

CPED STAFF REPORT

Prepared for the City Planning Commission CPC Agenda Item #8 November 16, 2020 PLAN11728

LAND USE APPLICATION SUMMARY

Property Location: 188 ½ Glenwood Avenue and 151 Holden Street North

Project Name: Shelby Commons

Prepared By: Hilary Dvorak, Principal City Planner, (612) 673-2639

Applicant: Woda Cooper Development, Inc.

Project Contact: Mike Krych with BKV Group

Request: To construct a new four-story, 46-unit residential development.

Dwelling Units	46 dwelling units
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Required Applications:

Conditional Use Permit	To allow a 17-space surface parking lot located in the DP Downtown Parking Overlay District.
Site Plan Review	For a new four-story, 46-unit residential building.
Preliminary and Final Plat	To create a new lot and a new outlot.

SITE DATA

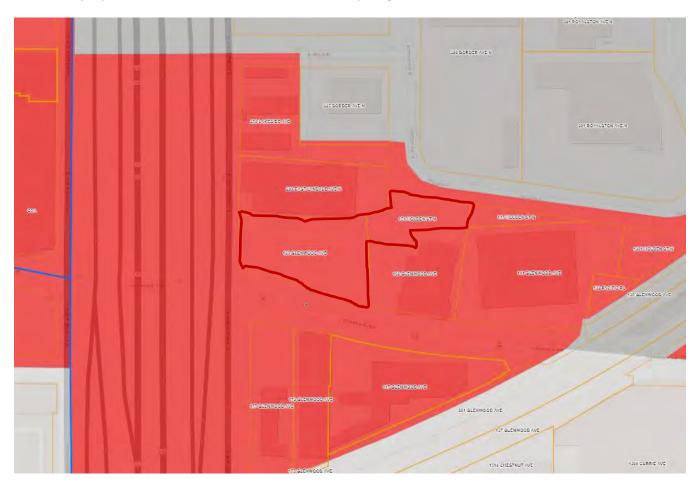
Existing Zoning	C3A Community Activity Center District DP Downtown Parking Overlay District	
Lot Area	30,226 square feet / .69 acres	
Ward(s)	5	
Neighborhood(s)	North Loop	
Future Land Use	Community Mixed use	
Goods and Services Corridor	Glenwood Avenue	
Built Form	Transit 30	

Date Application Deemed Complete	October 15, 2020	Date Extension Letter Sent	Not applicable
End of 60-Day Decision Period	December 14, 2020	End of 120-Day Decision Period	Not applicable

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The site is located on the northeast corner of Glenwood Avenue and East Lyndale Avenue North. The site is vacant. It has historically been used for open storage.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The site is surrounded by a mixture of commercial and residential properties. The site is located in the North Loop neighborhood.



PROJECT DESCRIPTION. The applicant is proposing to construct a new four-story, 46-unit residential development on the site. This will be an affordable housing project. There will be a 17-space surface parking lot located towards the back of the building and 33 parking spaces will be provided underneath the building. Access to the site will be from Glenwood Avenue. There will be an outdoor play area for children located towards the back of the building in addition to a community garden space. The exterior building materials include corrugated metal, fiber cement panel, brick and glass.

The site is located in the DP Downtown Parking Overlay District. Accessory surface parking lots require a conditional use permit in the overlay district. The applicant is proposing to have a 17-space surface parking lot on the site.

Site plan review is required as the proposed development contains more than 10 dwelling units.

The current site also includes the property located at 151 Holden Street North. In the future, the City of Minneapolis would like to extend Border Avenue North to Glenwood Avenue. In order to do this, the property boundaries of 188 ½ Glenwood Avenue and 155 Holden Street North would need to be modified. As part of this

application, the properties will be replatted to accommodate the future needs for the street. There will be one new lot and one new outlot created through the plat.

When Border Avenue North is extended to Glenwood Avenue, the hope is that the proposed driveway leading to Glenwood Avenue would be eliminated and a new curb cut would be constructed to Border Avenue North. CPED and the City's Public Works Department is in conversations with the applicant about this.

RELATED APPROVALS. Not applicable.

PUBLIC COMMENTS. No public comment letters have been submitted for this development. Any correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to allow a 17-space surface parking lot located in the DP Downtown Parking Overlay District based on the following findings:

- 1. The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.
 - The establishment of a surface parking lot will not be detrimental to or endanger the public health, safety, comfort or general welfare. The site has historically been used for open storage. The proposed surface parking lot will be located towards the back of the building. If and when Border Avenue North is extended to Glenwood Avenue, the parking lot will be buffered from it by a community garden and landscaping.
- The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.
 - The establishment of a surface parking lot will not be injurious to the use and enjoyment of other property in the vicinity or impede the normal or orderly development and improvement of surrounding property. The proposed surface parking lot will be located towards the back of the building. The proposed surface parking lot will be screened by the building itself, a community garden and landscaping
- 3. Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.
 - The Public Works Department has reviewed the preliminary plans and will review the final plans for compliance with standards related to access and circulation, drainage, and sewer/water connections. The applicant will be required to continue to work closely with the Public Works Department, the Plan Review Section of CPED and the various utility companies during the duration of the development to ensure that all procedures are followed and that the development complies with all city and other applicable requirements. The applicant is aware that the final plans are expected to incorporate any applicable comments or modifications as required by the Public Works Department.
- 4. Adequate measures have been or will be taken to minimize traffic congestion in the public streets.
 - The parking requirement for the development is one space per dwelling unit or 46 spaces. However, the site is located within a quarter-mile from a bus transit stop with midday service headways of 15 minutes of less. Given this, the development qualifies for a 50 percent parking reduction. The parking requirement for the development is 23 spaces. The applicant is proposing to provide 47 parking spaces within the building.

5. The conditional use is consistent with the applicable policies of the comprehensive plan.

The proposed use would be consistent with the applicable guidance and policies of *Minneapolis 2040 (2020)*:

Future Land Use	Guidance	Staff Comment
Community Mixed Use	Large-scale mixed use development is encouraged throughout these areas, with commercial uses fronting on major streets. Commercial retail spaces are typically smaller in order to generate pedestrian activity, and are often a destination for customers coming from outside of the market area. Active uses that are accessible to the general public such as office, food service, retail, or medical establishments are required at the street level; therefore single-use residential development is not permitted. Contiguous expansion of commercial zoning is allowed.	The applicant is proposing a single-use residential development. The City's new comprehensive plan, Minneapolis 2040, went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not have a base zoning district that matches the height and density called for on the Built Form map. The City will address any conflicts by amending the zoning code with the adoption of Built Form Overlay Districts. These overlay districts will contain performance standards that match the scale of development called for in the adopted Built Form Map. This work is currently underway and is expected to be complete by the end of 2020.
Goods and Services Corridor	Guidance	Staff Comment
Glenwood Avenue	Goods and Services Corridors serve two purposes: 1) To indicate where commercial uses should front in relation to properties guided for commercial future land uses, and 2) In addition to the guidance for the mixed use land use categories found in this section, Goods and Services Corridors identify where the establishment or expansion of commercial uses can be considered. Properties immediately adjacent to a Goods and Services Corridor may be considered for commercial activity, allowing for uses similar in scale and scope to the Neighborhood and Corridor Mixed Use categories.	The site is zoned C3A Community Activity Center District. No commercial uses are proposed in the development. The City's new comprehensive plan, <i>Minneapolis</i> 2040, went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not

Built Form Guidance	Guidance	have a base zoning district that matches the height and density called for on the Built Form map. The City will address any conflicts by amending the zoning code with the adoption of Built Form Overlay Districts. These overlay districts will contain performance standards that match the scale of development called for in the adopted Built Form Map. This work is currently underway and is expected to be complete by the end of 2020. Staff Comment
Transit 30	New and remodeled buildings in the Transit 30 district should reflect a variety of building types on both moderate and large sized lots. Upper floors of taller buildings should be set back to increase access to light and air. Building heights should be 10 to 30 stories. Building heights should be at least 10 stories in order to best take advantage of the access to transit, jobs, and goods and services provided by the Transit 30 district. Requests to exceed 30 stories will be evaluated on the basis of whether or not a taller building is a reasonable means for further achieving Comprehensive Plan goals.	The applicant is proposing to construct a four-story building on the site. The building would be short of the minimum requirement of 10 stories in the Transit 30 built form district. The City's new comprehensive plan, Minneapolis 2040, went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not have a base zoning district that matches the height and density called for on the Built Form map. The City will address any conflicts by amending the zoning code with the adoption of Built Form Overlay Districts. These overlay districts will contain performance standards that match the scale of development called for in the adopted Built Form Map. This work is currently underway and is expected to be complete by the end of 2020.

The following goals from *Minneapolis 2040 (2020)* apply to this proposal:

- Goal 3. Affordable and accessible housing: In 2040, all Minneapolis residents will be able to afford and access quality housing throughout the city.
- Goal 6. High-quality physical environment: In 2040, Minneapolis will enjoy a high-quality and distinctive physical environment in all parts of the city.
- Goal 9. Complete neighborhoods: In 2040, all Minneapolis residents will have access to employment, retail services, healthy food, parks, and other daily needs via walking, biking, and public transit.

The following policies and action steps from *Minneapolis 2040 (2020)* apply to this proposal:

Policy 6. Pedestrian-Oriented Building and Site Design: Regulate land uses, building design, and site design of new development consistent with a transportation system that prioritizes walking first, followed by bicycling and transit use, and lastly motor vehicle use.

- p. Discourage the establishment of and minimize the size of surface parking lots. Mitigate the negative effect of parking lots through screening, landscaping, minimizing curb cuts, sufficient number of down-cast, glare-free light fixture, and other measures.
- q. Encourage the design of parking areas in ways that minimize their contribution to the urban heat island.
- 6. The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.

If the requested land use applications are approved, the proposal will comply with all provisions of the C3A Community Activity Center District and the DP Downtown Parking Overlay District.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

Applicable Standards of Chapter 530, Site Plan Review

BUILDING PLACEMENT AND DESIGN

Building placement – Requires alternative compliance

- The first floor of the building is located within eight feet of the front property line along both Glenwood Avenue and East Lyndale Avenue North except the entryway is set back 23 feet from the front property line along Glenwood Avenue. Alternative compliance is required.
- The placement of the building reinforces the street wall, maximizes natural surveillance and visibility, and facilitates pedestrian access and circulation.
- The area between the building and the front property lines includes amenities such as landscaping and a placeholder for public art.
- All on-site accessory parking is located to the rear of the site.

Principal entrances – *Meets requirements*

• The proposed project would comply with the principal entrances standards as the principal entrance faces Glenwood Avenue.

Visual interest – *Meets requirements*

• The proposed project would comply with the visual interest standards.

Exterior materials – *Meets requirements*

- The applicant is proposing brick, glass, metal panel, fiber cement (≤ 5/8") and fiber cement siding as the building's primary exterior materials. Exterior material changes at a later date may require review by the Planning Commission and an amendment to the site plan review.
- Each elevation would comply with the City's durability standards for exterior materials.
- The application is consistent with the City's policy of allowing no more than three exterior materials per elevation, excluding windows, doors, and foundation materials.
- Plain face concrete block is not proposed along any public streets, sidewalks, or adjacent to a residence or office residence district.
- The exterior materials and appearance of the rear and side walls of the building are similar to and compatible with the front of the building.

Percentage of Exterior Materials per Elevation

Material	Allowed Max	North	South	East	West
Brick (face)	100%	2%	3%	6%	0%
Glass	100%	32%	33%	26%	30%
Metal Panel	75%	54%	47%	36%	54%
Fiber Cement (≤ 5/8")	30%	0%	0%	25%	15%
Fiber Cement Siding	30%	12%	17%	7%	1%

Windows - Meets requirements

• The proposed project would comply with the minimum window requirements.

Window Requirements for Residential Uses

Floor	Code		Proposed	
1st floor facing Glenwood Avenue	20% minimum	264 sq. ft.	45%	594 sq. ft.
2nd floor and above facing Glenwood Avenue	10% minimum	182 sq. ft.	More t	han 10%
1st floor facing East Lyndale Avenue North	20% minimum	155 sq. ft.	49%	384 sq. ft.
2nd floor and above facing East Lyndale Avenue North	10% minimum	107 sq. ft.	More than 10%	
1st floor facing the on-site surface parking lot	20% minimum	206 sq. ft.	37%	378 sq. ft.
2nd floor and above facing the on-site surface parking lot	10% minimum	142 sq. ft.	More t	han 10%

Ground floor active functions – *Meets requirements*

• The proposed project would comply with the ground floor active functions requirements as 100 percent of the ground floor facing both Glenwood Avenue and East Lyndale Avenue North contains active functions.

Roof line – *Meets requirements*

The principal roof line of the building would be flat which is similar to that of surrounding buildings.

Parking garages – Meets requirements

• The proposed parking garage would comply with the applicable site plan review standards as it is located underneath the building.

ACCESS AND CIRCULATION

Pedestrian access – Meets requirements

• There would be clear and well-lit walkways at least four feet in width connecting building entrances to the adjacent public sidewalk and on-site parking facilities.

Transit access – *Not applicable*

• No transit shelters are proposed as part of this development.

Vehicular access – *Meets requirements*

• The proposed project would comply with the vehicular access requirements. As proposed, all vehicle traffic will enter and exit the site from a curb cut along Glenwood Avenue.

LANDSCAPING AND SCREENING

General landscaping and screening – *Meets requirements*

- The proposed project would comply with the general landscaping and screening requirements.
- The remainder of the required landscaped area is covered with turf grass, native grasses, perennial flowering plants and ornamental trees.

Landscaping and Screening Requirements

Requirement	Required	Proposed
Lot Area		30,226 sq. ft.
Building Footprint		12,954 sq. ft.
Area Not Covered by Buildings		17,272 sq. ft.
Landscaped Area	3,455 sq. ft.	6,611 sq. ft.
Canopy Trees (1:500 sq. ft.)	7 trees	8 trees
Shrubs (1:100 sq. ft.)	35 shrubs	178 shrubs

Parking and loading landscaping and screening – Meets requirements

 The proposed project would comply with the applicable parking and loading landscaping and screening requirements.

Additional landscaping requirements – Meets requirements

• The project appears to comply with the additional landscaping requirements in sections 530.180, 530.190, 530.200, and 530.210 of the zoning code.

ADDITIONAL STANDARDS

Concrete curbs and wheel stops – *Meets requirements*

• The parking lot would be defined by a six-inch by six-inch continuous concrete curb.

Site context – *Meets requirements*

• The proposed project would comply with the site context requirements.

Crime prevention through environmental design – *Meets requirements*

• The proposed project would comply with crime prevention through environmental design (CPTED) standards.

Historic preservation – *Not applicable*

• This site is neither historically designated or located in a designated historic district, nor has it been determined to be eligible for designation.

Applicable Regulations of the Zoning Ordinance

The proposed use is permitted in the C3A Community Activity Center District. However, a surface parking lot is a conditional use in the DP Downtown Parking Overlay District.

Off-street Parking and Loading – *Meets requirements*

• The proposed project complies with the applicable vehicle parking, bicycle parking, and loading requirements.

Vehicle Parking Requirements Per Use (Chapter 541)

Use	Minimum	Reductions	Minimum	Maximum	Proposed
Residential Dwellings	46	Transit Incentives (23)	23	78	47
	46	(23)	46	78	47

Bicycle Parking Requirements (Chapter 541)

Use	Minimum	Short-Term	Long-Term	Proposed
Residential Dwellings	23		Not less than 90%	21 Long-Term 16 Short-Term
	23		21	21 Long-Term 16 Short-Term

Loading Requirements (Chapter 541)

Use	Loading Requirement	Loading Spaces	Proposed
Residential Dwellings	None	None	None

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None None	None
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Building Bulk and Height – *Meets requirements*

• The proposed project would meet the applicable bulk and height requirements.

Building Bulk and Height Requirements

Requirement Code		Bonuses	Total	Proposed
Lot Area		-	-1	30,226 sq. ft. / .69 acres
Gross Floor Area				53,227 sq. ft.
Max. Floor Area Ratio	2.7	-	2.7	1.76
Max. Building Height	4 stories or 56 ft., whichever is less		4 stories or 56 ft., whichever is less	4 stories or 49 ft.

Lot and Residential Unit Requirements – *Meets requirements*

- The proposed project would meet the applicable lot and residential unit requirements.
- The proposed dwelling units meet the minimum gross floor area requirement of 500 square feet per unit.
- The application is subject to the delayed phase-in for smaller projects with 20-49 units and the project is not subject to the requirements of Inclusionary Zoning per section 535.90(c)(1) of the zoning code.

Lot and Residential Unit Requirements Summary

Requirement	Code	Proposed
Lot Area	5,000 sq. ft. min.	30,226 sq. ft.
Lot Width	40 ft. min.	206 ft. on Glenwood Avenue 115 ft. on East Lyndale Avenue North
Impervious Surface Area	100% max.	78%
Lot Coverage	100% max.	46%
Dwelling Units (DU)		46 DUs
Net Residential Area		43,124 sq. ft.

Yard Requirements – *Meets requirements*

• The project would meet the applicable yard requirements.

Minimum Yard Requirements

Setback	Zoning District	Overriding Regulations	Total Requirement	Proposed
Front (Glenwood Avenue)	0 ft.		0 ft.	Between 2 and 23 ft.
Front (East Lyndale Avenue North)	0 ft.		0 ft.	Between 3 and 7 ft.

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Interior Side (North)	0 ft.	 0 ft.	Between 7 and 59 ft.
Interior Side (East)	0 ft.	 0 ft.	Between 32 and 42 ft.

Signs – Meets requirements with Conditions of Approval

- All signs are subject to Chapter <u>543</u>, On-Premise Signs. The applicant will be required to submit a separate sign permit application for any signage that is proposed.
- In the C3A Community Activity Center District there can be one square foot of signage for every one linear
 foot of primary building wall. Wall signs are limited to 180 square feet in size. Projecting signs are limited to
 48 square feet in size. There is no height limitation for either wall signs or projecting signs. Freestanding
 monument signs are limited to 80 square feet in size and can be no taller than 8 feet. The zoning code limits
 the number of freestanding signs on a zoning lot to one.
- The applicant has indicated that they would like to install a monument sign towards the corner of Glenwood Avenue and East Lyndale Avenue North and a wall sign on the building; however, no specific signs have been proposed.

Screening of Mechanical Equipment – *Meets requirements*

- Mechanical equipment is subject to the screening requirements of <u>Chapter 535</u>, Regulations of General Applicability.
- The applicant is proposing to locate a transformer towards the back of the building. The transformer will be screened from the public street by the building itself.

Refuse Screening – Meets requirements

- Refuse and recycling storage containers are subject to the screening requirements in <u>Chapter 535</u>, Regulations of General Applicability.
- All refuse and recycling storage containers are located within the building.

Lighting – *Meets requirements*

- Existing and proposed lighting must comply with Chapter 535, Regulations of General Applicability.
- The project is consistent with the applicable lighting requirements.

Fences – *Not applicable*

- Fences must comply with the requirements in Chapter 535, Regulations of General Applicability.
- The applicant is not proposing to have any fencing on the site.

Specific Development Standards – *Not applicable*

• The uses within the development are not subject to specific development standards in <u>Chapter 536</u>, Specific Development Standards.

DP Downtown Parking Overlay District Standards – Requires conditional use permit

• The site is located in the DP Downtown Parking Overlay District. Accessory surface parking lots require a conditional use permit in the overlay district. The applicant is proposing to have a 17-space surface parking lot on the site. The conditional use permit has been evaluated above.

