Provided On-Site

There will be 163 vehicle parking stalls are provided onsite, 74 parking stalls will be in the structured parking facility with 89 parking stalls provided in the surface lot.

Parking Required by Minneapolis Code

This development is in the Pedestrian Oriented Overlay District.

According to the City of Minneapolis' Code of Ordinances, no additional modifications are placed on residential developments in the Pedestrian Oriented Overlay District. Therefore, a minimum of 1.00 spaces per dwelling unit, or 163 spaces for this development are required. No maximum parking limit is stated. With 163 parking stalls provided the proposed development meets the City of Minneapolis' requirements.

Expected Parking Demand - ITE

The Institute of Transportation Engineers (ITE) has put together a document, *ITE Parking Generation, 5th Edition*, that compiled parking demand data from different land uses. Using that data, a peak parking demand of 90 stalls on an average weekday was calculated for an apartment building of this size. Given that the 163 stalls are proposed for the site, there is expected to be more than enough parking to accommodate the development's residents, employees, and visitors.

b. Bicycle Parking Forecasting & Analysis

Section 541.180 of the City of Minneapolis Code of Ordinances states that for the residential portion of the site, one bicycle parking space is required for every two dwelling units. With 163 dwelling units, 82 bicycle parking spaces are required with at least 74 of them meeting the standards for long-term bicycle parking. Required long-term bicycle parking spaces are to be located in enclosed and secured or supervised areas protecting from theft, vandalism, and weather and shall be accessible to intended users. Required long-term bicycle parking for residential uses are not to be located within dwelling units or within deck or patio areas accessory to dwelling units.

The concept site plan shows one bicycle storage room which can be utilized to meet the needs for the long-term parking for the site. No exterior bicycle parking is shown. Exterior bicycle parking spaces in a well-lit area near a main access to the development should be provided for visitors.

7. Site Analysis

a. Access Points

The proposed development will utilize the existing access point onto West River Road. No additional accesses are proposed resulting in no change to the number of curb cuts.

The proposed access is roughly 225 feet south of the Minneapolis Park & Recreation Board's building access and 370 feet north of W Broadway Avenue/West River Road. No concerns arise due to this access. Though not needed for capacity reasons, the existing dedicated northbound left turn lane at the site access should remain to accommodate the transition to the wider section of West River Road south of the site access.

b. Pedestrian Facilities

The development will add new sidewalk along the west side of West River Road from the site access to the intersection with Broadway Avenue providing access to the existing pedestrian facilities from the parking lot and back entrance area.

c. Loading Zones and Trash Removal

Minneapolis Code of Ordinances *Section 541.480 – Specific off-street loading requirements* requires a small sized loading zone of at least 10 feet by 25 feet for the residential portion of the site.

A small loading space for residential use is identified in the surface lot parking area of the development to accommodate resident move in and move out operations. The space is located within a single parking spot adjacent to an accessible stall and a standard stall. Sufficient parking stall width should be provided for the loading space to ensure loading and unloading vehicles do not infringe upon the accessible parking stalls area.

The trash and recycle area is proposed to be located within the parking garage. If the trash is not being moved curbside for collection, turning movements for trash collection vehicles will need to be checked to ensure that the trucks will be able to navigate through the garage.

8. Travel Demand Management (TDM) Plan

The purpose of this section is to provide a description of 2025 West River Road's TDMP Plan and related programs to support the use of alternative methods of transportation other than a single occupancy vehicle. The developer or property manager will develop and implement measures to encourage non-single occupancy vehicle modes of transportation with the goal of having at least 30% of its transportation trips being made by transit, bicycling or walking. The developer or property manager is responsible for and specifically commits to the implementation of the following actions.

a. Designate a Transportation Coordinator

The developer or property manager will designate an employee or contractor to act as the Transportation Coordinator. That employee or contractor will implement, maintain and monitor TDM activities. The Transportation Coordinator will serve as the conduit for providing up-to-date information to site residents, employees and visitors.

b. Disseminate Information

The Transportation Coordinator will be responsible for clearly communicating information for all TDM activities to site employees, residents and visitors. This communication includes, but is not limited to, the following:

- A packet of multi-modal information (either printed, digital, or both) to be given to any new resident upon signing a lease and moving in. These packets will include maps and information on alternate modes of travel, bus routes, NiceRide MN, car sharing and carpool options, and bike routes.
- Distributing information on MnDOT's real-time traveler information program: 5-1-1 or www.511mn.org.
- Hosting a session to have a non-profit organization speak to residents and building employees about different sustainable transportation options and programs.

c. Promote Walking

This site is located in an area with dense pedestrian facilities. Proper lighting should be provided, and clear sidewalks will be maintained, along the frontage of the site to promote and encourage pedestrian travel. The site will meet Minneapolis' requirements for on-site lighting to provide a pedestrian-friendly environment.

d. Promote Bicycling

This site is located in an area that is also well suited for bicyclists. The site will provide convenient and safe bicycle storage for at least 82 bicycle parking spaces. At least 74 of these spaces will meet standards for long-term bicycle parking. Adhering to these requirements will satisfy the requirements in Minneapolis' Code of Ordinances.

The site will provide a bicycle maintenance station in the bicycle storage room for use by residents.

The Transportation Coordinator will provide maps and information to direct riders through the area and to adjacent bicycle trails, as well as to bike repair services close-by.

It is recommended that external bicycle racks also be added in a well-lit area in front of the site for resident and visitor use. Bicycle racks should be placed in such a way that pedestrian traffic on the sidewalk is not impacted.

A nearby Nice Ride bicycle sharing station is located within the Minneapolis Park & Recreation Board's parking facilities. Information on ZAP Twin Cities will be included in the packets for residents. ZAP Twin Cities is a program incentivizing bicycle commuting that enters users into monthly prize drawings.

e. Promote Transit

Transit information will be shared with each resident and employee at the site. At a minimum, bus timetables and stop locations will be posted on-site to better inform residents of transit options in the area. This information will include links to Metro Transit's website as well as mobile apps that can be used to give real time transit information.

The Transportation Coordinator will work with Metro Transit to improve the facilities at the bus stop on the northeast corner of W Broadway Avenue/N 2nd Street. Due to existing geometric constraints the addition of a shelter at this location may not be possible. Based on this, conversations with Metro Transit on the possibility of moving the bus shelter roughly 500 feet east should occur. This would allow space for the addition of a bus shelter, but the decision should be made by Metro Transit.

Residents will be informed of Metro Transit's "Go-To Card" passes for hassle-free transit.

f. Promote Car Sharing & Carpooling

The Transportation Coordinator will work with a car-sharing service (such as Hour Car or Zipcar) in analyzing the feasibility of a reserved space station in the site parking area.