Applicable Policies of the Comprehensive Plan

The proposed use would be consistent with the applicable guidance and policies of *Minneapolis 2040 (2020)*:

Future Land Use	Guidance	Staff Comment
Community Mixed Use	Large-scale mixed use development is encouraged throughout these areas, with commercial uses fronting on major streets. Commercial retail spaces are typically smaller in order to generate pedestrian activity, and are often a destination for customers coming from outside of the market area. Active uses that are accessible to the general public such as office, food service, retail, or medical establishments are required at the street level; therefore single-use residential development is not permitted. Contiguous expansion of commercial zoning is allowed.	The applicant is proposing a single-use residential development. The City's new comprehensive plan, Minneapolis 2040, went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not have a base zoning district that matches the height and density called for on the Built Form map. The City will address any conflicts by amending the zoning code with the adoption of Built Form Overlay Districts. These overlay districts will contain performance standards that match the scale of development called for in the adopted Built Form Map. This work is currently underway and is expected to be complete by the end of 2020.
Goods and Services Corridor	Guidance	Staff Comment
Glenwood Avenue	Goods and Services Corridors serve two purposes: 1) To indicate where commercial uses should front in relation to properties guided for commercial future land uses, and 2) In addition to the guidance for the mixed use land use categories found in this section, Goods and Services Corridors identify where the establishment or expansion of commercial uses can be considered. Properties immediately adjacent to a Goods and Services Corridor may be considered for commercial activity, allowing for uses similar in scale and scope to the Neighborhood and Corridor Mixed Use categories.	The site is zoned C3A Community Activity Center District. No commercial uses are proposed in the development. The City's new comprehensive plan, <i>Minneapolis</i> 2040, went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not have a base zoning district that

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Built Form	Guidance	Staff Comment
Transit 30	New and remodeled buildings in the Transit 30 district should reflect a variety of building types on both moderate and large sized lots. Upper floors of taller buildings should be set back to increase access to light and air. Building heights should be 10 to 30 stories. Building heights should be at least 10 stories in order to best take advantage of the access to transit, jobs, and goods and services provided by the Transit 30 district. Requests to exceed 30 stories will be evaluated on the basis of whether or not a taller building is a reasonable means for further achieving Comprehensive Plan goals.	The applicant is proposing to construct a four-story building on the site. The building would be short of the minimum requirement of 10 stories in the Transit 30 built form district. The City's new comprehensive plan, Minneapolis 2040, went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not have a base zoning district that matches the height and density called for on the Built Form map. The City will address any conflicts by amending the zoning code with the adoption of Built Form Overlay Districts. These overlay districts will contain performance standards that match the scale of development called for in the adopted Built Form Map. This work is currently underway and is expected to be complete by the end of 2020.

The following policies and action steps from *Minneapolis 2040 (2020)* apply to this proposal:

Policy 1. Access to Housing: Increase the supply of housing and its diversity of location and types.

a. Allow housing to be built in all areas of the city, except in Production and Distribution areas.

- c. Allow multifamily housing on public transit routes, with higher densities along high-frequency routes and near METRO stations.
- f. Encourage inclusion of units that can accommodate families in new and rehabilitated multifamily housing developments.

Policy 5. Visual Quality of New Development: Ensure a high-quality and distinctive physical environment in all parts of the city through building and site design requirements for both large and small projects.

- a. Allow and encourage a variety of architectural styles.
- c. Ensure that exterior building materials are durable, sustainable, create a lasting addition to the built environment, and contribute positively to the public realm and reflect existing context.
- d. Require that the appearance and materials of the rear and side walls of new buildings are similar to and compatible with the front of the building.
- e. Require adequate distribution of windows and architectural features in order to create visual interest.
- h. Promote an attractive environment by minimizing visual clutter and confusion caused by a proliferation of signage; ensuring that signage is appropriately scaled to the pedestrian experience.
- j. Require the screening of utilities, communication, transformers, and other service connections to buildings. Burying connections and lines is encouraged.
- o. Regulate setbacks, orientation, pattern, materials, height, and scale of small-scale residential buildings to ensure consistency with built-form guidance and existing context.
- s. Balance visual quality of new development, including articulation of buildings, with energy efficiency of new buildings.

Alternative Compliance

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for <u>alternative compliance</u>. Alternative compliance is requested for the following requirements:

Standard	Description	Staff Recommendation
Building	The first floor of the building is located	The building has been designed to
Placement	within eight feet of the front property line	follow the curb line along
	along both Glenwood Avenue and East	Glenwood Avenue. Where the
	Lyndale Avenue North except the entryway	street jogs; so, does the building.
	is set back 23 feet from the front property	This pushes the entryway back
	line along Glenwood Avenue.	from the property line. The
		applicant is proposing to have
		landscaping and bike racks located
		in this space. Staff recommends
		granting alternative compliance.

PRELIMINARY/FINAL PLAT

The Department of Community Planning and Economic Development has analyzed the application for a preliminary and final plat, based on the following <u>findings</u>:

1. The subdivision is in conformance with these land subdivision regulations, the applicable regulations of the zoning ordinance and policies of the comprehensive plan.

Subdivision Regulations:

The subdivision is in conformance with the design requirements of the land subdivision regulations.

Zoning Ordinance:

The subdivision is in conformance with the applicable regulations of the zoning ordinance.

Comprehensive Plan:

The proposed project would be consistent with the applicable guidance and policies of <u>Minneapolis 2040</u> (2020):

Future Land Use	Guidance	Staff Comment
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section, Goods and Services Corridors identify where the establishment or expansion of commercial uses can be considered. Properties immediately adjacent to a Goods and Services Corridor may be considered for commercial activity, allowing for uses similar in scale and scope to the Neighborhood and Corridor Mixed Use categories.

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Built Form

Guidance

Transit 30

New and remodeled buildings in the Transit 30 district should reflect a variety of building types on both moderate and large sized lots. Upper floors of taller buildings should be set back to increase access to light and air. Building heights should be 10 to 30 stories. Building heights should be at least 10 stories in order to best take advantage of the access to transit, jobs, and goods and services provided by the Transit 30 district. Requests to exceed 30 stories will be evaluated on the basis of whether or not a taller building is a reasonable means for further achieving Comprehensive Plan goals.

Staff Comment

The applicant is proposing to construct a four-story building on the site. The building would be short of the minimum requirement of 10 stories in the Transit 30 built form district. The City's new comprehensive plan, Minneapolis 2040, went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not have a base zoning district that matches the height and density called for on the Built Form map. The City will address any conflicts by amending the zoning code with the adoption of Built Form Overlay Districts. These overlay districts will contain

performance standards that match
the scale of development called
for in the adopted Built Form Map.
This work is currently underway
and is expected to be complete by
the end of 2020.

The following policies and action steps from *Minneapolis 2040* (2020) apply to this proposal:

Policy 10. Street Grid: Restore and maintain the traditional street and sidewalk grid.

- a. Explore options to restore the street grid or add new streets and sidewalks to larger blocks or tracts of land as part of new development or redevelopment.
- d. Maintain the City's ownership of streets in order to preserve the network of city streets and arterials.
- e. Restore the street grid when appropriate, taking advantage of opportunities as they arise in conjunction with new development and through capital projects. Explore options to proactively dedicate capital improvement funds to reconnect the street grid, so resources are already available when opportunities arise. Where restoring auto access is no longer feasible or aligned with other City plans, the City will pursue restoring streets for use by pedestrians, bicyclists, or transit users.
- The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.
 - The applicant is proposing to combine several platted lots into one platted lot and one outlot. The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.
- 3. All land intended for building sites can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.
 - The land intended for building can be used safely without endangering the users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.
- 4. The lot arrangement is such that there will be no foreseeable difficulties, for reasons of topography or other conditions, in securing building permits and in providing driveway access to buildings on such lots from an approved street. Each lot created through subdivision is suitable in its natural state for the proposed use with minimal alteration.
 - The parcels created by this application presents no foreseeable difficulties for development. No significant alterations to the land appear necessary.
- 5. The subdivision makes adequate provision for stormwater runoff, and temporary and permanent erosion control in accordance with the rules, regulations and standards of the city engineer and the requirements of these land subdivision regulations. To the extent practicable, the amount of stormwater runoff from the site after development will not exceed the amount occurring prior to development.
 - The Public Works Department has reviewed the preliminary plans and will review the final plans for compliance with standards related to access and circulation, drainage, and sewer/water connections. The applicant will be required to continue to work closely with the Public Works Department, the Plan Review

Department of Community Planning and Economic Development

PLAN11728

Section of CPED and the various utility companies during the duration of the development to ensure that all procedures are followed and that the development complies with all city and other applicable requirements. The applicant is aware that the final plans are expected to incorporate any applicable comments or modifications as required by the Public Works Department.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by Woda Cooper Development, Inc. for the properties located at 188 ½ Glenwood Avenue and 151 Holden Street North:

A. Conditional Use Permit.

Recommended motion: <u>Approve</u> the conditional use permit to allow a 17-space surface parking lot located in the DP Downtown Parking Overlay District, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

B. Site Plan Review.

Recommended motion: <u>Approve</u> the site plan review for a new four-story, 46-unit residential building, subject to the following conditions:

- 1. All site improvements shall be completed by November 16, 2022, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
- 2. CPED staff shall review and approve the final site, elevation, landscaping, and lighting plans before building permits may be issued.
- 3. All signs shall comply with Chapter 543, On-Premise Sigs of the zoning code. All signage requires a separate permit from CPED.

C. Preliminary/Final Plat.

Recommended motion: **Approve** the application for a preliminary and final plat.

ATTACHMENTS

- 1. Written description and findings submitted by applicant
- 2. Civil and architectural plans
- 3. Correspondence





October 14, 2020

RE: Statement of Proposed Use of All Land to be Subdivided and Description of the Project

Woda Cooper Development, Inc. is submitting a Land Use Application for a proposed family development located at 188 ½ Glenwood Avenue, Minneapolis, MN 55405 in the North Loop neighborhood. The Land Use Application is for a 4-story apartment building, known as Shelby Commons, an affordable housing development. Shelby Commons will be developed by Woda Cooper Development, Inc. with Project for Pride in Living, Inc. (PPL) as a co-developer. This will provide for the development of 46 low-income housing units, all affordable. These units will consist of 11 one-bedroom units, 23 two-bedroom units, and 12 three-bedroom units.

PPL will be providing supportive services, including on-site case management, for 12 units. These supportive services are provided as opportunities to residents at their own will. Residents are not required to use these services. 5 of the supportive housing units will be one-bedroom units for persons with disability. PPL will use its Housing Supports contract to help subsidize the rents of these units. The remaining 7 supportive units will be set aside for high priority homeless with 4 of these units being one-bedroom units, and 3 being two-bedroom units. Shelby Commons has been awarded 12 project-based section 8 vouchers from the Minneapolis Public Housing Authority and is currently awaiting a response from Minnesota Housing Finance Agency and the Minneapolis Community Planning and Economic Development on its applications for 9% low income housing tax credits. Shelby Commons will use 7 of the 12 vouchers to subsidize the rents of the supportive units set aside for high priority homeless. The remaining 5 vouchers will be used on general occupancy units, 1 on a two-bedroom unit, and 4 on three-bedroom units.

Units will be available to residents at varying income levels, with some units set aside at 30%, 50%, 60%, and 80% of the HUD Multifamily Tax Subsidy Projects (MTSP) income limits. In addition to this, all units will be affordable to households at the 60% HUD MTSP income limits. This presents a great opportunity for low income households at or below the 80% income limit to find quality living at a rate which would be very affordable to such households.

Other amenity spaces Shelby Commons has to offer include a community room, community laundry, and a supportive services office. There will also be an outdoor playground for children along with community garden plots. Along Glenwood Avenue, Shelby Commons will feature a unique urban rain garden, and there will be an opportunity for public art, both on the sidewalk/entry plaza and on select portions of the building exterior facade. Because Shelby Commons is a large family development, we have provided parking at a ratio just over 1 to 1, with 47 total parking spaces, 32 of which are in an underground garage, and 15 of which are in a surface lot located at the rear of the building.

Woda Cooper Development is seeking to replat the two tax parcels at 188 ½ Glenwood Avenue. These parcels have tax identification numbers of 22-029-24-33-0053, and 22-029-24-33-0058. The city of Minneapolis is hoping to construct a Border Avenue Extension project to extend the existing

Border Avenue south to Glenwood Avenue. Woda Cooper Development intends to replat the land to the west of this as Lot 1, and the remaining land, the land of the proposed Border Avenue Extension and the land to the east, as Outlot A. As described previously, Lot 1 will be used for the Shelby Commons development. Outlot A will be designated as an outlot for the time being until the future proposed roadway extension proceeds, at which time it will be final platted.

Sincerely,
Parker See

Parker Zee

Assistant Vice President of Development and Data Manager

www.wodagroup.com



October 1, 2020

RE: Statement on the Conditional Use Permit for a 15-Space Accessory Surface Parking Lot

Woda Cooper Development, Inc. is submitting a Land Use Application for a proposed family development located at 188 ½ Glenwood Avenue, Minneapolis, MN 55405 in the North Loop neighborhood. The Land Use Application is for a 4-story apartment building, known as Shelby Commons, an affordable housing development. Shelby Commons will be developed by Woda Cooper Development, Inc. with Project for Pride in Living, Inc. (PPL) as a co-developer. This will provide for the development of 46 low-income housing units, all affordable. These units will consist of 11 one-bedroom units, 23 two-bedroom units, and 12 three-bedroom units.

Woda Cooper Development respectfully requests a Conditional Use Permit for a 15-space accessory surface parking lot at the Shelby Commons development. This development is located in the C3A Community Activity Center District as well as the DP Downtown Parking Overlay District. The DP district requires a Conditional Use Permit for any accessory parking lot. The DP district has two specific provisions that any accessory parking lot must adhere to:

- 1. The parking lot shall be located on the same zoning lot as the principal use served.
- 2. The number of parking spaces shall not exceed twenty (20) spaces.

The 15-space accessory surface parking lot meets both of these requirements and is not in violation of the underlying C3A zoning district.

This surface parking lot is located on the interior of the property, behind the proposed apartment building, so this will not be detrimental to public health, safety, comfort, or general welfare. This surface lot will provide a benefit to the community. Shelby Commons is a large multifamily development with 46 total units. With this lot, there will be a total of 47 parking spaces (32 will be in an underground garage) that may be used by residents. Despite being near convenient transportation options in a large city, it is anticipated that many low-income residents will need cars and will benefit from the inclusion of these spaces. Some households may not have cars, but there will also be households that require 2 cars for their commuting needs. If residents have more convenient parking available just outside their home, they will not be forced to search for monthly parking spaces in other areas within the city of Minneapolis, and residents will not need to park on streets. This will prove to be very important, with many residents in the labor force who will require convenient access to parking.

Being a small, private surface lot only available to residents at Shelby Commons, this use will not impact the enjoyment of other property in the vicinity of the site. This use also will not intrude on the ability to develop adjacent property.

This development will have adequate utilities, access roads, drainage, and other facilities to provide for the surface parking lot. Access will already be provided to give access into the underground parking garage, and this will easily be extended right into the surface parking lot. Additionally, there

will be enough exterior lighting provided to ensure this surface parking lot is safe for all residents. No other facilities are needed for this use.

Adequate measures have been taken to minimize traffic congestion in the public streets. No access to the development has changed as a result of adding this surface parking lot, but the inclusion of extra spaces in the lot allows for more residents to park in this private lot. By parking at the development, these residents will not need to park on the street or in other lots, and residents will not have a need to temporarily stop on Glenwood Avenue for loading/unloading, which could result in a traffic lane being blocked.

Although this surface parking lot may detract from the need to use public transit, this conditional use is consistent with the Minneapolis 2040 plan. The comprehensive plan seeks to provide more affordable and accessible housing. This lot will be used for affordable housing and is beneficial to the large families that will live in Shelby Commons. The comprehensive plan also focuses on the need for jobs. Being able to use the spaces in this lot will allow residents to drive to jobs that may have been more difficult to access by walking, biking, or public transit. Being able to drive their own cars will significantly increase opportunities to find employment anywhere in Minneapolis.

Another goal of the comprehensive plan is to eliminate disparities. Residents at Shelby Commons are disadvantaged from an income standpoint and will be a very diverse community. By providing additional parking, more residents will have the ability thrive with access to amenities or jobs nearby that are most easily accessible by car. This will give all residents an opportunity to improve their wealth, helping to eliminate this income disparity.

For the required findings presented along with the adherence to the Downtown Parking Overlay District provisions, Woda Cooper Development believes that a 15-space surface parking lot will prove to be beneficial to the community as a whole, but will most significantly benefit the residents of Shelby Commons.

Thank you for your consideration of this Conditional Use Permit request.

Sincerely,

Parker Zee

Parker See

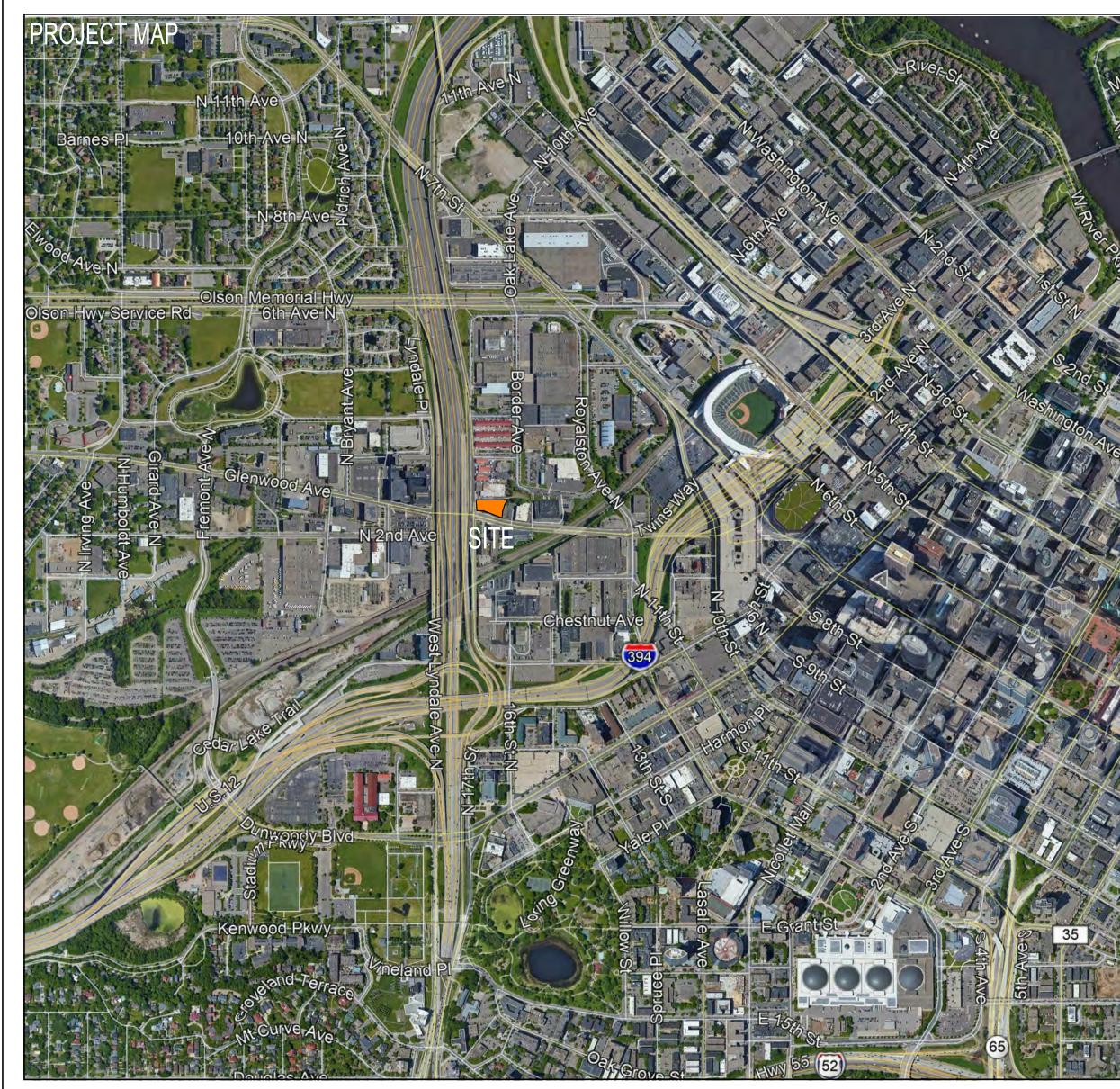
Assistant Vice President of Development and Data Manager



SHELBY COMMONS

188 1/2 GLENWOOD AVE, MINNEAPOLIS, MN 55401 LAND USE APPLICATION/PDR SUBMITTAL - 10/02/2020





002 1 003 1 004 2 005 2 006 2 007 2 008 2	Discipline 0 - GENERAL	Sheet Number	Sheet Name	2020-10-02 LAND USE APPLICATION	2020-10-02 PDR SUBMITTAL
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004 2 005 2 006 2 007 2 008 2	1 - SURVEYOR	1 of 1	PRELIMINARY PLAT	X	X
006 2 007 2 008 2	2 - CIVIL	C100	GENERAL NOTES AND LEGEND	Х	Х
007 2 008 2	2 - CIVIL	C200	SITE DEMOLITION PLAN	Х	Х
008 2	2 - CIVIL	C300	SWPPP GENERAL INFORMATION	Х	Х
	2 - CIVIL	C301	SWPPP - NARRATIVE	Х	Х
009 2	2 - CIVIL	C302	SWPPP - EXISTING CONDITIONS	Х	Х
	2 - CIVIL	C303	SWPPP - PROPOSED CONDITIONS	Х	Х
010 2	2 - CIVIL	C304	SWPPP DETAILS	Х	Х
011 2	2 - CIVIL	C400	GRADING AND DRAINAGE PLAN	Х	Х
012 2	2 - CIVIL	C500	UTILITY PLAN	Х	Х
013 2	2 - CIVIL	C600	SITE LAYOUT AND PAVING PLAN	Х	Х
014 2	2 - CIVIL	C700	CIVIL DETAILS	Х	Χ
015 2	2 - CIVIL	C701	CIVIL DETAILS	Х	Χ
016 3	3 - LANDSCAPE	L100	SITE LANDSCAPE PLAN	Х	Χ
017 4	4 - ARCH	A1-0	SITE PLAN	Х	Х
	4 - ARCH	A2-0.0	GARAGE PLAN	Х	
	4 - ARCH	A2-0.1	LEVEL 1	Х	
	4 - ARCH	A2-0.2	LEVEL 2	Х	
	4 - ARCH	A2-0.3	LEVEL 3	Х	
	4 - ARCH	A2-0.4	LEVEL 4	Х	
	4 - ARCH	A2-0.5	ROOF PLAN	Х	
	4 - ARCH	A3-0	ELEVATIONS	Х	Х
	4 - ARCH	A4-0	RENDERINGS	Х	
026 4	4 - ARCH	A5-0 E-011	SHADOW STUDY	Х	

PROJECT TEAM

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PROJECT TITLE

SHELBY COMMONS

ISSUE # DATE DESCRIPTION
1 10/02/2020 LAND USE APPLICATION

CERTIFICATION

CONSTRUCTION

DRAWN BY
CHECKED BY
COMMISSION NUMBER
2385.03

SHEET TITLE

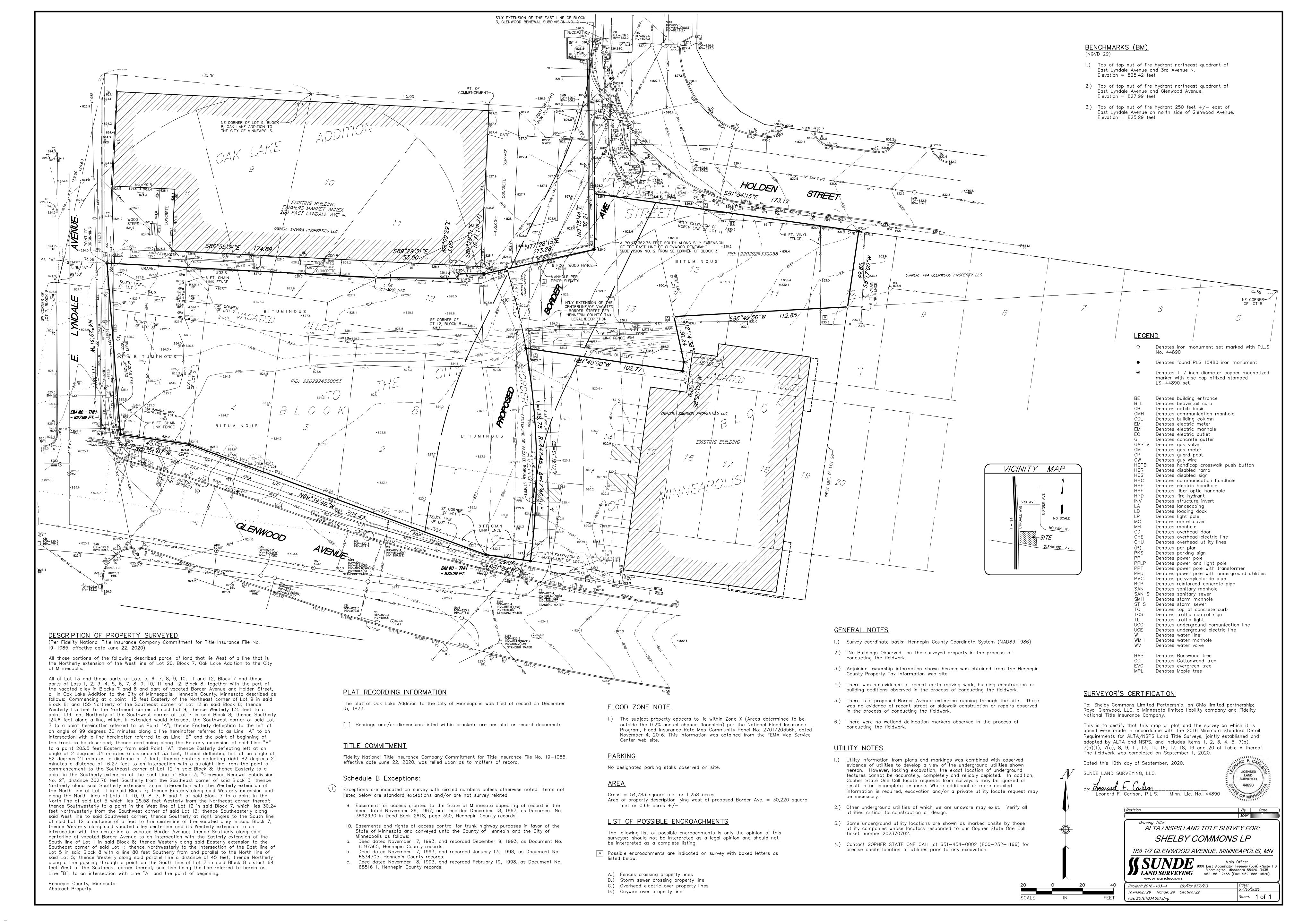
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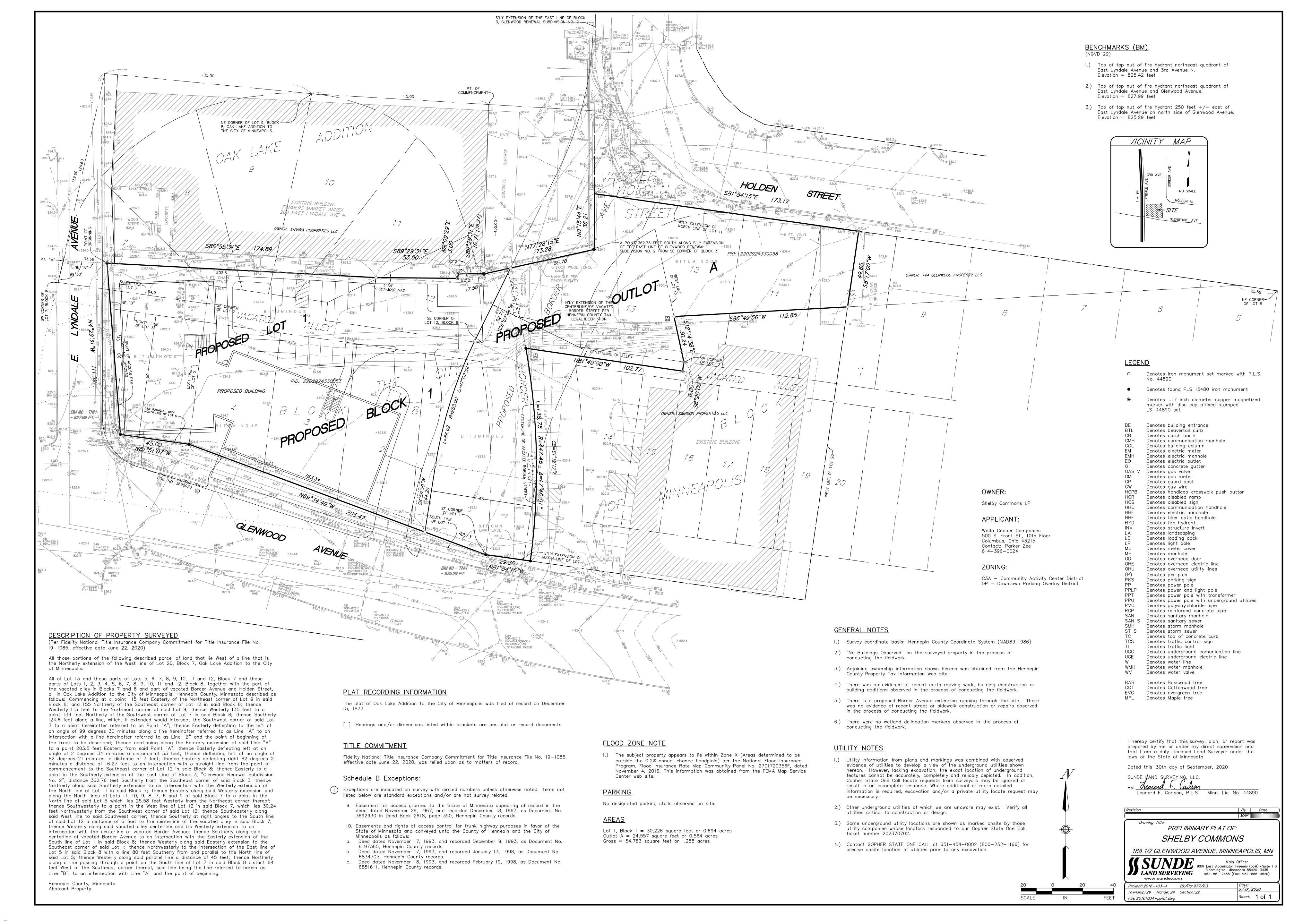
SHEET NUMBER

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EROSION CONTROL NOTES

- 1. GENERAL CONTRACTOR MUST CALL A CONSTRUCTION START 48 HOURS PRIOR TO ANY LAND DISTURBANCES (612) 673-3867. FAILURE TO DO SO MAY RESULT IN FINES, THE REVOCATION OF PERMIT AND A STOP WORK ORDER BEING ISSUED.
- 2. INSTALL PERIMETER EROSION CONTROL AT THE LOCATIONS SHOWN ON THE PLANS PRIOR TO THE COMMENCEMENT OF ANY LAND DISTURBANCE OR CONSTRUCTION ACTIVITIES. (HAY BALES ARE NOT AN ACCEPTABLE PERIMETER CONTROL)
- REMOVE ALL SOILS AND SEDIMENTS TRACKED OR OTHERWISE DEPOSITED ONTO PUBLIC AND PRIVATE PAVEMENT AREAS. REMOVAL SHALL BE ON A DAILY BASIS WHEN TRACKING OCCURS AND MAY BE ORDERED BY MINNEAPOLIS INSPECTORS AT ANY TIME IF CONDITIONS WARRANT. SWEEPING SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THE CONSTRUCTION AND DONE IN A MANNER TO PREVENT DUST BEING BLOWN TO ADJACENT PROPERTIES.
- 4. INSTALL INLET PROTECTION AT ALL PUBLIC AND PRIVATE CATCH BASIN INLETS, WHICH RECEIVE RUNOFF FROM THE DISTURBED AREAS. GENERAL CONTRACTOR SHALL CLEAN, REMOVE SEDIMENT OR REPLACE STORM DRAIN INLET PROTECTION DEVICES ON A ROUTINE BASIS SUCH THAT THE DEVICES ARE FULLY FUNCTIONAL FOR THE NEXT RAIN EVENT. SEDIMENT DEPOSITED IN AND/OR PLUGGING DRAINAGE SYSTEMS IS THE RESPONSIBILITY OF THE GENERAL CONTRACTOR. HAY BALES OR FILTER FABRIC WRAPPED GRATES ARE NOT ALLOWED FOR INLET PROTECTION.
- 5. LOCATE SOIL OR DIRT STOCKPILES NO LESS THAN 25 FEET FROM ANY PUBLIC OR PRIVATE ROADWAY OR DRAINAGE CHANNEL. IF REMAINING FOR MORE THAN SEVEN DAYS, STABILIZE THE STOCKPILES BY MULCHING, VEGETATIVE COVER, TARPS, OR OTHER MEANS. CONTROL EROSION FROM ALL STOCKPILES BY PLACING SILT BARRIERS AROUND THE PILES. TEMPORARY STOCKPILES LOCATED ON PAVED SURFACES MUST BE NO LESS THAN TWO FEET FROM THE DRAINAGE/GUTTER LINE AND SHALL BE COVERED IF LEFT MORE THAN 24 HOURS.
- MAINTAIN ALL TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES IN PLACE UNTIL THE CONTRIBUTING DRAINAGE AREA HAS BEEN STABILIZED, INSPECT TEMPORARY EROSION AND SEDIMENT CONTROL DEVICES ON A DAILY BASIS AND REPLACE DETERIORATED, DAMAGED. OR ROTTED EROSION CONTROL DEVICES IMMEDIATELY.
- . TEMPORARILY OR PERMANENTLY STABILIZE ALL CONSTRUCTION AREAS WHICH HAVE UNDERGONE FINAL GRADING. AND ALL AREAS IN WHICH GRADING OR SITE BUILDING CONSTRUCTION OPERATIONS ARE NOT ACTIVELY UNDERWAY AGAINST EROSION DUE TO RAIN, WIND AND RUNNING WATER WITHIN 7-14 DAYS. USE SEED AND MULCH, EROSION CONTROL MATTING, AND/OR SODDING AND STAKING IN GREEN SPACE AREAS. REMOVE ALL TEMPORARY SYNTHETIC. STRUCTURAL, NON-BIODEGRADABLE EROSION AND SEDIMENT CONTROL DEVICES AFTER THE SITE HAS UNDERGONE FINAL STABILIZATION WITH PERMANENT VEGETATION ESTABLISHMENT. FINAL STABILIZATION FOR PURPOSES OF THIS REMOVAL IS 70% ESTABLISHED COVER OVER DENUDED AREA.
- 8. READY MIXED CONCRETE AND CONCRETE BATCH/MIX PLANTS ARE PROHIBITED WITHIN THE PUBLIC RIGHT OF WAY. ALL CONCRETE RELATED PRODUCTION, CLEANING AND MIXING ACTIVITIES SHALL BE DONE IN THE DESIGNATED CONCRETE MIXING/WASHOUT LOCATIONS AS SHOWN IN THE EROSION CONTROL PLAN. UNDER NO CIRCUMSTANCE MAY WASHOUT WATER DRAIN ONTO THE PUBLIC RIGHT OF WAY OR INTO ANY PUBLIC OR PRIVATE STORM DRAIN CONVEYANCE.
- CHANGES TO APPROVED EROSION CONTROL PLAN MUST BE APPROVED BY THE EROSION CONTROL INSPECTOR PRIOR TO IMPLEMENTATION. GENERAL CONTRACTOR TO PROVIDE INSTALLATION AND DETAILS FOR ALL PROPOSED ALTERNATE TYPE DEVICES.
- 10. IF DEWATERING OR PUMPING OF WATER IS NECESSARY, THE GENERAL CONTRACTOR IS RESPONSIBLE FOR OBTAINING ANY NECESSARY PERMITS AND/OR APPROVALS PRIOR TO DISCHARGE OF ANY WATER FROM THE SITE. IF THE DISCHARGE FROM THE DEWATERING OR PUMPING PROCESS IS TURBID OR CONTAINS SEDIMENT LADEN WATER, IT MUST BE TREATED THROUGH THE USE OF SEDIMENT TRAPS. VEGETATIVE FILTER STRIPS, OR OTHER SEDIMENT REDUCING MEASURES SUCH THAT THE DISCHARGE IS NOT VISIBLY DIFFERENT FROM THE RECEIVING WATER. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED AT THE DISCHARGE POINT TO PREVENT SCOUR EROSION. THE GENERAL CONTRACTOR SHALL PROVIDE A DEWATERING/PUMPING PLAN TO THE EROSION CONTROL INSPECTOR PRIOR TO INITIATING DEWATERING ACTIVITIES.
- 11. EROSION CONTROL SHALL BE PLACED ALONG THE PERIMETER OF THE SITE EXCAVATION. EROSION CONTROL SHALL BE PLACED SO IT DOES NOT DISTURB THE EXISTING PAVEMENT OR DRIVE LANES THAT ARE TO REMAIN. MANY METHODS OF EROSION CONTROL WILL WORK AND IT IS THE GENERAL CONTRACTOR'S RESPONSIBILITY TO INSTALL THE MEASURE MOST APPROPRIATE TO THE SITE CONDITIONS AND THAT WHICH MEETS CITY OF MINNEAPOLIS AND MPCA STANDARDS. GRAPHICALLY SHOWN ON THE PLANS FOR CLARITY BUT SHALL BE PLACED IN THE MOST APPROPRIATE LOCATIONS NOT TO DAMAGE EXISTING PAVEMENT AND/OR CURBS TO REMAIN. DAMAGED PAVEMENT AND/OR CURBS SHALL BE PAID FOR SOLELY BY THE CONTRACTOR. SEE DETAILS AND SPECIFICATIONS.
- 12. REMOVE ALL TEMPORARY SYNTHETIC, STRUCTURAL NON-BIODEGRADABLE EROSION AND SEDIMENT CONTROL DEVICES AFTER THE SITE HAS UNDERGONE FINAL STABILIZATION AND PERMANENT VEGETATION HAS BEEN ESTABLISHED, MINIMUM VEGETATION ESTABLISHMENT IS 70% COVER, MAINTAIN ALL TEMPORARY EROSION CONTROL DEVICES UNTIL 70% ESTABLISHED COVER IS ACHIEVED.
- 13. READY MIXED CONCRETE AND CONCRETE BATCH PLANTS PROHIBITED WITHIN PUBLIC RIGHT-OF-WAY. UNDER NO CIRCUMSTANCES MAY WASHOUT WATER DRAIN ONTO THE PUBLIC RIGHT-OF-WAY OR INTO THE STORM SEWER. CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING DESIGNATED CONCRETE WASHOUT AREA THAT COMPLIES WITH MPCA REQUIREMENT.
- 14. ALL EROSION CONTROL ELEMENTS ARE TEMPORARY. GENERAL CONTRACTOR TO INSTALL EROSION CONTROL ELEMENTS PRIOR TO START OF LAND DISTURBING ACTIVITIES, MAINTAIN IN GOOD CONDITION DURING CONSTRUCTION AND REMOVE FROM THE SITE UPON COMPLETION OF FINAL PAVING AND TURF ESTABLISHMENT.
- 15. GENERAL CONTRACTOR TO PROVIDE TEMPORARY SEED AND MULCH ON ALL NON-PAVED AREAS WITHIN 7 DAYS AFTER ROUGH GRADING IS COMPLETED. SEED WITH ANNUAL RYE SEED AT 60 LBS PER ACRE AND WOOD MULCH FIBER AT 45 LBS PER 1,000 SF.
- 16. GENERAL CONTRACTOR TO PREVENT DIRT AND/OR DEBRIS FROM ENTERING STORM SEWER OR BEING TRANSPORTED OFF-SITE IN AN UNCONTROLLED MANNER. CONTRACTOR TO VERIFY AT PROJECT CLOSEOUT THAT STORM SEWER SYSTEM IS CLEAR OF SEDIMENT AND/OR DEBRIS AND IS FULLY FUNCTIONAL.
- 17. STRAWBALES ARE NOT ALLOWED ON SITE IN ANY CAPACITY.