The Transportation Coordinator will investigate ways in which it can assist residents with carpooling opportunities. This will include providing a designated rideshare drop-off/pick-up area within the development.

g. Minimize the Impact of Vehicle Emissions

In an effort to reduce the impact of vehicle emissions due to the site, it is recommended that 5-10 interior stalls be constructed with in place conduit to facilitate the easy transition to electric vehicle charging stations.

h. Minimize the Impact of Trucks

To the extent possible, the developer or their successors will encourage truck and service vehicles (not including small package deliveries) to access the site outside of the weekday peak hours (from 7-9 a.m. and 4-6 p.m.).

i. Monitor Effectiveness

The Transportation Coordinator will conduct baseline surveys to assess resident commuting habits within six months of 50% building occupancy. The Transportation Coordinator will then conduct an annual follow up survey.

If the survey shows that the modal goals of 30% pedestrian/bike/transit are not met, the property owner commits to ongoing mitigations or alternatives strategies that will be employed in the future to address the situation.

9. Conclusions and Recommendations

The traffic impacts of the proposed development were thoroughly studied, and the principal findings are:

- The proposed residential development is expected to generate 716 new trips during an average weekday, 51 new trips during the a.m. peak hour and 70 new trips during the p.m. peak hour.
- Traffic from the developments is not anticipated to have significant impact on the area traffic with existing, No-Build, and Build peak hour operations remaining similar.
- Updated striping and signage should be placed along West River Road, at the intersection with West Broadway Avenue, to notify drivers of the lane configuration and operations.
- The proposed site provides sufficient parking to meet City of Minneapolis requirements for off-street parking.
- At least 82 bicycle parking spaces with 74 meeting the standard of long-term bicycle parking will be provided on site meeting the City of Minneapolis' requirements.
- Exterior bike racks should be provided to encourage building residents, visitors, and employees to use cycling as a mode of transportation.

The travel demand management measures documented in Section 8 of this report shall be implemented to encourage non-single occupancy vehicle modes of transportation for the site's residents and employees.

10. Appendix

- A. Site Plan
- B. The Language of Traffic Engineering
- C. Figures 1-3
- D. Traffic Counts
- E. Trip Generation Table
- F. Level of Service (LOS)
- G. Capacity Analysis Backup
 - AM Existing
 - PM Existing
 - AM 2022 No-Build
 - PM 2022 No-Build
 - AM 2022 Build
 - PM 2022 Build



Our Community is Our Business

Hawthorne Neighborhood Council

2944 Emerson Avenue North • Minneapolis, MN • 55411

Tel: 612.529.6033 Fax: 612.529.0218

www.hawthorneneighborhoodcouncil.org

August 20, 2020

Mei-Ling Smith, AICP
Senior City Planner
City of Minneapolis – Community Planning and Economic Development
250 S. 4th Street, Room 300 | Minneapolis, MN 55415

RE: Letter of Support for Affordable Housing at 2025 West River Road

Dear Ms. Smith,

The Hawthorne Neighborhood Council has reviewed the proposed development at 2025 West River Road otherwise known as the Broadway Pizza site. We offer our support for the project as presented by the developer Schafer Richardson to be affordable housing.

The Hawthorne Neighborhood Council seeks to improve the quality of life in the Hawthorne neighborhood through empowering the residents in order that they can address the physical, cultural, social and economic needs of the community. We provide a forum for the Hawthorne residents, block clubs, workers, and business owners to come together with one another and with governmental agencies that are involved in the neighborhood to eliminate barriers to our vision. Our goal is to create a community of residents and business owners, with well-maintained properties, living and working with pride in a neighborhood that has earned a positive image.

We strongly support the Schafer Richardson proposal for affordable housing at 2025 West River Road. We appreciate that the new development will provide financially accessible housing to residents in an area of Hawthorne that lacks affordable housing. Together, our aligned efforts can make lasting change for the residents of our Minneapolis community.

Sincerely,

Dee Phillips
Board Chair

"To improve the quality of life in the Hawthorne neighborhood through empowering the residents in order that they can address the physical, cultural, social, and economic needs of the community"

From: [Susan Pilarski](#)
To: aduininck@ncsrc.org; raya.esmaeili@metro council.org; samuel.rockwell@gmail.com; [Schroeder, Jeremy](#); cmeyer@minneapolis parks.org; kimberly.caprini@mpls.k12.mn.us; Raya.Esmaeili@metc.state.mn.us; adlpierdesign@gmail.com; Amy.sweasy@hennepin.us; [David Frank](#); [Ellison, Jeremiah](#); Smith, Mei-Ling C.; [Baldwin, Lisa M.](#)
Subject: [EXTERNAL] Broadway Pizza proposed redevelopment / Letter of concern for Public Safety
Date: Thursday, May 14, 2020 2:04:15 PM

May 14, 2020

Dear Planning Commissioners and members of the Commission of the Whole,

I am aware of the proposed plan to develop the Broadway Pizza site on West River Parkway and Broadway to build 163 units of affordable housing.

As a resident of the Hawthorne neighborhood, in my opinion, the proposed development for this site does not follow the housing goals of the City's 2040 plan; it also doesn't follow the goals and guidelines of the Minneapolis Near Northside Master Plan or of the Above the Falls Plan. In addition, there is a strong impact on public safety which I would like to address below:

- Since I reside very close to the site and live it every day, the corner of Broadway and West River Parkway is a very high traffic area. This includes both automobile and truck traffic (18 wheelers as well). The large trucks cannot fit under the railroad bridge on Broadway so they are diverted onto W. River Parkway to follow 22nd St. to the 94 freeway. In addition to all the truck traffic, very often cars run the red light on Broadway, making it dangerous to cross for both vehicle and foot traffic.
- Currently, there is no mass transit stop. If that were to change and a bus stop was placed on the corner of Broadway and W. River Parkway, pedestrians would have to be aware of the heavy traffic flow when crossing Broadway for an eastbound ride. This can be especially dangerous for children.
- For pedestrians crossing the Broadway bridge by foot, bike or for the disabled, there is no guard rail to protect pedestrians from traffic. Often there is broken glass on the sidewalk making it difficult to cross.
- The property is located next to the large Minneapolis Recycling center as well as the railroad tracks for both the Burlington Northern and Canadian Pacific railroads. These are working tracks that are very close to the proposed plan and used daily. Often, I've witnessed teens and children walking the tracks and climbing on parked railroad cars, whether there is a fence or not. All this adds to a very busy traffic center and a safety and pollution concern for residents. Would anyone be able to open a window?