- 1. ALL EXISTING INFORMATION TAKEN FROM SURVEY BY CLARK ENGINEERING, DATED NOVEMBER 02, 2015
- 2. A GEOTECHNICAL EXPLORATION AND ENGINEERING REVIEW WILL BE COMPLETED ON SITE.
- GENERAL CONTRACTOR TO FIELD VERIFY ALL EXISTING CONDITIONS INCLUDING LOCATIONS OF EXISTING UTILITIES, AND NOTIFY ENGINEER OF ANY DISCREPANCIES PRIOR TO STARTING CONSTRUCTION.
- 4. ALL AREAS DISTURBED BY CONSTRUCTION WHICH ARE OUTSIDE THE LIMITS OF PAVING ARE TO BE RESTORED AND REVEGITATED.
- ALL UTILITY DEMOLITION AND/OR ABANDONMENT TO BE PERFORMED IN ACCORDANCE WITH CITY OF MINNEAPOLIS AND STATE OF MINNESOTA REGULATIONS AND STANDARDS.
- 6. EXISTING UTILITIES ARE SHOWN IN THEIR APPROXIMATE LOCATIONS. GENERAL CONTRACTOR TO FIELD VERIFY THE LOCATION OF ALL EXISTING UTILITIES WHICH MAY INCLUDE BUT IS NOT LIMITED TO" ELECTRIC, TELEPHONE, GAS, CABLE TV, COMPUTER CABLE, FIBER OPTIC CABLE, SANITARY SEWER, STORM SEWER, STEAM, CONDENSATE, ELECTRICAL DUCT BANK AND WATERMAIN. GENERAL CONTRACTOR TO CONTACT GOPHER ONE-CALL BEFORE EXCAVATING.
- ALL EXISTING UTILITIES AND OTHER IMPROVEMENTS ARE TO REMAIN UNLESS NOTED OTHERWISE.
- 8. GENERAL CONTRACTOR TO PROTECT FROM DAMAGE ALL EXISTING IMPROVEMENTS. LANDSCAPING. STRUCTURES AND UTILITIES THAT ARE TO REMAIN. GENERAL CONTRACTOR TO REPAIR ANY DAMAGE AT OWN EXPENSE.
- MINNESOTA STANDARDS AND REGULATIONS. 10. GENERAL CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS PRIOR TO
- 11. PROVIDE BARRICADES AT STREETS AND SIDEWALKS PER CITY OF MINNEAPOLIS REQUIREMENTS.

9. ALL WORK TO CONFORM WITH CITY OF MINNEAPOLIS AND STATE OF

- 12. WASTE MATERIALS INCLUDING PAVEMENT REMOVED DURING CONSTRUCTION. WASTE PIPING AND SUPPLIES, CONSTRUCTION DEBRIS AND EXCESS EXCAVATED MATERIAL SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED FROM THE PROJECT SITE AND DISPOSED OF PROPERLY BY THE GENERAL CONTRACTOR.
- 13. ADEQUATE DRAINAGE SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION AND ANY DRAINAGE DITCH OR STRUCTURE DISTURBED DURING CONSTRUCTION SHALL BE RESTORED TO THE SATISFACTION OF THE OWNING AUTHORITY. ALL CONSTRUCTION STORM RUNOFF SHALL COMPLY WITH THE CITY OF MINNEAPOLIS REQUIREMENTS.
- 14. WHEN WORKING AROUND EXISTING TELEPHONE OR ELECTRICAL POLES, THE GENERAL CONTRACTOR SHALL BRACE THE POLE FOR SUPPORT.
- 15. WHEN WORKING AROUND EXISTING UTILITIES THAT BECOME EXPOSED, THE GENERAL CONTRACTOR SHALL PROVIDE SUFFICIENT SUPPORT TO PREVENT EXCESSIVE STRESS ON THE PIPING. THE CONTRACTOR IS FULLY RESPONSIBLE FOR ANY DAMAGES CAUSED BY FAILURE TO EXACTLY LOCATE AND PRESERVE THESE UNDERGROUND FACILITIES.
- 16. ALL EXCAVATIONS MUST COMPLY WITH THE REQUIREMENTS OF OSHA 29 CFR, PART 1926, SUBPART P "EXCAVATIONS AND TRENCHES". THIS DOCUMENT STATES THAT EXCAVATION SAFETY IS THE SOLE RESPONSIBILITY OF THE GENERAL CONTRACTOR.
- 17. GENERAL CONTRACTOR SHALL COORDINATE WITH ARCHITECT'S AND MECHANICAL ENGINEER'S DRAWINGS TO VERIFY LOCATION, SIZE AND QUANTITY OF ALL UTILITY CONNECTIONS.
- 18. ALL MATERIALS FOR PROPOSED CONSTRUCTION OR REPAIR OF EXISTING FACILITIES SHALL BE NEW PRODUCTS DIRECT FROM THE FACTORY AND FREE FROM DEFECTS.
- 19. GENERAL CONTRACTOR SHALL NOT BLOCK DRAINAGE FROM OR DIRECT EXCESS DRAINAGE ONTO ADJACENT PROPERTY.
- 20. PROVIDE THE FOLLOWING MINIMUM COVER OVER THE TOP OF PIPE AS FOLLOWS:
- A. 8' OVER WATER MAIN

STARTING CONSTRUCTION.

- B. 5' OVER SANITARY SEWER C. 1' OVER STORM SEWER
- 21. MAXIMUM CROSS-SLOPES FOR SIDEWALKS IS 2.00% AND ADA ACCESS ROUTES SHALL NOT EXCEED 5.00%. RAMPS SLOPES SHALL NOT EXCEED 1" PER FOOT (8.33%). MAXIMUM SLOPES FOR HANDICAP PARKING STALLS AND ACCESS AISLES SHALL NOT EXCEED 2.00% IN ANY DIRECTION.
- 22. PERMITS AND APPROVAL ARE REQUIRED FROM ENVIRONMENTAL SERVICES FOR THE FOLLOWING ACTIVITIES:
- AFTER HOURS WORK TEMPORARY STORAGE OF IMPACTED SOILS ON SITE PRIOR TO DISPOSAL OR
- REMEDIATION OF CONTAMINATED SOIL AND GROUNDWATER, REUSE OF IMPACTED SOILS ON SITE
- DEWATERING AND DISCHARGE OF ACCUMULATED STORM WATER OR GROUND WATER TO CITY SEWERS FLAMMABLE WASTE TRAPS, UNDERGROUND OR ABOVE GROUND TANK
- INSTALLATION OR REMOVAL
- WELL CONSTRUCTION OR SEALING ON-SITE ROCK CRUSHING.
- CONTACT TOM FRAME AT TOM.FRAME@CI.MINNEAPOLIS.MN.US OR (612) 673-5807, FOR PERMIT APPLICATIONS AND APPROVALS.
- 22. NO CONSTRUCTION, DEMOLITION OR COMMERCIAL POWER MAINTENANCE EQUIPMENT SHALL BE OPERATED WITHIN THE CITY BETWEEN THE HOURS OF 6:00 PM AND 7:00 AM ON WEEKDAYS OR DURING ANY HOURS ON SATURDAYS, SUNDAYS AND STATE AND FEDERAL HOLIDAYS, EXCEPT UNDER PERMIT. CONTACT ENVIRONMENTAL SERVICES AT (612) 673-3867 FOR PERMIT INFORMATION.
- 23. ANY CONCRETE CONSTRUCTION WORK WITHIN THE PUBLIC RIGHT-OF-WAY MUST BE PERFORMED BY A LICENSED AND BONDED CONCRETE CONTRACTOR.
- 24. A \$15.000 SIDEWALK CONTRACTOR'S BOND MUST BE OBTAINED FROM PUBLIC WORKS SIDEWALK INSPECTIONS PRIOR TO THE START OF ANY WORK WITHIN THE PUBLIC RIGHT-OF-WAY. PLEASE LOG ON TO: <u>WWW.SIDEWALK.MPLS.MNROWAY.NET</u> AND FOLLOW THE INSTRUCTIONS ON
- 25. A SIDEWALK CONSTRUCTION PERMIT MUST BE OBTAINED PRIOR TO THE START OF ANY WORK IN THE PUBLIC RIGHT-OF-WAY. LOG ON TO: <u>WWW.SIDEWALK.MPLS.MN.ROWAY.NET</u> FOR A PERMIT.
- 26. AN OBSTRUCTION PERMIT IS REQUIRED ANYTIME CONSTRUCTION WORK IS PERFORMED IN THE PUBLIC RIGHT-OF-WAY. PLEASE CONTACT SCOTT KRAMER AT (612) 673-2383 REGARDING DETAILS OF SIDEWALK AND LANE CLOSURES. LOG ON TO http://minneapolis.mn.roway.net FOR A PERMIT.
- 27. ALL COSTS FOR RELOCATION AND/OR REPAIR OF CITY FACILITIES SHALL BE BORNE BY THE GENERAL CONTRACTOR AND/OR PROPERTY OWNER.
- 28. PRIOR TO STARTING ANY CONSTRUCTION, CALL THE FORESTRY PRESERVATION COORDINATOR (FPC) TO DETERMINE IF A TREE WORK PERMIT IS NEEDED AND TO ENSURE THAT PUBLIC TREES ARE PROTECTED OR COMPENSATED FOR. A PUBLIC TREE IS ANY TREE THAT IS GROWING IN THE BOULEVARD OR PUBLIC RIGHT-OF-WAY AS WELL AS ON ANY OTHER MUNICIPAL PUBLIC PROPERTY.
- 29. IT IS ILLEGAL TO REMOVE, PRUNE OR PLANT A PUBLIC TREE PER ORDINANCE WITHOUT RECEIVING A PERMIT FROM THE MPRB. REMOVAL, PRUNING AND PLANTING VIA PERMIT MUST CONFORM TO MPRB FORESTRY DIVISION SPECIFICATIONS.
- 30. TREE WORK THAT OCCURS ON PUBLIC TREES WITHOUT A PERMIT WILL BE STOPPED AND THE POLICE WILL BE NOTIFIED. IF NECESSARY. COMPENSATION THAT RESULTS FROM ILLEGAL TREE WORK WILL BE COLLECTED THROUGH LITIGATIONS.
- 31. PUBLIC TREES HAVE A MONETARY VALUE. THE ROOT ZONE, TRUNK AND BRANCHES OF ALL PUBLIC TREES GROWING WITHIN THE PUBLIC RIGHT-OF-WAY NEED TO BE PROTECTED WITH CONSTRUCTION FENCE THAT EXTENDS AS FAR AS THE SPREAD OF THE OUTER MOST BRANCHES. IF A PUBLIC TREE CAN NOT BE PRESERVED, COMPENSATION WILL BE REQUIRED FOR THE VALUE OF THE TREE OR DAMAGE TO IT.
- 32. IF A CRANE IS USED ON SITE AND IS WITHIN 50 FEET OF A PUBLIC TREE, THE FORESTRY PRESERVATION COORDINATOR (FPC) NEEDS TO BE NOTIFIED.
- 33. MULCH PLACED AROUND EXISTING OR NEWLY PLANTED PUBLIC TREES MUST BE WOOD CHIPS AND MUST NOT TOUCH THE TRUNK OF THE TREE.
- 34. CONTACT CRAIG PINKALLA (612) 499-9233 REGARDING REMOVAL OR PROTECTION OF TREES IN THE CITY RIGHT OF WAY.
- 35. CONTACT ANDREA SWENSON AT (612) 673-3263 PRIOR TO CONSTRUCTION FOR THE REMOVAL OF ANY CITY OF MINNEAPOLIS RIGHT OF WAY SIGNS THAT MAY BE IN THE WAY OF CONSTRUCTION.

- 36. AN ENCROACHMENT PERMIT SHALL BE REQUIRED FOR ALL STREETSCAPE ELEMENTS IN THE PUBLIC RIGHT-OF-WAY SUCH AS: PLANTS & SHRUBS PLANTERS, TREE GRATES AND OTHER LANDSCAPING ELEMENTS, SIDEWALK FURNITURE (INCLUDING BIKE RACKS AND BOLLARDS), AND SIDEWALK ELEMENTS OTHER THAN STANDARD CONCRETE WALKWAYS SUCH AS PAVERS. STAIRS, RAISED LANDINGS, RETAINING WALLS, ACCESS RAMPS, AND RAILINGS (NOTE: RAILINGS MAY NOT EXTEND INTO THE SIDEWALK PEDESTRIAN AREA). PLEASE CONTACT MATT HANAN AT (612) 673-3607 FOR FURTHER INFORMATION.
- 37. ANY ELEMENTS OF AN EARTH RETENTION SYSTEM AND RELATED OPERATIONS (SUCH AS CONSTRUCTION CRANE BOOM SWINGS) THAT FALL WITHIN THE PUBLIC RIGHT-OF-WAY WILL REQUIRE AN ENCROACHMENT PERMIT APPLICATION. IF THERE ARE TO BE ANY EARTH RETENTION SYSTEMS WHICH WILL EXTEND OUTSIDE THE PROPERTY LINE OF THE DEVELOPMENT THEN A PLAN MUST BE SUBMITTED SHOWING DETAILS OF THE SYSTEM. ALL SUCH ELEMENTS SHALL BE REMOVED FROM THE PUBLIC RIGHT-OF-WAY FOLLOWING CONSTRUCTION WITH THE EXCEPTION OF TIE-BACKS WHICH MAY REMAIN BUT MUST BE UNCOUPLED AND DE-TENSIONED. PLEASE CONTACT MATT HANAN AT (612) 673-3607 FOR FURTHER INFORMATION.
- 38. CARE MUST BE TAKEN DURING CONSTRUCTION AND EXCAVATION TO PROTECT ANY SURVEY MONUMENTS AND/OR PROPERTY IRONS.
- 39. DRAWINGS DO NOT INDICATE AREAS OF TEMPORARY SUPPORT SYSTEMS. THE GENERAL CONTRACTOR IS RESPONSIBLE FOR ALL MEANS AND METHODS AND WILL HAVE TOTAL CONTROL OVER THE TYPES AND DESIGN OF ALL SHORING, SHEETING, BRACING, ANCHORAGES, EXCAVATION SUPPORT WALLS, DIRECTIONAL BORING, AUGER JACKING, SOIL STABILIZATION AND OTHER METHODS OF PROTECTING EXISTING IMPROVEMENTS. SEE SPECIFICATIONS FOR SUBMITTAL REQUIREMENTS.
- 40. GENERAL CONTRACTOR TO RECORD EXISTING CONDITIONS AS NEEDED (PHOTOGRAPHS, VIDEO PHOTOGRAPHY, FIELD SURVEYING, ETC.) TO ENABLE RECONSTRUCTION TO MATCH EXISTING CONDITIONS AS REQUIRED. GENERAL CONTRACTOR TO DOCUMENT EXISTING CONDITIONS SO THAT RECONSTRUCTED AREAS WILL HAVE POSITIVE DRAINAGE SIMILAR TO
- 41. WHERE DEMOLITION, EXCAVATION, UNDERPINNING, PILE DRIVING COMPACTING OR SIMILAR WORK IS TO BE PERFORMED ADJACENT TO OR IN THE IMMEDIATE VICINITY OF EXISTING STRUCTURES, THE OWNER WILL PROVIDE BUILDING SURVEYS AND SEISMIC MONITORING. GENERAL CONTRACTOR SHALL COORDINATE MONITORING OF BUILDING AND KEEPING OWNER INFORMED OF OPERATIONS THAT MAY IMPACT SENSITIVE PROJECTS OR STRUCTURES.
- 42. PLEASE CONTACT JOSEPH LAURIN AT (612) 673-5987 REGARDING EXISTING AND PROPOSED STREET LIGHTING. ALL STREET LIGHTING (EXISTING AND PROPOSED) SHALL BE SHOWN CLEARLY ON THE SITE PLAN. PER THE CITY OF MINNEAPOLIS STREET LIGHTING POLICY, STREET LIGHTING WILL BE STRONGLY ENCOURAGED ALONG MAJOR PEDESTRIAN CORRIDORS AND BUSINESS NODES AS IDENTIFIED IN THE MINNEAPOLIS STREET LIGHTING
- 43. ACCESS TO FIRE DEPARTMENT APPARATUS MUST BE MAINTAINED AT ALL TIMES DURING THE ENTIRETY OF THE PROCESS.
- 44. CONTACT ALLAN KLUGMAN AT (612) 673-2743 PRIOR TO CONSTRUCTION FOR THE TEMPORARY REMOVAL/TEMPORARY RELOCATION OF ANY CITY OF MINNEAPOLIS LIGHTING OR TRAFFIC SIGNAL SYSTEM THAT MAY BE IN THE WAY OF CONSTRUCTION.
- 45. STREET LIGHTING INSTALLED AS PART OF THIS PROJECT SHALL BE INSPECTED BY THE CITY, GENERAL CONTRACTORS SHALL ARRANGE FOR INSPECTIONS WITH THE TRAFFIC DEPARTMENT, PLEASE CONTACT DAVE PREHALL AT (612) 673-5759 FOR FURTHER INFORMATION. ANY LIGHTING INSTALLATIONS NOT MEETING CITY SPECIFICATIONS WILL BE REQUIRED TO BE REINSTALLED AT OWNER EXPENSE.
- 46. ANY METERED PARKING SPACES NEEDED TO FACILITATE PROJECT WORK MUST BE HOODED AT PROJECT EXPENSES FOR THE LENGTH OF DISRUPTION REQUIRED. IF SIDEWALK IS REMOVED AND PARKING LANE I CLOSED. THE METER AND SPACE DELINEATORS MUST BE REMOVED AND REINSTALLED UPON PROJECT COMPLETION AT PROJECT EXPENSES. THIS CAN BE REQUESTED AT THE SAME TIME THAT THE "OBSTRUCTION PERMIT" IS OBTAINED VIA HTTP//MINNEAPOLIS.MN.ROWAY.NET/ CONTACT SCOTT KRAMER AT (612) 673-2383 SCOTT.KRAMER.@MINNEAPOLISMN.GOV IF YOU HAVE ANY QUESTIONS REGARDING OBSTRUCTION PERMITS OF METER
- 47. PAVEMENT MARKINGS THAT EXIST PRIOR TO THE START OF THE PROJECT MUST BE REPLACED IN KIND. IF IT IS LATEX OR EPOXY PAINT THEN IT MUST GO BACK IN WITH THE TYPE OF PAINT UNLESS OTHERWISE DIRECTED BY CITY OF MINNEAPOLIS TRAFFIC DIVISION. ALL COSTS ASSOCIATED WITH THE PAVEMENT MARKING REPLACEMENT SHALL BE BORNE BY THE CONTRACTOR AND/OR PROPERTY OWNER.
- 48. ALL COSTS FOR RELOCATION AND/OR REPAIR OF CITY TRAFFIC FACILITIES INCLUDING TRAFFIC SIGNALS SYSTEMS, STREET LIGHTING, TRAFFIC SIGNS, PARKING METERS, AND PAVEMENT MARKINGS SHALL BE BORNE BY THE GENERAL CONTRACTOR AND/OR PROPERTY OWNER.
- 49. CONTACT SHANE MORTON AT (612) 673-5517 PRIOR TO CONSTRUCTION FOR THE REMOVAL OF ANY CITY OF MINNEAPOLIS RIGHT OF WAY SIGNS OR PAVEMENT MARKINGS THAT MAY BE IN THE WAY OF CONSTRUCTION.
- 50. ANY EXISTING CONCRETE INFRASTRUCTURE IN THE PUBLIC RIGHT OF WAY, INCLUDING BUT NOT LIMITED TO PUBLIC SIDEWALKS, CURB AND GUTTER, AND ADA PEDESTRIAN RAMPS, THAT IS EITHER CURRENTLY DEFECTIVE OR THAT IS DAMAGE DURING THE TIME OF SITE RE-DEVELOPMENT, MUST BE REMOVED AND REPLACED AT THE TIME OF SITE RE-DEVELOPMENT.
- 51. IF IMPACTED SOIL IS ENCOUNTERED DURING SITE ACTIVITIES WORK WILL NEED TO STOP AND NOTIFICATION PROVIDED TO THE MN DUTY OFFICER AT (651) 649-5451 OR 800-422-0798.