In my opinion, dense housing conditions such as this proposal should no longer be an acceptable best use building practice. Due to our current health crisis and whatever the future may hold, it isn't wise or safe to continue on the same path of building large apartments on every available parcel.

Often public policy doesn't account for the reality of the impact this type of safety/pollution may cause on the health of the community. I believe this space is more suitable for uses other than housing.

Susan Pilarski – Hawthorne resident
2344 River Pointe Circle

[EXTERNAL] This email originated from outside of the City of Minneapolis. Please exercise caution when opening links or attachments.



*Working to protect the Mississippi River
and its watershed in the Twin Cities area.*

101 East Fifth Street
Suite 2000
Saint Paul, MN 55101

651-222-2193
www.fmr.org
info@fmr.org

May 12, 2020

Dear Planning Commissioners:

In advance of this week's Committee of the Whole review of the revised development proposal for 2025 West River Road, we at Friends of the Mississippi River (FMR) would like to share some comments on the project. We recognize that this project is not yet pursuing formal approval, but we wanted to provide feedback now while the proposal is still being shaped.

For over 25 years, FMR has taken a particular interest in the Above the Falls region of North and Northeast Minneapolis. The disparities in this area's river access and contiguous riverfront trails are connected to other disparities such as income, health, and racial and environmental injustice. We stand alongside residents who advocate for a future that reconnects neighborhoods to their riverfront as industrial uses move elsewhere.

Early North Minneapolis riverfront redevelopments, such as this one, will set the tone for the decades of future development we anticipate as the Northside riverfront transitions away from industrial uses. We hope that the riverfront will welcome and reflect existing North Minneapolis residents, serving as an extension of the Northside rather than of the North Loop.

When a project seeks a Conditional Use Permit, as this one is, we hope to see something exceptional in exchange. We don't believe this proposal meets that bar. The updated project proposal does not appear to respond to many of the concerns we heard raised in February by the Committee of the Whole as well as members of the Hawthorne Neighborhood housing committee.

Most notably, this project still lacks any kind of commercial or public use. We strongly believe that public-serving commercial activity should be a priority for this site. As commissioners noted during February's review of this project, this site sits at a major intersection that's an important gateway to North Minneapolis and to the Mississippi riverfront. It's a midpoint between existing commercial nodes along Broadway Avenue on both sides of the river and could serve as a connection between those. Moreover, the loss of Broadway Pizza as a rare sit-down restaurant in the neighborhood is acutely felt. This proposal does not respond to these concerns. We encourage the Commission to continue seeking a first-floor commercial use in this project.

In February we also heard commissioners request that the project contribute to an active riverfront. The updated project does include some balconies and a modest terrace on the river side of the building, which we appreciate. The main outdoor building amenities, such as the play area and the

dog run, are still located near the parking lot and railroad tracks instead of near the river parkway. We understand the desire to maintain the slope that obscures the first-floor parking, but adding more river-facing amenities would improve the building's connection to its surrounding neighborhood.

Although Minneapolis will not adopt a new Mississippi River Corridor Critical Area (MRCCA) ordinance until later this year, we encourage the Commission to make decisions, when possible, in the spirit of this forthcoming ordinance. FMR hopes to see CUP requests accompanied by a written analysis of how the CUP is consistent with the character and purpose of the site's Critical Area designation. This should include some discussion of how a project relates to and enhances the Mississippi riverfront.

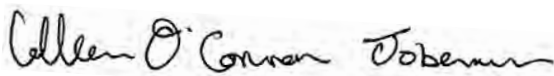
We also expect that CUPs will be contingent on the use of mitigation strategies. In cases of excessive structure height, such as this, mitigation strategies could include tiering the building back from the river, using natural-looking building materials that reduce visual impact, or increasing vegetative screening. In contrast, this revised design actually appears to increase the building's visual impact through reduced tiering and less varied siding.

Given this site's presence in a major international migration flyway, another mitigation strategy could be a commitment to using bird-safe glass. We hope to see the developer include some of these mitigation strategies should this project advance.

We do appreciate that this project responds to the need for affordable housing, and particularly for larger units. The inclusion of 3-bedroom units is laudable. The level of affordability provided by this project may help meet a regional need, though it will not meet North Minneapolis residents' affordable housing needs as rents will be above the existing market rate for the neighborhood.

We encourage the Commission to continue their thoughtful review of this project, and to advocate for a development deserving of this important gateway location. Thank you for your consideration.

Sincerely,

A handwritten signature in black ink, reading "Colleen O'Connor Toberman". The signature is fluid and cursive, with the first name "Colleen" being the most prominent.

Colleen O'Connor Toberman
River Corridor Director

05/13/20

City of Minneapolis Planning Commission
350 South 5th Street, Room 331
Minneapolis, MN 55415

SENT VIA EMAIL

RE: 2025 West River Road Proposed Redevelopment

Dear Minneapolis City Planning Commission,

We recently became aware of the 2025 West River Road proposal, Item #5 on the Committee of the Whole agenda, for discussion tomorrow, May 14, 2020.

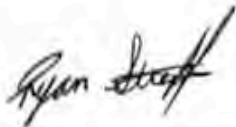
Our review of preliminary details of this redevelopment suggests that this project as proposed is not consistent with the letter or spirit of the 2040 Comprehensive Plan, Above the Falls Master Plan, or the Near Northside Master Plan. We have economic, traffic, safety, environmental, design, sustainability, and community impact concerns based upon the information shared thus far.

We want to play an active role in the planning discussions. We are in the process of researching and gathering additional information that we will share with you as the Planning Commission deliberations continue.

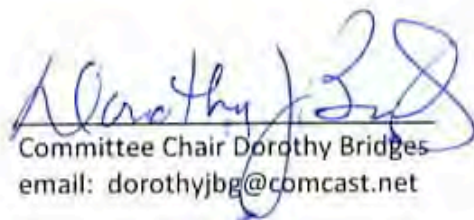
Please include us on all communications, meeting times and dates.

Sincerely,

Riverside Townhomes Association



President Ryan Streff
email: ryanstreff@hotmail.com



Committee Chair Dorothy Bridges
email: dorothejbg@comcast.net

CC: Councilmember Jeremiah Ellison, Ward 5
CPED Director David Frank
City Planner Mei-Ling Smith

City of Minneapolis

Planning Commission

350 South 5th Street

Minneapolis, MN 55415

Attention: Sam Rockwell, President

As a property homestead owner in the City of Minneapolis and the neighborhood of Hawthorne, I am writing to communicate regarding the plans being developed for the site where currently the Broadway Bar and Pizza is located at 2025 West River Road North. There is a proposal being brought before the City to redevelop the site in ways that are not beneficial to the community. As part of the proposal by Schafer Richardson, the developer, (with a phone number of 612-371-3000), Broadway Bar and Pizza will be sold and affordable housing will be built on site. Originally, there used to be the Riverview Supper Club nearby. The community could enjoy a meal on the Mississippi River. This is the Gateway to North Minneapolis and ideally the greater community could wish for a fine dining restaurant. It could be something like a P.F Chang's or Granite City, or other possibilities such as, African American cuisine, Asian, or Hispanic cuisine. This would attract new and younger citizens to the Northside. This could be the beginning of bringing some stability to the community by increasing income in the neighborhood. A patio could be added for the summertime with a very attractive building and menu creating a desire for people from the neighborhood and outside of the neighborhood to enjoy being together. Most Community residents have overwhelmingly requested a sit down restaurant on site. To my knowledge, Schafer Richardson has refused to consider this, even though they have newly constructed properties with this amenity.

We need change in North Minneapolis. We can't continue to do the same things and expect a different outcome. As far as housing, we all know there has been no market rate housing built in the 55411 zip code in the past ten years, only affordable housing. North Minneapolis needs the same types of amenities as other areas of the City. Let's not take away one of the few sit down restaurants on the Northside and not add another, or at least something more creative that will draw people to North Minneapolis.

Respectively,

Georgianna Yantos

From: [Ron Anderson](#)
To: [Smith, Mei-Ling C.](#)
Subject: [EXTERNAL] Concerns Regarding the Broadway Pizza Development
Date: Sunday, September 13, 2020 7:36:35 PM
Attachments: [Broadway Redevelopment Project Letter to Planning Commission 6-12-20 \(2\).pdf](#)

Ms. Smith,

I am reaching out to you to ask for your assistance regarding the proposed Shafer Richardson redevelopment project at West River Road and Broadway (at the the current site of Broadway Pizza). My wife and I live just down the street from that location in the Riverview Townhomes (two blocks North on West River Road). We moved there from a big house in Minnetonka 12 years ago to enjoy our empyy nesting years with the beautiful river front views and access to a wealth of great restaurants and entertainment attractions. As a multi-racial family, we also wanted to contribute to the revitalization of North Minneapolis.

My wife and I and our fellow residents of Riverview Townhomes have strong concerns over the current development plan. We would welcome an attractive mixed use development that would contribute to revitalizing North Minneapolis by providing much needed affordable AND market rate housing and a badly needed sit down restaurant. We oppose the current plan of a rather mundane all-affordable housing 163-unit apartment that:

- Isn't aesthetically worthy of such a prime riverfront location with spectacular views of the Minneapolis Skyline.
- Has a proposed density that is much too high for the site.
- Introduces significant safety and security issues.
- Continues to concentrate affordable housing in North Minneapolis (*as all 325 of the new rental units built in North Minneapolis since 2010 have been affordable housing units*).
- Wastes a rare opportunity to add the much needed market rate housing necessary to reestablish a thriving middle class in North Minneapolis.
- Is in violation of the Minnesota Department of Natural Resources maximum height for what the DNR has deemed a critical area along the Mississippi River.
- Is in direct opposition of a number of stated goals of the Minneapolis "2040 Plan".

I've attached a document below that explain the above concerns much more fully. Schafer Richardson (the project developer) will be making its presentation to the Minneapolis Planning Commission on September 21st at 4:30 PM. My wife, myself and our fellow residents would be extremely appreciative if you would take a few minutes to review the attached document and express your concerns to the appropriate parties to help ensure a positive conclusion to this process. We encourage Minneapolis to take full advantage of this rare opportunity to develop such a unique parcel of prime river front property in a manner that is worthy of being the "Gateway to North Minneapolis" and helps make the vision of the "2040 Plan" one big step closer to reality.

Please feel free to contact me with any questions, comments or concerns. And thank you in advance for your assistance.

Ron Anderson
2331 River Pointe Circle

Minneapolis, MN 55411
952-334-2902

[EXTERNAL] This email originated from outside of the City of Minneapolis. Please exercise caution when opening links or attachments.

06/12/20

City of Minneapolis Planning Commission
350 South 5th Street, Room 331
Minneapolis, MN 55415

RE: Proposed Redevelopment of Broadway Pizza Property/2025 West River Road North

Dear Commissioners:

We write with concern on behalf of the Riverview Townhomes Association about the proposed housing development plan on the site of the Broadway Pizza restaurant located at West Broadway & West River Road North in the North Minneapolis Hawthorne neighborhood.

We have examined the preliminary plans provided to us by Schaefer Richardson and wish to object strongly to this development for the following reasons and detailed in the table.

1. While the development of additional housing may be desirable for the City, it must be done with due consideration to the already fragile nature of our tax base in North Minneapolis. Continuing to concentrate only lower income housing in our community, without carefully considering projects that include mixed income housing, much like the development project being built on Lowry and Marshall Avenues in Northeast, puts us further behind the critically important goal of building community wealth. From 2010- 2018; 325 new rental units were developed in Near North, none of them were developed at Market Rate. All of the new projects were solely affordable housing.*Twin Cities Multifamily Market 2017-18.

2. We have seen multiple examples of failed projects in our City and hope to learn from these mistakes. A notable example of this is the July 1992 federal lawsuit (Hollman v. Cisneros) filed by Minnesota Legal Aid and the National Association for the Advancement of Colored People (NAACP) on behalf of a group of plaintiffs living in public housing in Minneapolis claiming that the policy of providing concentrated low-income housing in the city perpetuated racial and low-income segregation. In April 1995, a consent decree was signed that committed the co-defendants, including the City of Minneapolis, to a series of dramatic policy changes aimed at deconcentrating family public housing in Minneapolis. The so-called "Hollman decree" attempted to combine several important initiatives in public housing: (1) the redevelopment of older, less functional public housing projects into ***mixed-income, mixed-use communities***; and (2) the use of housing vouchers to allow low-income families to move out of neighborhoods of high poverty. The resulting Heritage Park on the West side of downtown Minneapolis is an example of how to create mixed income communities that, in the long run, are more vibrant and sustainable. We want our City's history to serve as a reminder to develop housing in a manner that reduces concentrations of poverty and creates income diversity to ensure a healthier community.

According to the Dougherty Mortgage Market Report, from 2010- 2018, "72 % of the new supply [of affordable housing] has occurred in five areas: Near North, Phillips and Longfellow in Minneapolis, and St. Anthony/Midway and West 7th Summit Hill in St. Paul." This statistic suggests that we are moving in a potentially harmful direction by not carefully dispersing affordable housing throughout the City.