LEGEND S) S EXISTING MANHOLE EXISTING CATCH BASIN EXISTING FLARED END SECTION EXISTING ROOF DRAIN (OUTLET) EXISTING GATE VALVE EXISTING FIRE HYDRANT **EXISTING STOP BOX** EXISTING AIR CONDITIONER EXISTING ELECTRIC MANHOLE EXISTING ELECTRIC BOX EXISTING ELECTRIC METER EXISTING ELECTRIC OUTLET POST EXISTING HANDHOLE EXISTING LIGHT EXISTING UTILITY POLE EXISTING GUY WIRE EXISTING SIGN **EXISTING SEMAPHORE** EXISTING TELEPHONE MANHOLE EXISTING COMMUNICATION BOX EXISTING GAS METER EXISTING PEDESTRIAN POST EXISTING BOLLARD --- EXISTING WOOD FENCE ----- ABND ---- EXISTING ABANDONED LINE EXISTING CONTOUR EXISTING ELECTRIC LINE EXISTING FIBER OPTIC ----- GAS ----- ABANDONED GAS LINE EXISTING GAS LINE -----OHW------ EXISTING OVERHEAD WIRE EXISTING SANITARY LINE ------ ss ----- EXISTING STORM LINE ------ EXISTING TELEPHONE LINE EXISTING WATER LINE — PROPOSED CONTOUR PROPOSED SPOT ELEVATION DENOTES SURFACE DRAINAGE — — PROPOSED SAWCUT LINE APPROXIMATE SOIL BORING LOCATION EXISTING UTILITY TO BE REMOVED PROPOSED STORM SEWER PROPOSED SANITARY SEWER --- ←DT--+- PROPOSED DRAINTILE PROPOSED CATCH BASIN PROPOSED MANHOLE PROPOSED CLEAN OUT PROPOSED HYDRANT PROPOSED GATE VALVE PROPOSED RETAINING WALL SILT FENCE EROSION CONTROL AT CB/MH PROPOSED TREE PROTECTION EXISTING TREE TO BE REMOVED HHHH CURB AND GUTTER REMOVAL EXISTING BITUMINOUS PAVING REMOVAL, SEE C200 EXISTING CONCRETE PAVING REMOVAL, SEE C200 EXISTING CONCRETE SIDEWALK REMOVAL, SEE C200 PROPOSED BITUMINOUS PAVING, SEE C600, SEE DETAIL 1/C700 PROPOSED CONCRETE PAVEMENT, SEE C600, SEE DETAIL 2/C700 PROPOSED CONCRETE SIDEWALK, SEE C600, SEE DETAIL 3/C700 PROPOSED CONSTRUCTION ENTRANCE, SEE C302

Architecture Interior Design Landscape Architecture Engineering

Boarman Kroos Vogel Group Inc.

222 North Second Street Long & Kees Bldg, Suite 101 Minneapolis, MN 55401 Telephone: 612.339.3752 www.bkvgroup.com

OWNER / DEVELOPER

CONSULTANTS

PIERCE PINI & 9298 CENTRAL AVENUE NE SUITE 312 BLAINE, MN 55434 TEL 763.537.1311

PROJECT TITLE

SHELBY

ISSUE #	DATE	DESCRIPTION

PDR/LAND USE 10-02-2020 CERTIFICATION I hereby certify that this plan, specification, or

State of Minnesota.

supervision and that I am a duly Licensed Professional Engineer under the laws of the

License Number

SHEET TITLE **GENERAL NOTES AND**

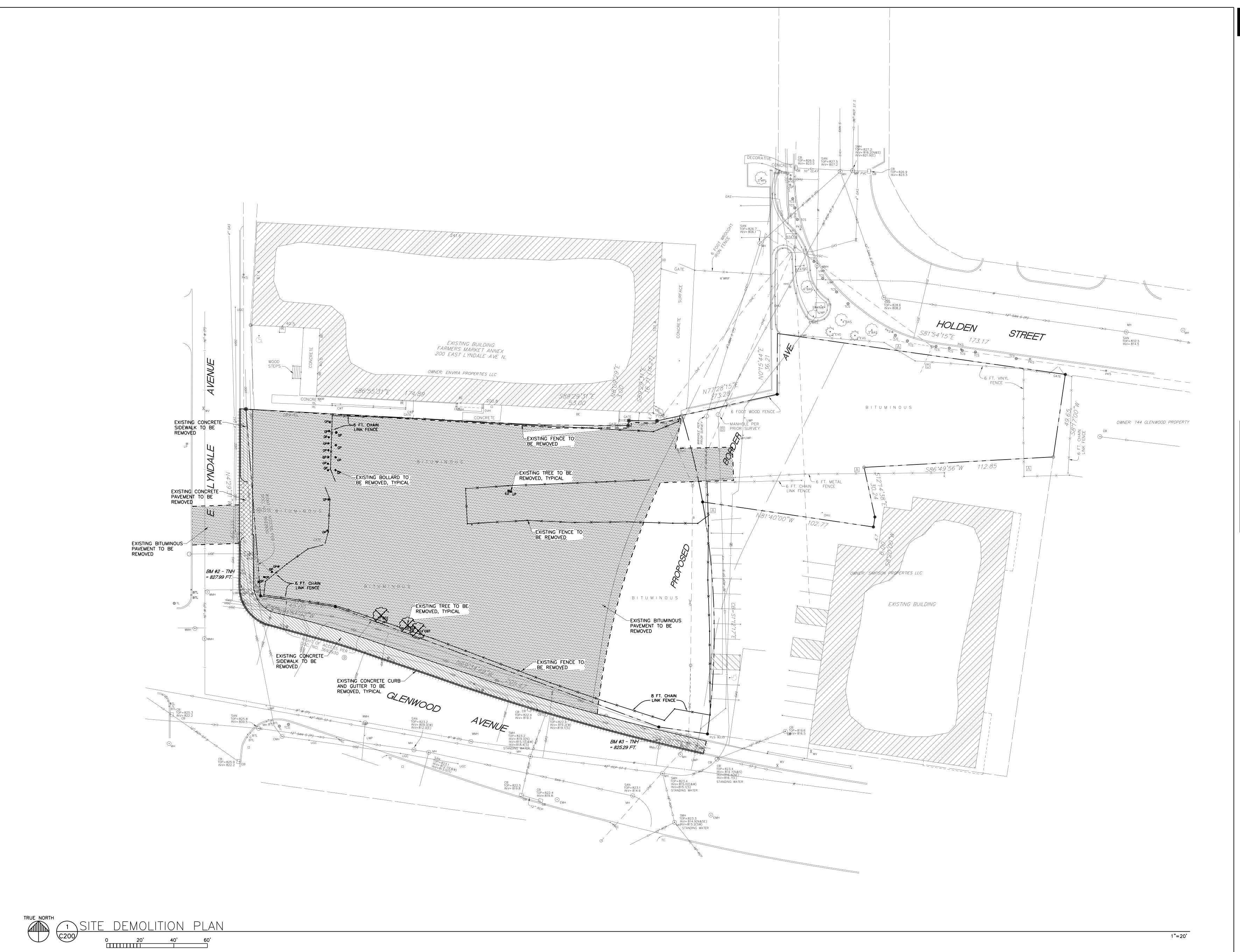
COMMISSION NUMBER

SHEET NUMBER

LEGEND

Rhonda S. Pierce

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BKV

Architecture
Interior Design
Landscape Architecture
Engineering

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222 North Second Street Long & Kees Bldg, Suite 101 Minneapolis, MN 55401 Telephone: 612.339.3752 www.bkvgroup.com EOE

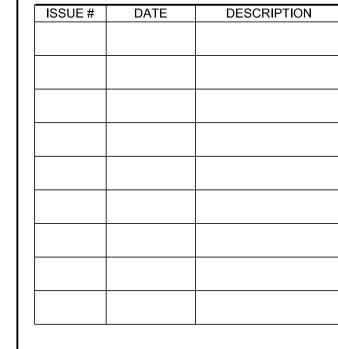
OWNER / DEVELOPER

CONSULTANTS

PIERCE PINI &
ASSOCIATES
9298 CENTRAL AVENUE NE,
SUITE 312
BLAINE, MN 55434
TEL 763.537.1311

PROJECT TITLE

SHELBY COMMONS



PDR/LAND USE 10-02-2020
CERTIFICATION

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Rhonda S. Pierce
41333
License Number

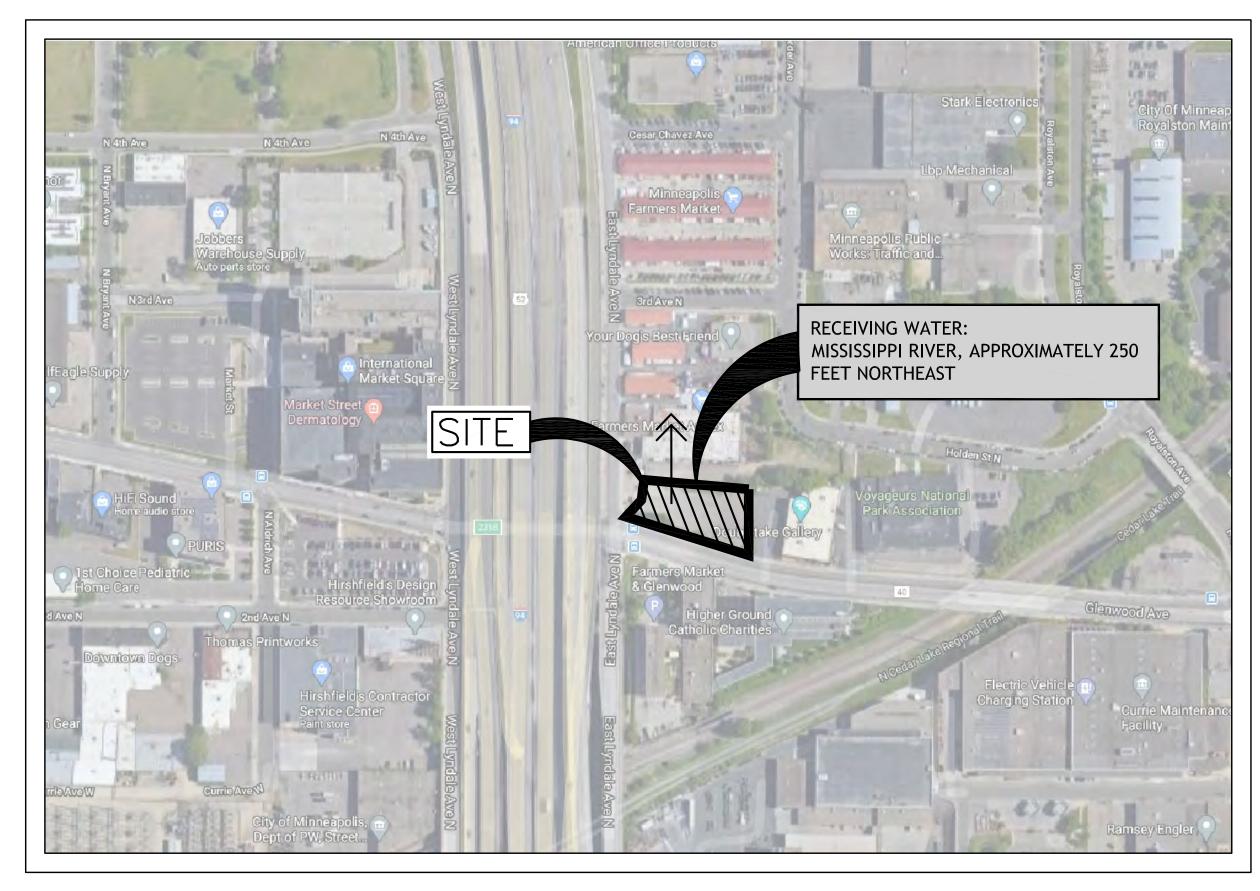
DRAWN BY DPM
CHECKED BY RSP
COMMISSION NUMBER 20-033
SHEET TITLE

SITE DEMOLITION PLAN

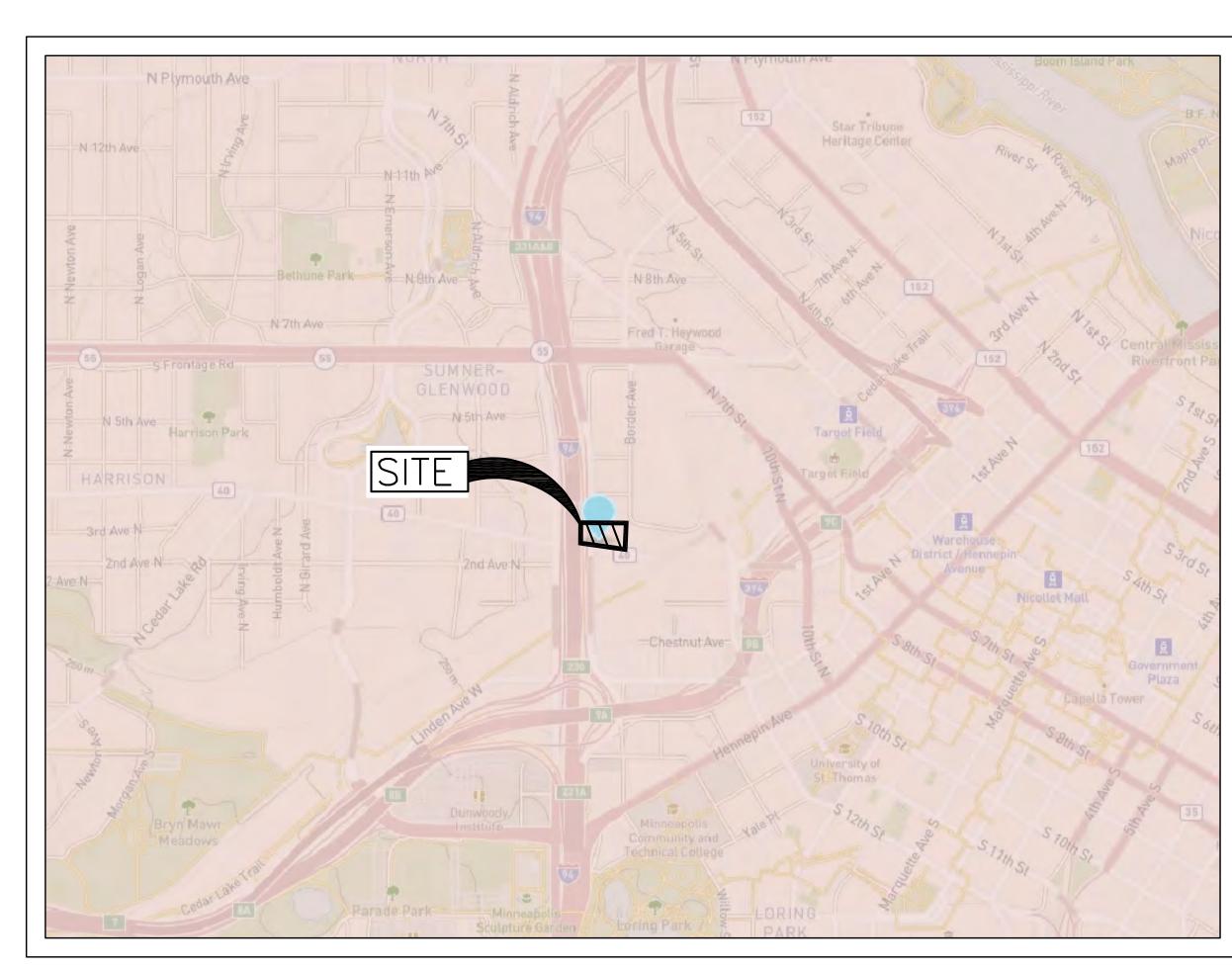
SHEET NUMBER

C200

2020 PIERCE PINI + ASSOCIATES, INC.



SITE LOCATION MAP NOT TO SCALE LATITUDE: 44.979523 LONGITUDE: -93.286076



ACREAGE SUMMARY	
TOTAL SITE AREA	0.894 ACRES
ON-SITE DISTURBED AREA	0.894 ACRES
OFF-SITE DISTURBED AREA (INCL. STAGING AREA)	0.053 ACRES
TOTAL DISTURBED AREA	0.947 ACRES
EXISTING IMPERVIOUS AREA	0.884 ACRES
PROPOSED IMPERVIOUS AREA	0.563 ACRES

SOIL ASSOCIATION	HYDROLOGIC SOIL GROUP
POORLY GRADED SAND (SP)	Α

ESTIMATED BMP QUAN	TITIES
SILT FENCE	560 LF
FILTER LOGS	130 LF
CATCH BASIN INSERTS	9 EA
ROCK CONSTRUCTION ENTRANCE	40 CY

NOTES:

QUANTITIES ARE FOR INFORMATIONAL PURPOSES ONLY. CONTRACTOR SHALL DETERMINE FOR THEMSELVES THE EXACT QUANTITIES FOR BIDDING AND CONSTRUCTION. CONTRACTOR SHALL NOT RELY ON THESE QUANTITIES FOR THEIR BID AND CIVIL ENGINEER IS NOT RESPONSIBLE FOR COST ESTIMATES OR ACTUAL CONSTRUCTION COSTS.

	CONTACT INFORMATION
	OWNER
	BUSINESS:
	CONTACT PERSON, TITLE:
	ADDRESS:
	TELEPHONE:
	EMAIL:
	ALTERNATE CONTACT PERSON, TITLE:
	ADDRESS:
	TELEPHONE:
	EMAIL:
1	

PARTY	RESPONSIBLE	FOR	LONG	TERM	MAINTENANCE
BUSINESS:					
CONTACT P	ERSON, TITLE:				
ADDRESS:					
TELEPHONE	:				
EMAIL:					

CONTRACTOR

BUSINESS: CONTACT PERSON, TITLE: ADDRESS: TELEPHONE: ALTERNATE CONTACT PERSON, TITLE: ADDRESS: TELEPHONE: EMAIL:

CONTRACTOR'S ONSITE REPRESENTATIVE **BUSINESS:** CONTACT PERSON, TITLE: ADDRESS:

NOTE: CONTRACTOR'S ONSITE REPRESENTATIVE SHALL BE COMPLETED ON THE PLAN SET KEPT IN THE CONSTRUCTION TRAILER. THIS INFORMATION SHALL ALSO BE PROVIDED TO THE OWNER AND CIVIL ENGINEER.

TELEPHONE: EMAIL: ALTERNATE CONTACT: TELEPHONE:

MN STATE DUTY OFFICER 651-649-5457 ALTERNATE: 1-800-422-0798

Architecture Interior Design Landscape Architecture Engineering

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OWNER / DEVELOPER

CONSULTANTS

PIERCE PINI & 9298 CENTRAL AVENUE NE, SUITE 312 BLAINE, MN 55434 TEL 763.537.1311

PROJECT TITLE

SHELBY COMMONS

ISSUE#	DATE	DESCRIPTION

PDR/LAND USE 10-02-2020 CERTIFICATION I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Rhonda S. Pierce	

41333
License Number

SHEET TITLE	
COMMISSION NUMBER	20-033
CHECKED BY	RSP
DRAWN BY	DPM

SWPPP **GENERAL**

INFORMATION

SHEET NUMBER

PROJECT DESCRIPTION:

A STORMWATER POLLUTION PREVENTION PLAN (SWPPP) IS REQUIRED AS PART OF THE GENERAL PERMIT AUTHORIZATION TO DISCHARGE STORMWATER ASSOCIATED WITH CONSTRUCTION ACTIVITY UNDER THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES)/STATE DISPOSAL SYSTEM (SDS) FOR SITES OVER ONE ACRE OF DISTURBANCE. THIS PERMIT IS ISSUED BY THE MINNESOTA POLLUTION CONTROL AGENCY (MPCA). A PERMIT FROM THE MPCA NOT IS REQUIRED FOR THIS PROJECT.

THE GOAL OF POLLUTION PREVENTION EFFORTS DURING PROJECT CONSTRUCTION IS TO CONTROL SOIL AND POLLUTANTS ON THE SITE AND PREVENT THEM FROM LEAVING THE PROJECT SITE AND FLOWING TO SURFACE WATERS. THE PURPOSE OF THIS SWPPP IS TO PROVIDE GUIDELINES FOR ACHIEVING THAT GOAL. THE SWPPP MUST BE KEPT ONSITE AND UPDATED AS NECESSARY DURING THE COURSE OF CONSTRUCTION TO KEEP IT CURRENT WITH ANY MODIFICATIONS TO THE POLLUTION CONTROL MEASURES BEING UTILIZED.

THE PROJECT CURRENTLY DOES NOT HAVE AN ADDRESS.

THIS PROJECT CONSISTS OF THE DEMOLITION OF AN EXISTING BITUMINOUS PARKING LOT AND A PERIMETER FENCE. THE PROPOSED CONSTRUCTION WILL CONSIST OF A NEW APARTMENT COMPLEX BUILDING WITH SURFACE LEVEL AND UNDERGROUND PARKING LOTS.

A STORMWATER MANAGEMENT SYSTEM IS NOT REQUIRED FOR THE PROJECT DUE TO THE PROJECT AREA BEING LESS THAN 1 ACRE IN SIZE.

THE PROPOSED PROJECT IS NOT REQUIRED TO MEET THE CITY OF MINNEAPOLIS STORMWATER REQUIREMENTS

REGULATORY CONTEXT: DISCHARGE TO SPECIAL OR IMPAIRED WATERS WITHIN ONE MILE OF SITE:

-THIS PROJECT DISCHARGES TO BASSETT CREEK AND THE MISSISSIPPI RIVER. BOTH BASSETT CREEK AND THE MISSISSIPPI RIVER ARE LISTED AS AN IMPARIED WATER ON MPCA'S 303(D) IMPAIRED WATERS LIST. BASSETT CREEK IS LISTED FOR CHLORIDE, FISHES BIOASSESSMENTS, FECAL COLIFORM. THE MISSISSIPPI RIVER IS LISTED FOR MERCURY IN FISH TISSUE, FECAL COLIFORM, AND PCB IN FISH TISSUE IMPAIRMENTS. SINCE BASSETT CREEK AND THE MISSISSIPPI RIVER ARE LOCATED WITHIN ONE MILE OF THE SITE, BMPS AS DEFINED IN THE NPDES PERMIT C.1 AND C.2 IN APPENDIX A APPLY. THESE ARE AS FOLLOWS:

C.1 DURING CONSTRUCTION.

- A. STABILIZATION OF ALL EXPOSED SOIL AREAS MUST BE INITIATED IMMEDIATELY TO LIMIT SOIL EROSION BUT IN NO CASE COMPLETED LATER THAN SEVEN (7) DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
- B. TEMPORARY SEDIMENT BASIN REQUIREMENTS DESCRIBED IN PART III.C MUST BE USED FOR COMMON DRAINAGE LOCATIONS THAT SERVE AN AREA WITH FIVE (5) OR MORE ACRES DISTURBED AT ONE TIME.

C.2. POST CONSTRUCTION

A. THE WATER QUALITY VOLUME THAT MUST BE RETAINED ON SITE BY THE PROJECT'S PERMANENT STORMWATER MANAGEMENT SYSTEM DESCRIBED IN PART III.D SHALL BE ONE (1) INCH OF RUNOFF FROM THE NEW IMPERVIOUS SURFACES CREATED BY THE PROJECT. SEE PART III.D.1. FOR MORE INFORMATION ON INFILTRATION DESIGN, PROHIBITIONS AND APPROPRIATE SITE CONDITIONS.

THE REQUIREMENT IN SECTION C.2 DOES NOT APPLY BECAUSE THE INCREASE IN IMPERVIOUS AREA IS LESS THAN 1.0 ACRE FOR THIS PROJECT.

PLACEMENT OF FILL IN WATERS OF THE STATE:

-N/A <u>DRINKING WATER SUPPLY MANAGEMENT AREA:</u>

DRINKING WATER SUPPLY MANAGEMENT ARE.
-N/A

THE PROJECT STORMWATER DISCHARGE IS NOT ANTICIPATED TO IMPACT ANY OF THE FOLLOWING:

-OUTSTANDING RESOURCE VALUE WATERS, TROUT WATERS, WETLANDS, CALCAREOUS FENS, PROPERTIES LISTED BY THE NATIONAL REGISTER OF HISTORIC PLACES OR ARCHAEOLOGICAL SITES

THE PROJECT STORMWATER DISCHARGE IS NOT SUBJECT TO ADDITIONAL REGULATION DUE TO ANY OF THE FOLLOWING:

-OTHER FORMAL ENVIRONMENTAL REVIEWS, ENDANGERED OR THREATENED SPECIES

STORMWATER POLLUTION PREVENTION PLAN (SWPPP) IMPLEMENTATION RESPONSIBILITIES:

- THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR ALL ON-SITE IMPLEMENTATION OF THE SWPPP, INCLUDING THE ACTIVITIES OF ALL OF THE GENERAL CONTRACTOR'S SUBCONTRACTORS DURING CONSTRUCTION.
 THE GENERAL CONTRACTOR SHALL PROVIDE A PERSON(S) KNOWLEDGEABLE AND EXPERIENCED IN THE APPLICATION OF EROSION PREVENTION AND SEDIMENT CONTROL BMPS TO OVERSEE ALL INSTALLATION AND MAINTENANCE OF BMPS AND IMPLEMENTATION OF THE SWPPP.
- 3. THE GENERAL CONTRACTOR SHALL PROVIDE PERSON(S) MEETING THE TRAINING REQUIREMENTS OF THE NPDES PERMIT TO CONDUCT INSPECTION AND MAINTENANCE OF ALL EROSION PREVENTION AND SEDIMENT CONTROL BMPS IN ACCORDANCE WITH THE REQUIREMENTS OF THE PERMIT.

STORMWATER DISCHARGE DESIGN REQUIREMENTS:

THE FOLLOWING SIZING CRITERIA APPLY TO THE DESIGN OF STORMWATER TREATMENT FACILITIES. N/A INDICATES NOT APPLICABLE OR NOT CONSTRUCTED AS PART OF THIS PROJECT.

- 1. TEMPORARY SEDIMENTATION BASINS: N/A
- 2. PERMANENT WET SEDIMENTATION BASINS: N/A3. PERMANENT INFILTRATION/FILTRATION: N/A
- 4. PERMANENT REGIONAL PONDS: N/A

5. ALTERNATIVE METHODS: N/A

SEQUENCE OF CONSTRUCTION: THE FOLLOWING SEQUENCE DESCRIBES, IN GENERAL, THE WORK ON THE SITE:

- THE GENERAL CONTRACTOR SHALL VERIFY THAT ALL PERMITS HAVE BEEN OBTAINED AND/OR OBTAIN THE NECESSARY PERMITS.
- THE GENERAL CONTRACTOR SHALL PERFORM SITE INSPECTIONS, RECORD KEEPING AND RECORD RETENTION IN ACCORDANCE WITH ALL PERMITS.
 THE GENERAL CONTRACTOR SHALL INSTALL ALL PERIMETER AND DOWN-GRADIENT EROSION CONTROL AND
- THE GENERAL CONTRACTOR SHALL INSTALL ALL PERIMETER AND DOWN-GRADIENT EROSION CONTROL AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES (BMPS), CONSTRUCTION ENTRANCES AND INLET PROTECTION DEVICES PRIOR TO SITE GRADING, EXCAVATION, STOCKPILING OR DISTURBING EXISTING VEGETATIVE COVER.
 THE GENERAL CONTRACTOR SHALL PERFORM SITE GRADING, EXCAVATION, STOCKPILING WORK IN ACCORDANCE WITH THE STORMWATER POLLUTION PREVENTION PLAN (SWPPP).
 THE GENERAL CONTRACTOR SHALL INSTALL, INSPECT, MONITOR AND MAINTAIN TEMPORARY AND PERMANENT
- EROSION CONTROL BMPS AS SHOWN ON PLANS CONTINUOUSLY DURING THE WORK. THE GENERAL CONTRACTOR SHALL STABILIZE ALL EXPOSED SOILS NO LATER THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

 6. THE GENERAL CONTRACTOR SHALL REPLACE OR REPAIR EROSION CONTROL AND SEDIMENT CONTROL BMPS THAT
- ARE NOT FUNCTIONING PROPERLY.
 7. THE GENERAL CONTRACTOR SHALL PERFORM SITE RESTORATION ACTIVITIES FOR PERMANENT VEGETATIVE
- ESTABLISHMENT.

 8 THE GENERAL CONTRACTOR SHALL REMOVE SEDIMENT CONTROL DEVICES WHEN SITE HAS REACHED.
- 8. THE GENERAL CONTRACTOR SHALL REMOVE SEDIMENT CONTROL DEVICES WHEN SITE HAS REACHED STABILIZATION.

1. CONSTRUCTION ACTIVITY FIELD REQUIREMENTS:

FURNISHED BY THE GENERAL CONTRACTOR.

- ALL FIELD REQUIREMENTS SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS OF THE
- STORMWATER POLLUTION PREVENTION PLAN (SWPPP).

 A. THE GENERAL CONTRACTOR MUST IMPLEMENT THE SWPPP AND PROVIDE BMPS IDENTIFIED IN THE SWPPP IN
- AN APPROPRIATE AND FUNCTIONAL MANNER.

 B. THE GENERAL CONTRACTOR SHALL RESPOND TO CHANGING SITE CONDITIONS AND IMPLEMENT/SUPPLEMENT EROSION PREVENTION AND SEDIMENT CONTROL MEASURES UTILIZED TO PROVIDE ADEQUATE PROTECTION OF DISTURBED SOILS AND ADEQUATE PREVENTION OF SEDIMENT TRANSPORT OFF—SITE. AT A MINIMUM, THE FOLLOWING STORM WATER POLLUTION PREVENTION CONSTRUCTION ACTIVITY FIELD REQUIREMENTS SHALL BE

2. EROSION PREVENTION PRACTICES

- THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING EROSION PREVENTION PRACTICES:

 A. THE GENERAL CONTRACTOR SHALL ATTEMPT TO PHASE ALL WORK TO MINIMIZE EROSION AND MAINTAIN VEGETATIVE COVER TO THE EXTENT POSSIBLE. THE LOCATION OF AREAS NOT TO BE DISTURBED MUST BE DELINEATED ON THE SITE BEFORE CONSTRUCTION BEGINS.
- B. STABILIZATION ON ALL EXPOSED SOILS MUST BE INITIATED IMMEDIATELY WHENEVER CONSTRUCTION ACTIVITY HAS PERMANENTLY OR TEMPORARILY CEASED ON ANY PORTION OF THE SITE AND WILL NOT RESUME FOR A PERIOD EXCEEDING <u>7 CALENDAR DAYS</u>, INCLUDING STOCKPILES WITH SIGNIFICANT SILT, CLAY OR ORGANIC COMPONENTS. STABILIZATION MUST BE COMPLETED WITHIN <u>7 CALENDAR DAYS</u>.
- C. THE NORMAL WETTED PERIMETER OF ANY TEMPORARY OR PERMANENT DRAINAGE DITCH THAT DRAINS WATER FROM A CONSTRUCTION SITE OR DIVERTS WATER AROUND A SITE MUST BE STABILIZED BY THE GENERAL CONTRACTOR WITHIN 200 FEET FROM THE PROPERTY EDGE, OR FROM THE POINT OF DISCHARGE TO ANY SURFACE WATER WITHIN 24 HOURS OF CONNECTING TO A SURFACE WATER. TEMPORARY OR PERMANENT DITCH SWALES BEING USED AS A SEDIMENT CONTAINMENT SYSTEM DO NOT NEED TO BE STABILIZED UNTIL THEY ARE NO LONGER USED AS A SEDIMENT CONTAINMENT SYSTEM, AFTER WHICH THEY MUST BE
- STABILIZED WITHIN 24 HOURS.

 D. TEMPORARY OR PERMANENT ENERGY DISSIPATION AT PIPE OUTLETS MUST BE PROVIDED WITHIN 24 HOURS OF CONNECTING TO A SURFACE WATER.
- E. THE GENERAL CONTRACTOR MUST DIRECT DISCHARGES FROM BMPS TO VEGETATED AREAS OF THE SITE IN ORDER TO INCREASE SEDIMENT REMOVAL AND MAXIMIZE INFILTRATION UNLESS INFEASIBLE. THE GENERAL CONTRACTOR MUST UTILIZE VELOCITY DISSIPATION DEVICES IF NECESSARY TO PREVENT EROSION WHEN DIRECTING STORMWATER TO VEGETATED AREAS.

3. SEDIMENT CONTROL PRACTICES

- THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING SEDIMENT CONTROL PRACTICES:

 A. THE GENERAL CONTRACTOR MUST INSTALL ALL DOWN GRADIENT PERIMETER CONTROLS BEFORE ANY UP GRADIENT DISTURBANCE BEGINS. THE GENERAL CONTRACTOR SHALL MAINTAIN PERIMETER CONTROLS UNTIL FINAL STABILIZATION HAS BEEN ESTABLISHED.
- B. THE GENERAL CONTRACTOR SHALL PROVIDE GRADING AND BMP INSTALLATION TO LIMIT ALL SLOPES OF 3H:1V OR STEEPER TO AN UNBROKEN LENGTH OF 75 FEET OR LESS.
- C. IF DOWN GRADIENT SEDIMENT CONTROLS ARE OVERLOADED, THE GENERAL CONTRACTOR MUST INSTALL ADDITIONAL UPGRADIENT SEDIMENT CONTROL PRACTICES OR REDUNDANT BMPS TO ELIMINATE OVERLOADING. THE SWPPP MUST BE AMENDED TO IDENTIFY THESES ADDITIONAL PRACTICES.
- D. TIMING AND INSTALLATION OF SEDIMENT CONTROL DEVICES CAN BE ADJUSTED BY THE GENERAL CONTRACTOR TO ACCOMMODATE SHORT-TERM ACTIVITIES SUCH AS CLEARING AND GRUBBING OR VEHICLE PASSAGE. ANY SHORT-TERM ACTIVITY MUST BE COMPLETED AS QUICKLY AS POSSIBLE AND THE SEDIMENT CONTROL PRACTICES MUST BE INSTALLED IMMEDIATELY AFTER THE ACTIVITY IS COMPLETED AND IN ALL CASES PRIOR TO THE NEXT PRECIPITATION EVENT.

- E. ALL STORM SEWER INLETS AND OUTLETS SHALL BE PROTECTED BY THE GENERAL CONTRACTOR WITH APPROPRIATE BMPS DURING THE WORK. THESE PRACTICES SHALL REMAIN IN PLACE UNTIL THE POTENTIAL SOURCES FOR DISCHARGING SEDIMENT TO INLETS HAVE BEEN STABILIZED BY THE GENERAL CONTRACTOR.
 F. TEMPORARY SOIL STOCKPILES MUST HAVE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS. SOIL STOCKPILES SHALL NOT BE PLACED IN SURFACE WATERS OR STORMWATER CONVEYANCES. ALL SOIL STOCKPILES THAT REMAIN UNDISTURBED FOR A PERIOD GREATER THAN 7 DAYS SHALL BE PROTECTED BY THE GENERAL CONTRACTOR WITH COVER OF MULCH, EROSION CONTROL MATS, OR PLASTIC SHEETING.
- G. THE GENERAL CONTRACTOR SHALL IMPLEMENT MEASURES TO CONTROL VEHICLE TRACKING OFF SITE. ROCK CONSTRUCTION ENTRANCES OR EQUIVALENT SYSTEM MUST BE INSTALLED BY THE GENERAL CONTRACTOR TO MINIMIZE TRACKING FROM SITE. THE GENERAL CONTRACTOR SHALL PROVIDE STREET SWEEPING AS NECESSARY IF BMPS ARE NOT ADEQUATE TO PREVENT SEDIMENT FROM BEING TRACKED ONTO THE STREET.
- H. THE GENERAL CONTRACTOR MUST MINIMIZE SOIL COMPACTION AND PRESERVE TOPSOIL, UNLESS INFEASIBLE. MINIMIZING SOIL COMPACTION IS NOT REQUIRED WHERE THE FUNCTION OF A SPECIFIC AREA OF THE SITE DICTATES THAT IT BE COMPACTED.
 I. THE GENERAL CONTRACTOR MUST PRESERVE A 50 FOOT NATURAL BUFFER OR PROVIDE REDUNDANT
- SEDIMENT CONTROLS WHEN A SURFACE WATER IS LOCATED WITHIN 50 FEET OF THE PROJECT DISTURBANCE LIMITS AND STORMWATER FLOWS TO THE SURFACE WATER.

 J. IF POLYMERS, FLOCCULANTS, OR OTHER SEDIMENTATION TREATMENT CHEMICALS ARE USED ON SITE, THE
- GENERAL CONTRACTOR MUST COMPLY WITH THE FOLLOWING REQUIREMENTS.

 a. THE GENERAL CONTRACTOR MUST USE CONVENTIONAL EROSION AND SEDIMENT CONTROLS PRIOR TO CHEMICAL ADDITION TO ENSURE EFFECTIVE TREATMENT. CHEMICALS MAY ONLY BE APPLIED WHERE TREATED STORMWATER IS DIRECTED TO A SEDIMENT CONTROL SYSTEM WHICH ALLOWS FOR THE SETTLEMENT OF THE FLOC PRIOR TO DISCHARGE.
- b. CHEMICALS MUST BE SELECTED THAT ARE APPROPRIATELY SUITED TO THE TYPES OF SOILS LIKELY TO BE EXPOSED DURING CONSTRUCTION. CHEMICALS MUST BE USED IN ACCORDANCE WITH ACCEPTED ENGINEERING PRACTICES, AND WITH DOSING SPECIFICATION AND SEDIMENT REMOVAL DESIGN SPECIFICATION PROVIDED BY THE MANUFACTURER.

4. <u>DEWATERING AND BASIN DRAINING</u> THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR THE FOLLOWING DEWATERING REQUIREMENTS:

- A. THE GENERAL CONTRACTOR'S DEWATERING ACTIVITIES THAT HAVE SEDIMENT-LADEN DISCHARGE WATER MUST BE DISCHARGED INTO A TEMPORARY OR PERMANENT SEDIMENTATION BASIN WHENEVER POSSIBLE, OTHERWISE IT MUST BE DISCHARGED THROUGH SOME FORM OF BEST MANAGEMENT PRACTICE (BMP) BY CONTRACTOR TO LIMIT SEDIMENT FROM LEAVING THE SITE. PRIOR TO DISCHARGE, THE GENERAL CONTRACTOR SHALL PERFORM A VISUAL TEST TO ENSURE ADEQUATE TREATMENT IS OBTAINED IN THE BASIN OR BMP AND APPLY ADDITIONAL TREATMENT AS REQUIRED TO ENSURE ADEQUATE TREATMENT.
- B. THE GENERAL CONTRACTOR SHALL DISCHARGE WATER FROM DEWATERING IN A MANNER THAT DOES NOT CAUSE NUISANCE CONDITIONS. THE DISCHARGE WATER SHALL BE DISPERSED OVER AN ACCEPTED ENERGY DISSIPATION MEASURE AND NOT ADVERSELY AFFECT THE RECEIVING WATER OR DOWNSTREAM LANDOWNERS OR WETLANDS.
- C. IF GENERAL CONTRACTOR IS USING FILTERS WITH BACKWASH WATER, THE GENERAL CONTRACTOR SHALL HAUL THE BACKWASH WATER AWAY FOR DISPOSAL, RETURN THE BACKWASH WATER TO THE BEGINNING OF THE TREATMENT PROCESS, OR INCORPORATE THE BACKWASH WATER INTO THE SITE IN A MANNER THAT DOES NOT CAUSE EROSION.

5. INSPECTIONS AND MAINTENANCE

- THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR PERFORMING THE FOLLOWING INSPECTIONS AND MAINTENANCE:
- A. WHEN INSPECTIONS FIND EROSION PREVENTION AND SEDIMENT CONTROL BMPS THAT ARE NONFUNCTIONAL, ALL NONFUNCTIONAL BMPS MUST BE REPAIRED, REPLACED, OR SUPPLEMENTED WITH FUNCTIONAL BMPS WITHIN 24 HOURS AFTER DISCOVERY. THE GENERAL CONTRACTOR SHALL ALSO PLACE ANY ADDITIONAL EROSION CONTROL MEASURES DEEMED NECESSARY WITHIN 24 HOURS OF NOTICE.
- B. THE GENERAL CONTRACTOR MUST ROUTINELY INSPECT THE SITE ONCE EVERY 7 DAYS DURING ACTIVE CONSTRUCTION AND WITHIN 24 HOURS AFTER A RAINFALL EVENT GREATER THAN 0.5 INCHES IN 24 HOURS.
 C. ALL INSPECTIONS AND MAINTENANCE CONDUCTED DURING CONSTRUCTION MUST BE RECORDED IN WRITING BY THE GENERAL CONTRACTOR AND RETAINED WITH THE SWPPP BY THE GENERAL CONTRACTOR. MAINTENANCE MUST BE COMPLETED BY THE GENERAL CONTRACTOR IN CONFORMANCE WITH NPDES PERMIT. THE GENERAL CONTRACTOR'S RECORDS MUST INCLUDE:
- a. DATE AND TIME OF INSPECTION.b. NAME OF PERSON CONDUCTING INSPECTION.
- c. FINDING OF INSPECTION INCLUDING RECOMMENDATIONS FOR CORRECTIVE ACTION.
- d. DETAILS OF CORRECTIVE ACTION TAKEN (DATE. TIME. PARTY COMPLETING MAINTENANCE ACTIVITIES)
- e. DATE AND AMOUNT OF RAINFALL GREATER THAN 0.5 INCHES IN 24 HOURS.
 f. IF ANY DISCHARGE IS OBSERVED TO BE OCCURRING DURING THE INSPECTION, A RECORD OF ALL POINTS OF THE PROPERTY FROM WHICH THERE IS A DISCHARGE MUST BE MADE, AND THE
- DISCHARGE SHALL BE DESCRIBED (COLOR, ODOR, FLOATING, SETTLED, OR SUSPENDED SOLIDS, FOAM, OIL SHEEN, AND OTHER INDICATORS) AND PHOTOGRAPHED.

 g. DOCUMENTATION OF CHANGES MADE TO SWPPP.