3. For the "average" citizen who is lucky enough to own a home, their greatest source of

wealth is that home. Only 30.5% of people in Hawthorne own their home. Since 2008, we lost 10,000 middle class homeowners in our community due the financial crisis. We need to make every effort to bring those homeowners back into Hawthorne neighborhood. While we are not suggesting that this becomes a home ownership project, this development must be part of the solution: to attract residents with a range of incomes and thereby stabilizing our Northside community.

4. The goals as stated and adopted by the City Council in its 2040 Comprehensive Plan (the "2040 Plan") are intended to provide direction to staff in the development of policies that guide the future development of the city where all communities thrive. As currently proposed, this development does not meet 2040 Plan goals important to the future of our community in North Minneapolis.

- a. The 2040 Plan Goal #9 states, "in 2040, all Minneapolis residents will have access to employment, retail services, healthy food, parks and other daily needs via walking, biking and public transit." This project does not meet the requirement of complete neighborhoods because it concentrates housing but does not provide any space for much-needed amenities, such as retail or food services that are open and accessible to the neighboring community.

One specific policy related to the critical nature retail holds in our communities is 2040 Plan policy #4, which describes active steps the City will take to reduce greenhouse gas emissions by including more commercial locations within walking and biking distance of our neighborhoods. Policy 4 states, "in most of Minneapolis, demand for retail is much higher than supply, indicating an opportunity to make retail more convenient for everyone and thereby reduce car trips and greenhouse gas emissions."

- b. There is overwhelming support in our community for a sit-down restaurant. A restaurant is not included in the plan because, according to the developer, an all-affordable housing project cannot economically support a restaurant. Clearly, that is untrue as developers in NE, the Loop and other areas contiguous to North Minneapolis have recently completed all-affordable housing developments that included a restaurant as well as other retail commercial use. But importantly, the better approach would be to develop a mixed income housing development, which would certainly support a restaurant.

- c. The proposed project is in violation of 2040 Plan policy #37, which prioritizes mixed-use housing. This policy reinforces our request for a more balanced approach to this project. The 2040 plan states that "City intervention may be needed throughout the city to support mixed-income housing, including both market rate and affordable housing, at all levels of affordability." In this instance, two of the 2040 action steps identified in Policy #37 are applicable to this project:

- i. Action Step a. "Create and refine policies, programs, regulations, and other tools to develop mixed-income housing throughout the city for ownership and rental housing." Any redevelopment of this land should be viewed as an opportunity to recoup some of the homeowners lost during the financial crisis and recruit new mixed-income residents.

- ii. Action Step c. "Cultivate a culture of mixed-income housing production throughout the city." Every community adjacent to North Minneapolis is developing mixed-income projects, so it is absolutely appropriate that such development be undertaken in North Minneapolis as well.
 - d. The 2040 Plan most importantly addresses housing specifically in the Northside in Policy #87: "Reverse institutional harms caused to the Northside community by building on the many assets of the community while also prioritizing community wealth building in the form of housing, small business, public safety, youth opportunities, and environmental justice." One remedy to these institutional harms that moves us towards reconciliation is for the City to commit to doing its part to build a spectrum of individual and community wealth in North Minneapolis by creating a more sustainable healthy housing stock that serves all existing, new and returning community residents. We have assets in our community and we need to leverage those in any proposed development.
- 5. The density of the project is much too high for the proposed site.
 - a. Our understanding is that HUD will not finance the project due to the close proximity of the railroad tracks and associated safety concerns. This project should be under more scrutiny and provide additional precautions given these known safety concerns.
 - b. Crossing Broadway on the River Road, especially during a typical rush hour, is very dangerous given the number of cars and their accelerated speed. The proposed housing will significantly increase the number of drivers using this intersection. According to the Vision Zero Crash Study, Broadway St. NE is on the [Crash Concentration Corridor](#). "Sixty-two (62) percent of all vehicle crashes occurred on six percent of the streets in the city." The intersection of the River Road and Broadway is included in that 6% of the Vehicle Crash Concentration Corridor. These corridors encompass an estimated 28 percent of the vehicle miles traveled on city streets (See Vision Zero Crash Study).
 - c. Eighteen-wheel semis frequently use this alternative route to turn from Broadway to West River Road as they cannot fit under the railroad bridge. They can barely make the turn. Large trucks also turn from West River Road onto Broadway. The additional amount of commuter and resident load will put tremendous stress on this route and cause significant traffic delays in this area and will result in traffic incidents with semi-trucks traveling this route daily.
 - d. The lack of a large play area for children in the proposed development makes this corner especially dangerous as children seek space to explore and play together.
- 6. The physical design of the development as currently proposed does not meet with character of the riverfront. There are many elements that are lacking including variation to roof lines, creative façade, or architectural design elements to achieve pedestrian friendly composition. There is no change to primary material to break up texture and no façade step backs or extensions to create variation. Additionally, its height elevation is in violation of the height restrictions established by the zoning requirements of the Mississippi River Corridor Critical Area. Please see attached Exhibit 1 for Slide Deck evaluation and comparison of proposed development with other recent developments.

RIVERVIEW TOWNHOMES ASSOCIATION

Comparison of Housing Type and Incomes in Certain Minneapolis Neighborhoods

	Total Population	Housing Units	Owner- occupied	Renter- occupied	Income less than \$35,000	Race = White	Race = Of Color	Below Poverty
Neighborhoods								
Hawthorne	4,717	1,780	30.5%	55.4%	57.2%	21.8%	78.2%	34.6%
Near North	7,241	2,431	31.4%	61.3%	53.4%	15.4%	84.6%	36.3%
Communities								
Near North	35,868	12,050	39.2%	52.8%	45.1%	23%	77%	30%
Northeast	37,473	17,606	52.5%	42.7%	28.7%	74.9%	25.1%	15.7%
Central	35,312	23,676	21%	69.3%	39.6%	66.2%	33.8%	24%
Camden	30,729	11,792	58.4%	34.8%	35.6%	44.1%	55.9%	22.7%
Calhoun Isles	32,424	18,964	35.5%	57.2%	19.8%	86.1%	13.9%	5.7%
City								
Minneapolis	416,021	185,260	44.4%	49.5%	31.8%	63.8%	36.2%	19.9%
County								
Hennepin	1,235,478	529,475	59.3%	35.7%	23.2%	72.6%	27.4%	11%
Source: http://www.mncompass.org/profiles/neighborhoods/minneapolis-saint-paul#!areas								

Evaluation of Proposed Development based on the 2040 Comp Plan, Near Northside Master Plan, and Above the Falls Master Plan

Plan	Proposed Plan Meets Goal	Proposed Plan does NOT Meet Goal	Notes
2040 Comprehensive Plan			
Goal #3 Affordable and Accessible Housing		X	Project does not promote the development of new housing options in an area that currently lacks housing choices.
Goal #6 High-Quality Physical Environment		X	Project does not provide a thoughtful connection between public spaces (i.e. public realm) and private development.
Policy 5 Visual Quality of New Development		X	Project does not have a variety of architectural styles. No brick or block for visual interest.
Goal #9 Complete Neighborhoods		X	Project and site location does not promote a Complete Neighborhood.