 D. IN AREAS OF PROJECT WHERE FINAL STABILIZATION IS COMPLETE INSPECTIONS CAN BE REDUCED TO ONCE A MONTH. THESE AREAS SHALL BE INSPECTED BY THE GENERAL CONTRACTOR FOR MINIMUM PERIOD OF 12
- NON-WINTER MONTHS AND WITHIN 24 HOURS OF FIRST SPRING RUNOFF OR PRIOR TO RESUMING CONSTRUCTION FOLLOWING ANY WINTER STOPPAGE, WHICHEVER COMES FIRST.

 E. THE GENERAL CONTRACTOR IS RESPONSIBLE FOR THE INSPECTION AND MAINTENANCE OF BMPS UNTIL
- ANOTHER PERMITTEE HAS OBTAINED COVERAGE, OR THE PROJECT HAS UNDERGONE FINAL STABILIZATION AND A NOTICE OF TERMINATION (N.O.T.) HAS BEEN SUBMITTED TO THE MPCA.

 F. ALL EROSION CONTROL MEASURES MUST BE INSTALLED AND MAINTAINED BY THE GENERAL CONTRACTOR
- ACCORDING TO THE DETAILS INCLUDED IN THE CONSTRUCTION DOCUMENTS AND IN ACCORDANCE WITH THE PRODUCT MANUFACTURER'S RECOMMENDATIONS. ALL NONFUNCTIONAL BMPS MUST BE REPAIRED OR REPLACED WITHIN 24 HOURS OF DISCOVERY, OR AS SOON AS FIELD CONDITIONS ALLOW.

 G. ALL PERIMETER CONTROL DEVICES MUST BE REPAIRED, REPLACED OR SUPPLEMENTED BY THE GENERAL
- G. ALL PERIMETER CONTROL DEVICES MUST BE REPAIRED, REPLACED OR SUPPLEMENTED BY THE GENERAL CONTRACTOR WHEN THEY BECOME NON-FUNCTIONAL OR THE SEDIMENT REACHES ONE-HALF THE HEIGHT OF THE DEVICE. THE GENERAL CONTRACTOR SHALL REPAIR OR REPLACE DEVICE THAT IS NONFUNCTIONAL BY THE END OF THE NEXT BUSINESS DAY AFTER DISCOVERY, OR THEREAFTER AS SOON AS FIELD CONDITIONS ALLOW.
- H. TEMPORARY AND PERMANENT SEDIMENTATION BASINS MUST BE DRAINED AND SEDIMENT REMOVED BY THE GENERAL CONTRACTOR ONCE THE SEDIMENT COLLECTED REACHES ONE HALF THE STORAGE VOLUME WITH 72 HOURS OF DISCOVERY, OR AS SOON AS FIELD CONDITIONS ALLOW.
 I. ALL SEDIMENT DEPOSITS WITHIN SURFACE WATERS OR STORMWATER CONVEYANCES MUST BE REMOVED AND RESTABILIZED BY THE GENERAL CONTRACTOR WITHIN 7 DAYS OF DISCOVERY, INCLUDING DELTAS AND STORM SEWER SEDIMENT DEPOSITS. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL
- PERMITS REQUIRED, IF NECESSARY, FOR SUCH SEDIMENT REMOVAL.

 J. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING EXISTING PAVED SURFACES CLEAN OF SEDIMENT. CONSTRUCTION ENTRANCES SHALL BE CHECKED DAILY BY THE GENERAL CONTRACTOR. IF THE ENTRANCE BECOMES INUNDATED WITH SEDIMENT, THE ENTRANCE WILL BE CLEANED OR REPLACED AS APPROPRIATE BY THE GENERAL CONTRACTOR. STREETS LEADING TO AND FROM THE CONSTRUCTION ENTRANCE SHALL BE CHECKED DAILY BY THE GENERAL CONTRACTOR FOR OFF—SITE SEDIMENT TRACKING ONTO PAVED SURFACES. THESE AREAS WILL BE SWEPT CLEAN OF ANY TRACKED MATERIALS BY THE GENERAL CONTRACTOR AS SOON AS POSSIBLE AND WITHIN 24 HOURS OF DISCOVERY. THE GENERAL CONTRACTOR SHALL EXTEND SWEEPING TO THE EXTREMITY OF ANY SEDIMENT TRACKING THAT OCCURS
- K. THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE TO REMOVE ANY OFF—SITE SEDIMENT ACCUMULATIONS IN A MANNER AND AT A FREQUENCY SUFFICIENT TO MINIMIZE OFF—SITE IMPACTS.
 L. ALL INFILTRATION/FILTRATION AREAS MUST BE INSPECTED BY GENERAL CONTRACTOR TO ENSURE THAT NO SEDIMENT FROM ONGOING CONSTRUCTION IS ACCUMULATING OVER THE INFILTRATION/FILTRATION AREA.
- SEDIMENT ACCUMULATED OVER INFILTRATION /FILTRATION MUST BE REMOVED BY THE GENERAL CONTRACTOR.

 M. THE GENERAL CONTRACTOR SHALL PROTECT INFILTRATION/FILTRATION AREAS FROM SEDIMENTATION AND OVER—COMPACTION. DURING EXCAVATION, SEDIMENT AND EROSION CONTROL DEVICES MUST BE UTILIZED BY THE CENERAL CONTRACTOR TO PREVENT SEDIMENTATION AND THE AREA MUST BE STAKED OFF AND
- OVER-COMPACTION. DURING EXCAVATION, SEDIMENT AND EROSION CONTROL DEVICES MUST BE UTILIZED BY THE GENERAL CONTRACTOR TO PREVENT SEDIMENTATION AND THE AREA MUST BE STAKED OFF AND MARKED SO THAT HEAVY CONSTRUCTION EQUIPMENT WILL NOT COMPACT THE SOIL.

 N. INSPECTIONS CAN BE SUSPENDED DUE TO FROZEN GROUND CONDITIONS UNTIL FIRST RUNOFF OCCURS OR

6. POLLUTION PREVENTION MEASURES

CONSTRUCTION ACTIVITIES RESUME.

- THE GENERAL CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTING THE FOLLOWING POLLUTION PREVENTION MANAGEMENT MEASURES ON THE SITE:
- A. THE GENERAL CONTRACTOR SHALL MINIMIZE THE EXPOSURE OF ALL PRODUCTS, MATERIALS, AND WASTES FROM STORMWATER WHICH MAY BE A SOURCE OF CONTAMINATION TO STORMWATER OR ARE NOT DESIGNED TO BE EXPOSED TO STORMWATER.
- B. BUILDING PRODUCTS THAT MAY LEACH POLLUTANTS MUST BE UNDER COVER (PLASTIC SHEETING,
 TEMPORARY ROOFS, ETC.) TO PREVENT THE DISCHARGE OF POLLUTANTS OR PROTECTED BY A SIMILARLY
 EFFECTIVE MEANS DESIGNED TO MINIMIZE CONTACT WITH STORMWATER.
 C. PESTICIDES, HERBICIDES, INSECTICIDES, FERTILIZERS, TREATMENT CHEMICALS, AND LANDSCAPE MATERIALS
- MUST BE UNDER COVER (PLASTIC SHEETING, TEMPORARY ROOFS, ETC.) TO PREVENT THE DISCHARGE OF POLLUTANTS OR PROTECTED BY A SIMILARLY EFFECTIVE MEANS DESIGNED TO MINIMIZE CONTACT WITH STORMWATER.

 D. HAZARDOUS MATERIALS, TOXIC WASTE, (INCLUDING OIL, DIESEL FUEL, GASOLINE, HYDRAULIC FLUIDS, PAINT
- SOLVENTS, PETROLEUM-BASED PRODUCTS, WOOD PRESERVATIVES, ADDITIVES, CURING COMPOUNDS, AND ACIDS) MUST BE STORED IN SEALED CONTAINERS TO PREVENT SPILLS, LEAKS OR OTHER DISCHARGE. RESTRICTED ACCESS STORAGE AREAS MUST BE PROVIDED TO PREVENT VANDALISM. STORAGE AND DISPOSAL OF HAZARDOUS MATERIALS MUST COMPLY WITH ALL STATE REQUIREMENTS.

 E. SOLID WASTE MUST BE STORED, COLLECTED, AND DISPOSED IN COMPLIANCE WITH ALL STATE
- REQUIREMENTS.
- F. PORTABLE TOILETS MUST BE POSITIONED SO THAT THEY ARE SECURE AND WILL NOT BE TIPPED OVER.
 SANITARY WASTE MUST BE DISPOSED OF IN ACCORDANCE WITH ALL STATE REQUIREMENTS.
 G. THE GENERAL CONTRACTOR SHALL TAKE REASONABLE STEPS TO PREVENT THE DISCHARGE OF SPILLED OR
- LEAKED CHEMICALS, INCLUDING FUEL, FROM ALL AREAS WHERE CHEMICALS OR FUEL WILL BE LOADED OR UNLOADED. THE GENERAL CONTRACTOR MUST CONDUCT FUELING IN A CONTAINED AREA UNLESS INFEASIBLE. THE GENERAL CONTRACTOR MUST ENSURE ADEQUATE SUPPLIES ARE AVAILABLE AT ALL TIMES TO CLEAN UP DISCHARGED MATERIALS AND THAT AN APPROPRIATE DISPOSAL METHOD IS AVAILABLE FOR RECOVERED SPILLED MATERIALS. ALL SPILLS MUST BE CLEANED UP AND REPORTED IN ACCORDANCE WITH STATE REQUIREMENTS. DRY CLEAN UP MEASURES SHALL BE USED WHERE POSSIBLE.

 H. THE GENERAL CONTRACTOR MUST LIMIT VEHICLE AND EQUIPMENT WASHING TO A DEFINED AREA WHEN
- COMPLETED ON THE PROJECT SITE. RUNOFF FROM THE WASHING AREA MUST BE CONTAINED IN A SEDIMENT BASIN OR OTHER SIMILARLY EFFECTIVE CONTROLS AND WASTE FROM THE WASHING ACTIVITY MUST BE PROPERLY DISPOSED OF. THE GENERAL CONTRACTOR MUST PROPERLY USE AND STORE SOAPS, DETERGENTS, OR SOLVENTS. NO ENGINE DEGREASING IS ALLOWED ONSITE.

 I. THE GENERAL CONTRACTOR MUST PROVIDE EFFECTIVE CONTAINMENT FOR ALL LIQUID AND SOLID WASTES GENERATED BY WASHOUT OPERATIONS (CONCRETE, STUCCO, PAINT, FORM RELEASE OILS, CURING COMPOUNDS, AND OTHER CONSTRUCTION MATERIALS) RELATED TO THE PROJECT CONSTRUCTION ACTIVITY. NO WASHOUT WASTES MAY CONTACT THE GROUND, AND THE CONTAINMENT MUST BE DESIGNED SO THAT IT DOES NOT RESULT IN RUNOFF FROM THE WASHOUT OPERATIONS OR AREAS. LIQUID AND SOLID WASTES

MUST BE DISPOSED OF PROPERLY AND IN COMPLIANCE WITH ALL MPCA RULES. A SIGN MUST BE

FACILITIES FOR DISPOSAL OF CONCRETE AND OTHER WASHOUT WASTES.

INSTALLED ADJACENT TO EACH WASHOUT FACILITY THAT REQUIRES SITE PERSONNEL TO UTILIZE PROPER

7. FINAL STABILIZATION

- THE GENERAL CONTRACTOR SHALL ENSURE FINAL STABILIZATION OF THE SITE. FINAL STABILIZATION REQUIRES THE FOLLOWING:
- A. ALL SOIL DISTURBING ACTIVITIES ARE COMPLETE AND A UNIFORM PERENNIAL VEGETATIVE COVER WITH A DENSITY OF 70% OVER THE ENTIRE PERVIOUS SURFACE HAS BEEN ACHIEVED, INCLUDING STABILIZATION OF ALL DITCHES AND SWALES.
- ALL DITCHES AND SWALES.

 B. THE GENERAL CONTRACTOR SHALL ENSURE THAT ALL PERMANENT STORMWATER TREATMENT SYSTEMS ARE
- CONSTRUCTED IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS IF THERE ARE ONSITE SYSTEMS.

 C. THE GENERAL CONTRACTOR SHALL REMOVE ALL TEMPORARY SYNTHETIC AND STRUCTURAL BMPS.
- D. THE GENERAL CONTRACTOR SHALL REMOVE ALL SEDIMENTS FROM STORM WATER CONVEYANCES AND PERMANENT WATER QUALITY BASINS.

INSPECTION AND ENTRY:

-THE GENERAL CONTRACTOR MUST ALLOW ACCESS AS REQUIRED BY CITY AND STATE REGULATIONS FOR REPRESENTATIVES OF THE CITY OR MPCA OR ANY MEMBER THEREOF WHEN AUTHORIZED BY IT, TO ENTER UPON THE PROJECT SITE FOR THE PURPOSE OF OBTAINING INFORMATION, EXAMINATION OF RECORDS, OR CONDUCTING SURVEYS OR INVESTIGATIONS.

8. CHANGES TO SWPPP

RAINFALL EVENT GREATER THAN ONE-HALF INCH.

- -THE PERMITTEE MUST AMEND THE SWPPP AS NECESSARY TO INCLUDE ADDITIONAL REQUIREMENTS, SUCH AS ADDITIONAL OR MODIFIED BMPS, DESIGNED TO CORRECT PROBLEMS IDENTIFIED OR ADDRESS SITUATIONS WHENEVER; 1. THERE IS A CHANGE IN DESIGN, CONSTRUCTION, OPERATION OR MAINTENANCE, WEATHER OR SEASONAL CONDITIONS THAT HAVE SIGNIFICANT EFFECT ON DISCHARGE. INSPECTION IS REQUIRED WITHIN 24 HOURS OF A
- INSPECTION OR INVESTIGATION BY SITE OPERATORS, LOCAL, STATE OR FEDERAL OFFICIALS INDICATE THE SWPPP IS NOT EFFECTIVE.
 THE SWPPP IS NOT ACHIEVING THE GENERAL OBJECTIVES OF CONTROLLING POLLUTANTS OR THE SWPPP IS NOT CONSISTENT WITH THE TERMS AND CONDITIONS OF THIS PERMIT.
- 4. THE MPCA DETERMINES THAT DISCHARGE MAY CAUSE OR CONTRIBUTE TO NON-ATTAINMENT OF ANY APPLICABLE WATER QUALITY STANDARDS OR THE SWPPP DOES NOT INCORPORATE THE REQUIREMENTS RELATED TO AN APPROVED TOTAL MAXIMUM DAILY LOAD (TMDL).

SWPPP CERTIFICATION:

 THIS STORMWATER POLLUTION PREVENTION PLAN WAS PREPARED BY INDIVIDUAL(S) TRAINED IN ACCORDANCE WITH THE PERMIT'S TRAINING REQUIREMENTS FOR PREPARATION OF SWPPPS. INDIVIDUAL(S) PREPARING THIS SWPPP:

PREPARED BY:
TIM SETALA, P.E.
PIERCE PINI AND ASSOCIATES
TIM@PIERCEPINI.COM
763-537-1311

TRAINING/CERTIFICATION:

DATE OF TRAINING/CERTIFICATION: 2019
CERTIFICATION PROGRAM: UNIVERSITY OF MINNESOTA — DESIGN OF CONSTRUCTION SWPPP — ARDEN HILLS, MN
INSTRUCTOR(S): JOHN CHAPMAN
CERTIFICATION EXPIRATION: 2022



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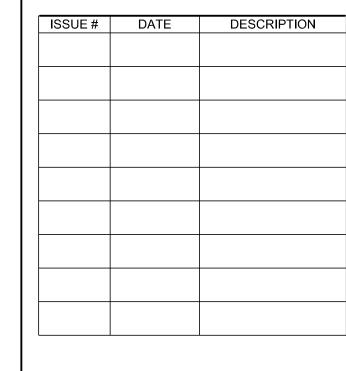
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PROJECT TITLE

SHELBY COMMONS



PDR/LAND USE 10-02-2020

CERTIFICATION

I hereby certify that this plan, specification, or

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supervision and that I am a duly Licensed

Professional Engineer under the laws of the

State of Minnesota.

Rhonda S. Pierce

License Number

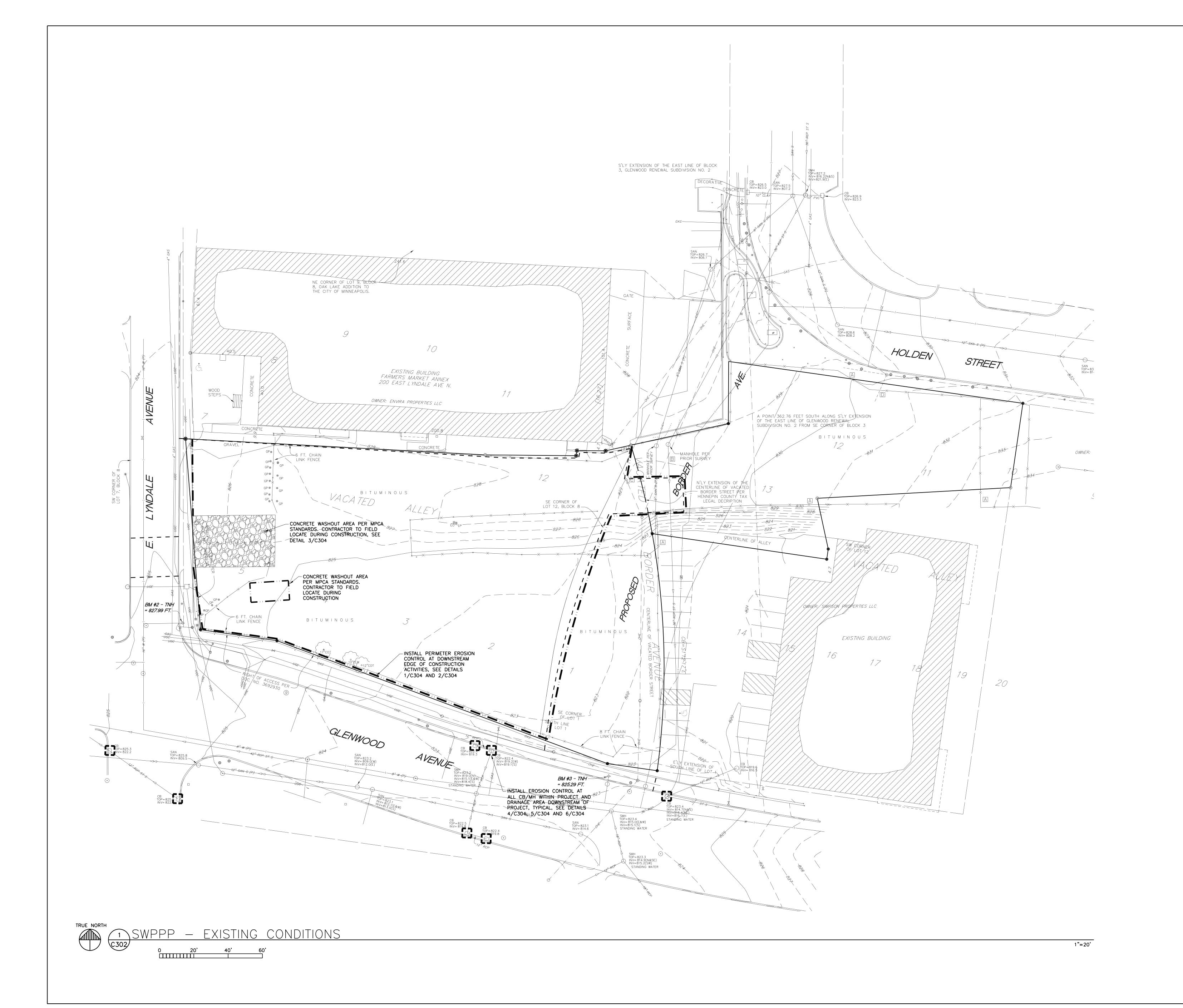
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SHEET TITLE