RIVERVIEW TOWNHOMES ASSOCIATION

			Access to goods and services are not available to residents in this area.
Policy #4 Access to Commercial Goods and Services		X	Project and site location does not promote a Complete Neighborhood. Access to goods and services are not available to residents in this area.
Goal 12 Healthy, Sustainable and Diverse Economy		X	Project does not expand business opportunities in the area. No commercial or restaurant space is being provided.
Policy #2 Access to Employment		X	Residents without a personal vehicle or a dedicated public transit route will not have access to employment.
Policy #87 Northside		X	Project does not promote a diverse housing stock nor does it achieve housing stability.
Policy #37 Mixed Income Housing		X	Project does not promote Mixed Income Housing.
Minneapolis Near Northside Master Plan			
Principles for Neighborhood Design Action Plan Guiding Principles		X	Project does not promote a mix of housing types. Buffers are not provided between non-compatible land uses. Project does not include commercial or retail space.
Urban Design Principles		X	Project does not provide a “design for social integration”, “encourage new and support existing essential neighborhood amenities”, “design for safety”, or provide a “design within historic context”.
Above the Falls Master Plan			
Community and Economic Development		X	Project does not provide or support a residential/mixed use development
		X	Project only creates an additional concentration of affordable units and a further concentration of poverty.
Housing		X	Project may negatively affect further development in North Minneapolis due to the concentration of affordable housing.

RIVERVIEW TOWNHOMES ASSOCIATION

Residential/Mix Use		X	Project does not support the market trends or the community needs. It only supports the concentration of poverty.
Community Development and Housing		X	Project does not expand housing choices. The community is in the need for additional tax base and “move up” housing options at the riverfront.
Supporting Residential Development		X	Project does not support the City’s housing policy. The City does not support concentrating new affordable housing in areas with high poverty rates.
		X	Project does not indicate how they will address the fairly weak transit system in the area.
Source: http://www.ci.minneapolis.mn.us/cped/planning/cped_plans			

We request consideration of the aforementioned concerns in any future decisions regarding redevelopment of 2025 River Road. We look forward to your response.

Sincerely,

Riverview Townhome Board and Residents

Ryan Streff
Board Member/Hawthorne Resident

Susan Vikse
Hawthorne Resident

Dorothy Bridges
Board Member/Hawthorne Resident

Angie Sherer
Hawthorne Resident

Tim Baylor
Hawthorne Resident

Jessica Sigel Sherer
Hawthorne Resident

Susan Pilarski
Hawthorne Resident

Angela Priebe
Hawthorne Resident

CC: Honorable Mayor Jacob Frey
Minneapolis City Council
Minneapolis Park Board

EXHIBIT 1

BUILDING DESIGN AND MATERIALS CONCERN

Broadway Pizza Redevelopment
2025 West River Road

PURPOSE:

This design slide deck is being provided to outline the neighborhood concerns of the Building Design and Materials that are being proposed by Schafer Richardson for this redevelopment project.

The design slide deck compares the proposed Schafer Richardson redevelopment to new/existing developments in the area that provide the following: 1) Creative façade and architectural design elements; 2) Variation in roof lines and roof styles; 3) Change in primary building materials and texture; 4) Façade step backs and façade extensions.

DESIGN CONSIDERATIONS:

- Gateway to North Minneapolis
- River Front / Pedestrian Design
- Quality Design and Building Materials
- Variation of roof lines and materials
- Timeless yet modern construction
- Integration with natural elements



Proposal Developer - Schafer Richardson



Proposed Project

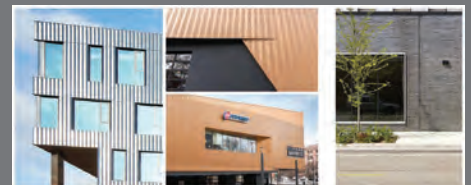
- Units: 163 Affordable Units
- Height: 7 Stories
- Parking: 163 stalls (74 below-grade and 89 surface stalls)

Building Materials

- Metal Siding - Two different colors at approx. 85% of the façade
- Block/Brick – One color along bottom at approx. 15% of the façade.

Design / Massing / Height Variation

- **NO** creative façade or architectural design elements to achieve a pedestrian friendly or river front facade composition.
- **NO** variation in roof lines to emphasize building entrances, building corners, or other large mundane sections of the structure.
- **NO** change to primary metal siding material to break up the texture and feel of large mundane facades.
- **NO** façade step backs or façade extensions to create variation.



Building Design and Material examples of apartments and other structures in the area.

- ❖ Creative façade and architectural design elements
- ❖ Variation in roof lines and roof styles
- ❖ Change in primary building materials and texture
- ❖ Façade step backs and façade extensions

Maverick Apartments Minneapolis, MN



Market Rate Housing

Grain Belt Apartments NE Minneapolis, MN



Market Rate Housing

Ironwood Apartments New Hope, MN



Market Rate Housing

Legends Apartments Columbia Heights, MN



Affordable Housing

Borealis North Look Apartments Minneapolis, MN



Market Rate Housing

Solhavn Apartments Minneapolis, MN



Market Rate Housing

5th Avenue Lofts / Apartments Minneapolis, MN



Market Rate Housing

Soltva Apartments Minneapolis, MN



Market Rate Housing

Phoenix Apartments Minneapolis, MN



Market Rate Housing

Junction Flats Apartments Minneapolis, MN



Market Rate Housing

Mezzo Apartments NE Minneapolis, MN



Market Rate Housing

Hawthorne EcoVillage Apartments North Minneapolis, MN



Affordable Housing

Riverview Townhomes North Minneapolis, MN



Market Rate Housing

LifeSource – North Minneapolis, MN



Non-profit company

Coloplast – North Minneapolis, MN



For-profit company

DC Group
North Minneapolis, MN



For-profit company

Ryan Streff

Hawthorne Resident

September 12, 2020

City of Minneapolis
Attn: Planning Commission & CPED
350 South 5th Street, Room 331
Minneapolis, MN 55415

RE: Letter of Objection / Proposed Redevelopment of Broadway Pizza Property / 163 Affordable Housing Units / 2025 West River Road North

Dear Planning Commission of the Whole,

I am providing this Letter of Objection as a Hawthorne Resident with regards to the proposed redevelopment of the Broadway Pizza property into a 7 story, 163-unit apartment complex. Based on my review of the City of Minneapolis Zoning Code, City of Minneapolis 2040 Comprehensive Plan and the Minneapolis Near Northside Master Plan the project as currently proposed does not meet the intent of the adopted codes and approved plans.

The City of Minneapolis needs to consider a more balanced approach for this redevelopment project and for other projects moving forward. I am concerned by the lack of market rate housing being proposed with this redevelopment project as well as other recent developments in my neighborhood and the Northside. To build community wealth we must attract more middle-income households to live on the Northside. We cannot continue to forget the Northside or the people that reside here. The City and its developers should take community history into account when proposing any new redevelopment or development projects. To achieve a more equitable City, some neighborhoods will have different needs to meet these goals. In addition to market rate housing, our area is in critical need of retail and commercial amenities, especially healthy food options. These needs should also be taken into consideration in all future development plans. The large density of the project and the lack of retail or commercial space is alarming to the neighborhood. This prime riverfront location should demand more scrutiny to address the concerns of the community.

Please review my examination below as to why I object to this proposed project and how the proposed project is in violation of the adopted codes and approved plans. I hope to see the following items addressed as this application moves through the process.

Zoning District/Purpose	As defined by City Code	Notes (Proposed Project Does NOT Meet)
Subject Property: Primary Zoning District	Community Activity Center District (C3A)	
Purpose: Commercial Districts	The commercial districts are established to provide a range of goods and services for city	<i>The project as proposed does not meet the purpose</i>

	residents, to promote employment opportunities and the adaptive reuse of existing commercial buildings, and to maintain and improve compatibility with surrounding areas. In addition to commercial uses, residential uses, institutional and public uses, parking facilities, limited production and processing and public services and utilities are allowed.	<i>or intent of "Commercial Districts".</i>
Purpose: Community Activity Center District (C3A)	The C3A Community Activity Center District is established to provide for the development of major urban activity and entertainment centers with neighborhood scale retail sales and services. In addition to entertainment and commercial uses, residential uses, institutional and public uses, parking facilities, limited production and processing and public services and utilities are allowed.	<i>The project as proposed does not meet the purpose or intent of "C3A" District.</i>
Building Bulk Requirements: C3A	Four (4) stories or fifty-six (56) feet, whichever is less	<i>The project as proposed does not meet the purpose or intent of the "Building Bulk Requirements".</i>

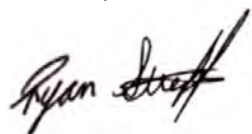
2040 Comprehensive Plan	Notes (Proposed Project Does NOT Meet)
Goal #6 High-Quality Physical Environment	<i>The project as proposed does not provide a thoughtful connection between public spaces (i.e. public realm) and the proposed private development and thus does not meet Goal #6 of the Comprehensive Plan. Due to the proposed project location, along an extremely busy thoroughfare, combined with the proposed high-density development, the project has not created an environment that facilitates positive human interactions or considers safety through urban design. With no commercial or restaurant space the project does not encourage activity that will promote a sense of place or community identity for this entrance to North Minneapolis or the Hawthorne Neighborhood. By integrating commercial or restaurant space into the proposed project it would help</i>

	<i>promote the neighborhood by creating a destination along the river front in addition with job creation (to supplement the jobs and the destination that will be lost by removal of Broadway Pizza at the site location). Broadway and West River Road is the gateway to North Minneapolis and the Hawthorne Neighborhood and thus a demand for a high-quality connection should be mandatory.</i>
Policy 5 Visual Quality of New Development	<i>The project as proposed does not include a variety of architectural styles to draw visual interest and thus does not meet Policy #5 of the Comprehensive Plan. Most of the façade is made up of what appears to be a very inexpensive façade covering that resembles brown corrugated cardboard and/or metal shipping containers. The design should incorporate additional brick (real brick and not brick panels) for visual interest. Broadway and West River Road is the gateway to North Minneapolis and the Hawthorne Neighborhood and thus a demand for high-quality design and materials should be mandatory.</i>
Goal #9 Complete Neighborhoods	<i>The project as proposed, and site location does not promote a Complete Neighborhood. Access to goods and services are not available to residents in this area and thus does not meet Goal #9 of the Comprehensive Plan.</i>
Policy #4 Access to Commercial Goods and Services	<i>The project as proposed, and site location does not promote a Complete Neighborhood. Access to goods and services are not available to residents in this area and thus does not meet Policy #4 of the Comprehensive Plan.</i>
Goal #12 Healthy, Sustainable and Diverse Economy	<i>The project as proposed does not expand business opportunities in the area. No commercial, restaurant, or retail space is being provided and thus does not meet Goal #12 of the Comprehensive Plan.</i>
Policy #2 Access to Employment	<i>Due to the proposed project location residents without a personal vehicle will not have access to employment as there is no dedicated bus route or bus stop for residents in this area.</i>
Goal #3 Affordable and Accessible Housing	<i>The project as proposed does not promote the development of new housing choices/options in North Minneapolis and the Hawthorne Neighborhood. While the development of affordable housing may seem like a suitable approach, it must be done with due consideration to the already fragile nature of the tax base in North Minneapolis. Continuing to concentrate only affordable housing in the community, without carefully considering projects that include mixed income housing, puts the community at a disadvantage with achieving this critically important goal of building community wealth.</i>
Policy #37 Mixed Income Housing	<i>The project as proposed does not promote Mixed Income Housing. The development should create Mixed Income Housing options available for the community through a more balanced and comprehensive approach. The City may need to</i>

	<i>intervene to support mixed-income housing, including both market rate and affordable housing.</i>
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Minneapolis Near Northside Master Plan	Notes (Proposed Project Does NOT Meet)
Principles for Neighborhood Design Action Plan Guiding Principles	<i>The project as proposed does not promote a mix of housing types. Buffers are not provided between non-compatible land uses. Project does not include commercial, restaurant, or retail space.</i>
Urban Design Principles	<i>Project does not provide a “design for social integration”, “encourage new and support existing essential neighborhood amenities”, “design for safety”, or provide a “design within historic context”.</i>

Sincerely,



Ryan Streff
Hawthorn Resident

2340 River Pointe Circle
Minneapolis, MN 55411

From: [Baldwin, Lisa M.](#)
To: [Smith, Mei-Ling C.](#)
Subject: FW: [EXTERNAL] Objection to 2025 West River Road North Project
Date: Tuesday, September 15, 2020 2:11:25 PM

FYI

From: Jamison Hauser <hauserjamison@gmail.com>
Sent: Tuesday, September 15, 2020 1:49 PM
Subject: [EXTERNAL] Objection to 2025 West River Road North Project

Dear Minneapolis City Planning Commission,

I am a resident of the Hawthorne Neighborhood and am writing to express my strong objection to the proposed project to redevelop 2025 West River Road North. This proposal does not currently meet the needs of our community.

The city of Minneapolis needs to consider a more balanced approach for new developments. I am concerned by the lack of market rate housing projects in recent and anticipated developments in my neighborhood. To build community wealth we must attract more middle income households to live here. We cannot continue to forget the Northside. I believe the city of Minneapolis needs more affordable housing, but I also believe there should be parity among all neighborhoods. New developments should take community history into account when creating their projects. To achieve a more equitable city, some neighborhoods will have different needs to meet that goal. Our area is in critical need of retail and commercial amenities, especially healthy food options. This should also be taken into consideration in future plans.

The large density of the project and the lack of retail or commercial space is alarming. This riverfront location should demand more scrutiny and address the concerns the community has.

We bought this home to start a family in. We bought this home as a safe place to raise children. We bought this home as a fun area to walk our dog to shops and restaurants. We are planning our future around this home and don't want your development to take that away from us. I hope to see the following items addressed as this application moves through the process.

Best,
Jamison and Bradley Beskau
2336 River Pointe Cir
Minneapolis, MN 55411

[EXTERNAL] This email originated from outside of the City of Minneapolis. Please exercise caution when opening
--

09/15/2020

City of Minneapolis Planning Commission
Attn: Mei Ling Smith, Senior Planner
350 South 5th St.
Minneapolis, MN 55415

RE: Proposed Redevelopment of the Broadway Pizza Property at 2025 W. River Road N.

Dear Ms. Smith and Commissioners:

As a homestead property owner of the Hawthorne neighborhood I'm writing to oppose the plans as currently proposed by Schafer Richardson to develop the Broadway Pizza site at 2025 West River Parkway N.

I live at Riverview Townhomes, a 29 unit owner occupied residential townhome development which is two blocks north of the proposed site and its closest neighbor. Riverview has been a stable residential development within the Hawthorne neighborhood for the past 16 years.

Within that timeframe, I've served as a City Council appointee to the Capital Long-Range Improvement Committee-CLIC for three terms representing more than one Councilmember. I currently represent the Park Board on the Upper Harbor Terminal Development Community Advisory Committee. I've spent a considerable amount of time in the discussion and decision processes that help define the goals of the city, following the various established plans, rules and regulations of the City of Minneapolis.

After reviewing the Schafer Richardson land use proposal I have the following concerns:

- The proposal as written does not meet the goals of the 2040 Plan. For example, the Schafer Richardson proposal states at #5 of their Conditional Use Permit regarding the 2040 Plan "... the development has incorporated in the most beneficial ways possible." Possible being the operative word. They refer to #87 of the 2040 Plan, Northside, by increasing access to affordable housing options in neighborhoods...

Policy #1 of the 2040 Plan clearly states: Increase the supply of housing and its diversity of location and types. Areas of our city that lack housing choice today were built that way intentionally... Today "to address these issues, the City of Minneapolis will expand opportunities to increase the housing supply in a way that meets the changing needs and desires. This means allowing more housing options, especially in areas that currently lack housing choice..."

The policy of the 2040 Plan as I read it is misleading by Schafer Richardson. The Hawthorne neighborhood, the 55411 zip code where the proposed all affordable housing development is to be constructed, has not had any market rate housing built in the past decade. In fact, according to the Twin Cities Multifamily Market, New Rental Production in the Central Cities by Neighborhood or District 2010-2018 shows the Near North is the only area in Minneapolis that has not built market rate housing. According to the study, only affordable housing has been built.

Only when a neighborhood has a range of housing options and amenities will it be stable and healthy. To make any change at all, it's also important to provide a path to homeownership, which this project does not promote. By restricting housing development for a neighborhood to only low income, the persistent effect is that we locate low income residents into certain neighborhoods with few job opportunities due to few businesses operating. This is a vicious cycle that the Planning Commission must be mindful of as it makes decisions about where affordable housing is placed in the City.

A mixed use development, with affordable and market rate housing as well as commercial space for a sit down restaurant that the community has requested, would be a more suitable option for the site.

- The site to be redeveloped is in the Mississippi River Critical Overlay District and the Shoreland Overlay District. Currently and when Schafer Richardson applied, building height restrictions are in place at 35 feet. Schafer Richardson has asked for a conditional use permit to build at a height more than double that. There are reasons for this regulation which is in place to preserve the health of the Mississippi River. This is not located in the Urban Core, where height is determined by the local government's underlying zoning requirements. Regulations regarding the built form tend to be in conflict and cause confusion with the existing river front rules. Too often policy makers turn the other way and give a pass when it is more advantageous to the developer. This height request by Schafer Richardson is not even close, tiered or not. Why start a precedent up and down the river front especially when we're in the process of working to develop the Upper Harbor Terminal.
- A traffic study was completed and reported to be within acceptable guidelines. The problem is it was conducted during a pandemic when the traffic is substantially less. The corner of Broadway and W. River Road N. is an extremely busy intersection and is a notable crash site. Extra precaution should be taken for the safety of children. This is not the best site for a high density housing project. It's really more suited for commercial space.

Finally, since the residents of Riverview Townhomes are the close residential neighbor to this project we only found out about it through hearsay. Neither Schafer Richardson nor the Hawthorne Neighborhood Council Board reached out to us to ask for our support or even have a discussion about the project. We attended meetings when we found out about them, but were not allowed to speak. We wrote several letters to the Hawthorne Board stating our concerns and asking if they planned to support the project. We did not receive a response, only that this was a matter between a private developer and a private seller. They asked that we not contact them any further about this, even though they represent us. It felt like a done deal without public input.

Because of its location, this is a project that has to be carefully considered since it will be a part of the river front for a long time. A mixed income, mixed use facility to include affordable housing, market rate housing and commercial space or a fully commercial property would receive wide support. This seems to be the rental housing standard going forward, and in an area where only affordable housing has been built in the last ten years, wealth building is an important goal for this neighborhood.

Susan Pilarski
2344 River Pointe Circle, Minneapolis, MN 55411