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SHEET NUMBER

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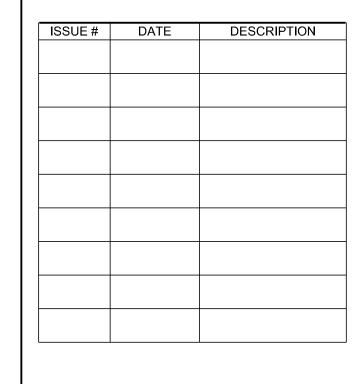
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I hereby certify that this plan, specification, or report was prepared by me or under my direct

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41333 License Number

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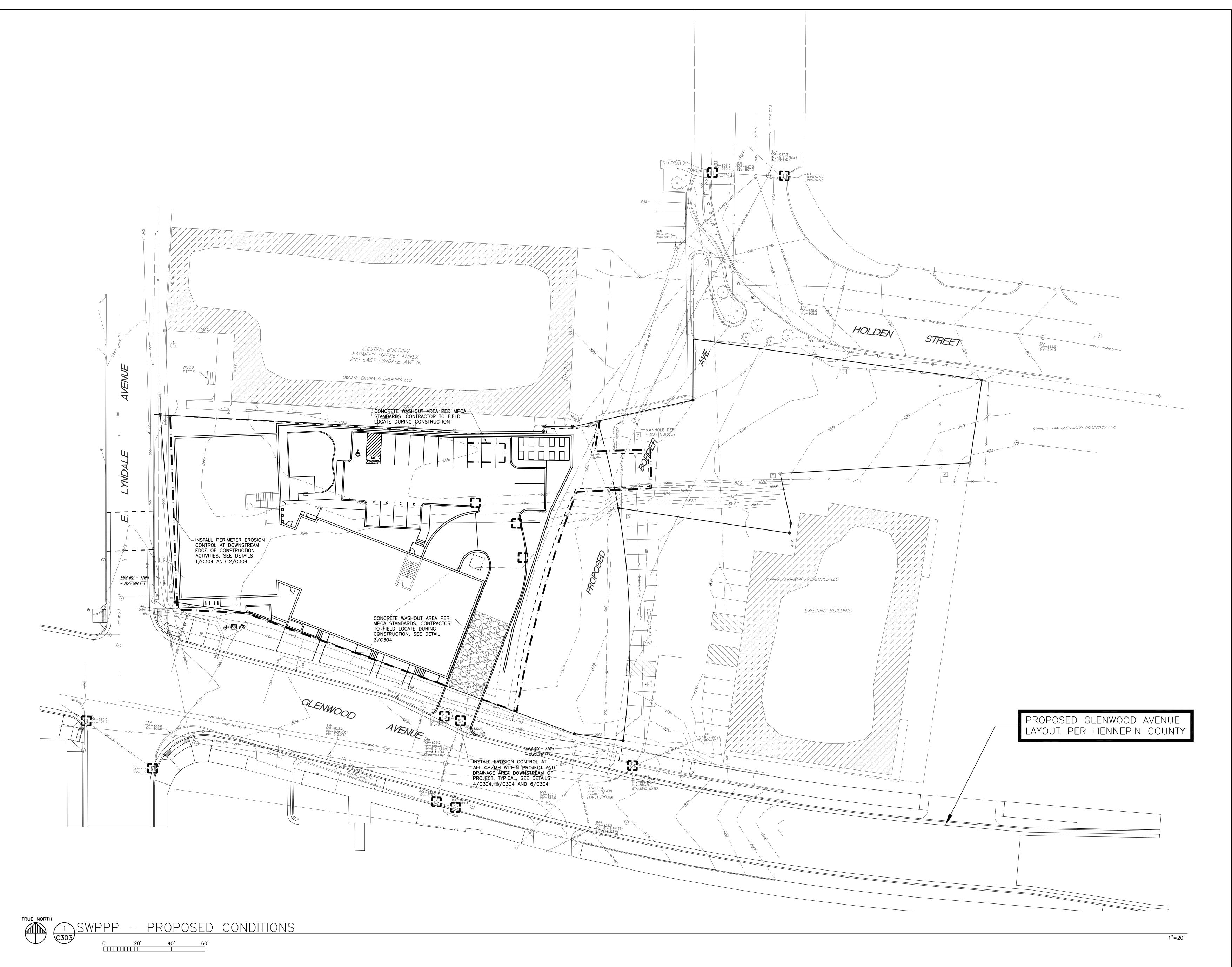
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CONDITIONS

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PROJECT TITLE

SHELBY COMMONS

	ISSUE#	DATE	DESCRIPTION
ŀ			

PDR/LAND USE 10-02-2020
CERTIFICATION

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Rhonda S. Pierce
41333
License Number

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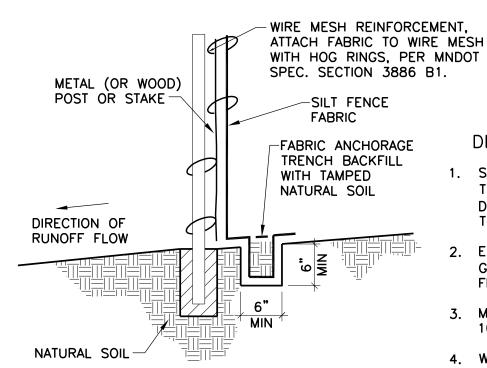
SWPPP -PROPOSED

CONDITIONS

SHEET NUMBER

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DESIGN RECOMMENDATIONS

SILT FENCES SHOULD BE INSTALLED ON THE CONTOUR (AS OPPOSED TO UP AND DOWN A HILL) AND CONSTRUCTED SO THAT FLOW CANNOT BYPASS THE ENDS. ENSURE THAT THE DRAINAGE AREA IS NO GREATER THAN 1/4 ACRE PER 100 FT OF

3. MAKE THE FENCE STABLE FOR THE 10-YEAR PEAK STORM RUNOFF.

4. WHERE ALL RUNOFF IS TO BE STORED BEHIND THE SILT FENCE, ENSURE THAT THE MAXIMUM SLOPE LENGTH BEHIND THE FENCE DOES NOT EXCEED THE SPECIFICATIONS SHOWN IN TABLE 1.

NO SCALE

SURFACE.

FIGURE 1 TYPICAL INSTALLATION FOR SILT FENCE

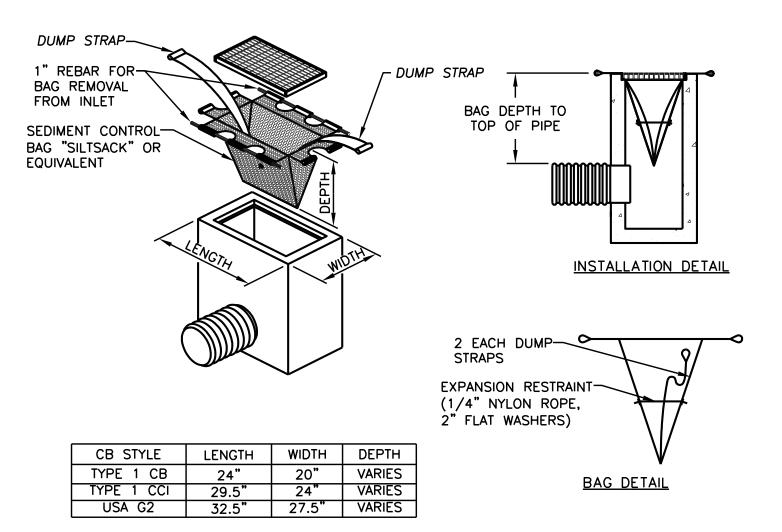
NOTE: SILT FENCE SHALL FOLLOW

MNDOT SPEC. SECTION 3886.

TABLE 1 MAXIMUM SLOPE LENGTH AND SLOPE FOR WHICH SILT FENCE IS APPLICABLE

		BY CALCULATION	BY CALCULATION	BY ACCEPTED DESIGN PRACTICES
SLOPE H:V	PERCENT	SILT FENCE STORAGE EQUALS 2 FT FOR A 100-YEAR EVENT	SILT FENCE STORAGE EQUALS 2 FT FOR A 2-YEAR EVENT OR 3 FT FOR A 100-YEAR EVENT	MAXIMUM SLOPE LENGTH
100:1	1%	400 FT	900 FT	100 FT
50:1	2%	200 FT	450 FT	75 FT
25:1	4%	100 FT	225 FT	75 FT
20:1	5%	80 FT	180 FT	75-50 FT
17:1	6%	67 FT	150 FT	50 FT
12.5:1	8%	50 FT	112 FT	50 FT
10:1	10%	40 FT	90 FT	50-25 FT
5:1	20%	20 FT	45 FT	25-15 FT
4:1	25%	16 FT	36 FT	15 FT
3:1	33%	12 FT	27 FT	15 FT
2:1	50%	8 FT	18 FT	15 FT

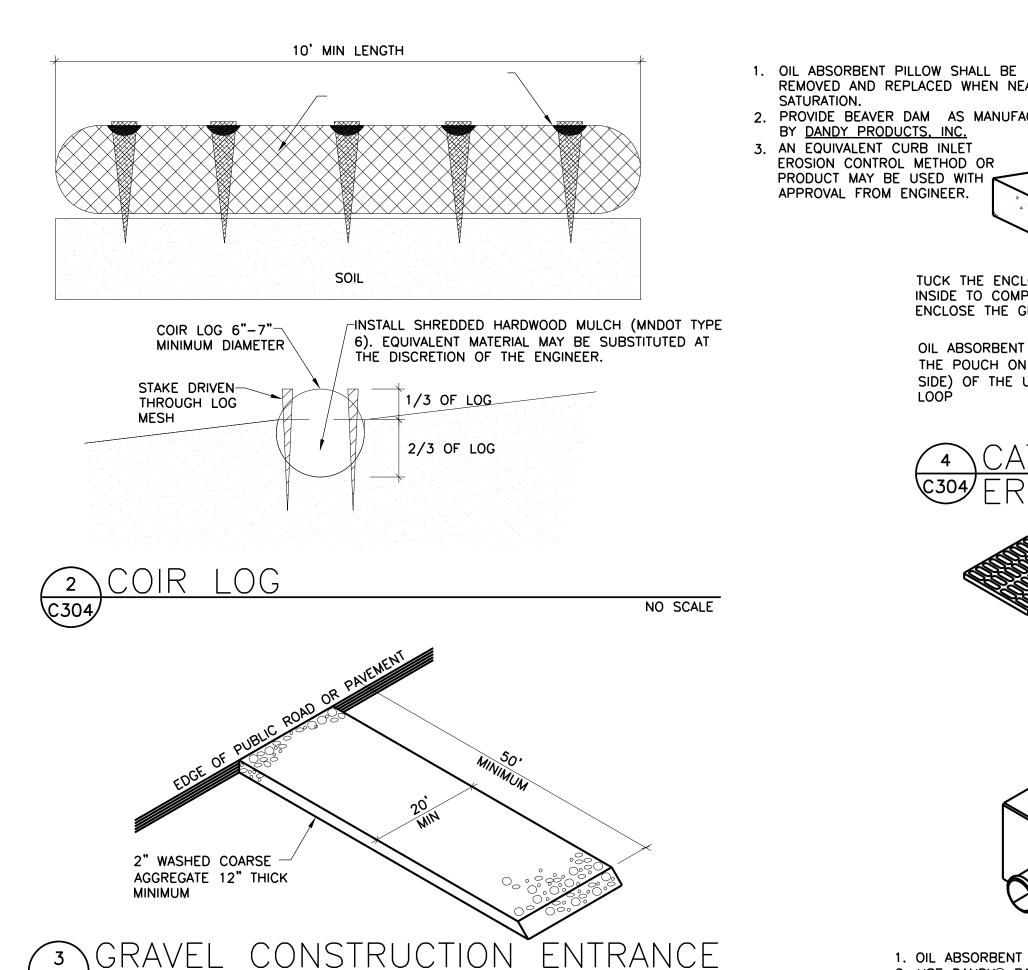


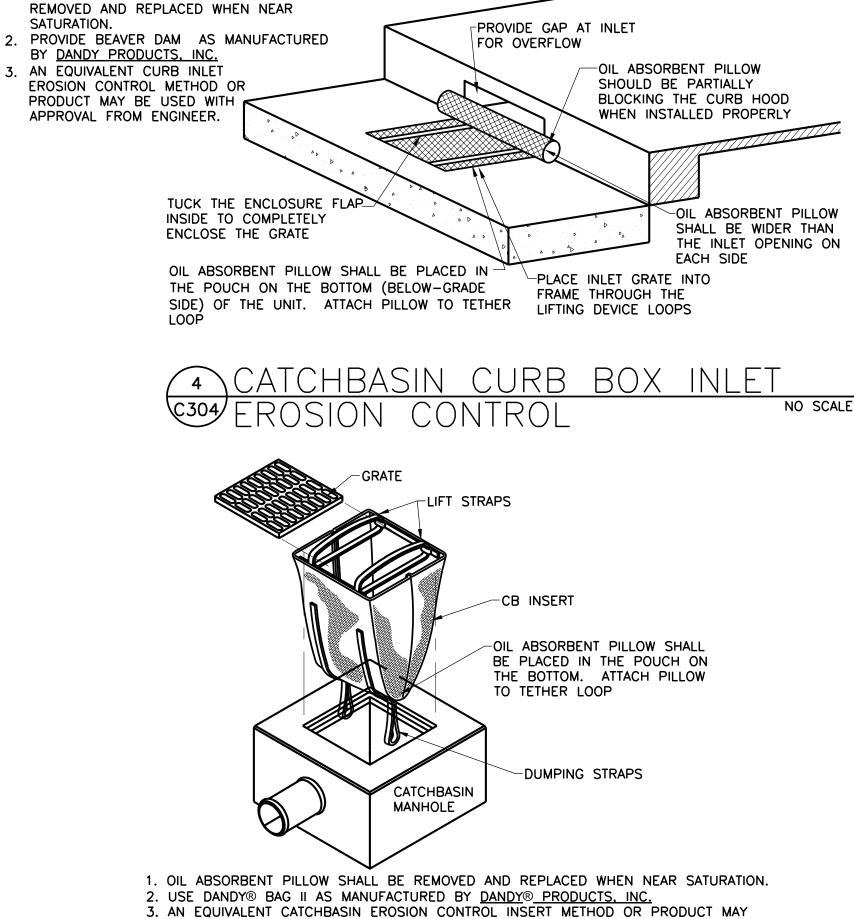


INLET SEDIMENT CONTROL DEVICE - SILT SACK

- 1. THE DIMENSION CHART ABOVE IS FOR STANDARD CATCH BASINS AND INLETS ONLY. THE
- CONTRACTOR IS RESPONSIBLE FOR PROVIDING THE CORRECT SIZE DEVICE FOR EACH INLET. 2. FOR NON-STANDARD CATCH BASINS AND INLETS, THE CONTRACTOR SHALL MEASURE DIMENSIONS IN THE FIELD AND ORDER THE APPROPRIATE SIZE(S).
- 3. THE INLET SEDIMENT CONTROL DEVICE SHALL BE OF HIGH FLOW DESIGN (200 GAL/MIN/FT), AS PER THE MANUFACTURER'S SPECS.
- 4. THE SEDIMENT CONTROL DEVICE SHALL BE INSPECTED DAILY BY THE CONTRACTOR AND MAINTAINED A MINIMUM ONCE PER MONTH OR WITHIN THE 48 HOURS FOLLOWING A STORM EVENT. FILTER SHALL BE CLEANED IN A MANNER WHICH ENSURES THAT ALL SEDIMENT REMAINS
- 5. SUBSTITUTION OF A SHEET OF FILTER FABRIC PLACED OVER THE OPENING OF THE INLET IS NOT APPROVED.
- 6. RECESSED CURB INLET CATCH BASINS MUST BE BLOCKED WHEN USING FILTER FABRIC INLET SACKS, SIZE OF FILTER INLET SACK TO BE DETERMINED BY MANUFACTURER.
- 7. THE FILTER SHALL BE REPLACED OR CLEANED WHEN THE BAG BECOMES HALF FULL.

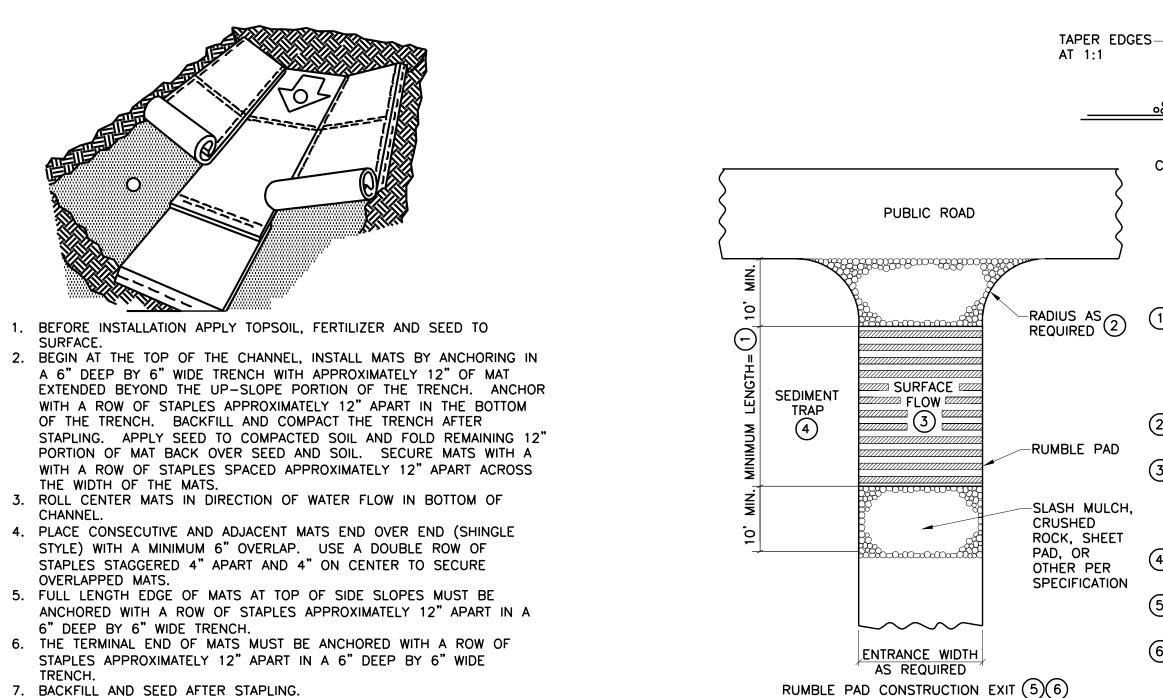






(5) CB INSERT EROSION CONTROL

BE USED WITH PRIOR APPROVAL FROM ENGINEER.



COMPACTED SOIL-TO 2" CRUSHED ROCK OR SLASH MULCH (1) MINIMUM LENGTH OF RUMBLE PAD SHALL BE 20 FEET, OR AS REQUIRED TO REMOVE SEDIMENT FROM TIRES. IF SIGNIFICANT SEDIMENT IS TRACKED FROM THE SITE, THE RUMBLE PAD SHALL BE LENGTHENED OR THE DESIGN MODIFIED TO PROVIDE ADDITIONAL VIBRATION. WASH-OFF LENGTH SHALL BE AS REQUIRED TO EFFECTIVELY REMOVE CONSTRUCTION SEDIMENT FROM VEHICLE TIRES. (2) PROVIDE RADIUS OR WIDEN PAD SUFFICIENTLY TO PREVENT VEHICLE TIRES FROM TRACKING OFF OF PAD WHEN LEAVING SITE. (3) IF RUNOFF FROM DISTURBED AREAS FLOWS TOWARD CONSTRUCTION EXITS, PREVENT RUNOFF FROM DRAINING DIRECTLY TO PUBLIC ROAD OVER CONSTRUCTION EXIT BY CROWNING THE EXIT OR SLOPING TO ONE SIDE. IF SURFACE GRADING IS INSUFFICIENT, PROVIDE OTHER MEANS OF INTERCEPTING (4) IF RUNOFF FROM CONSTRUCTION EXITS WILL DRAIN OFF OF PROJECT SITE, PROVIDE SEDIMENT TRAP WITH STABILIZED OVERFLOW.

-CORRUGATED STEEL PANELS

-GEOTEXTILE FABRIC

-CROSS SLOPE 3% OR FLATTER

 $^{ackslash}6$ " MIN. DEPTH OF 1"

(5) IF A TIRE WASH OFF IS REQUIRED THE CONSTRUCTION EXITS SHALL BE GRADED TO DRAIN THE WASH WATER TO A SEDIMENT TRAP.

(6) MAINTENANCE OF CONSTRUCTION EXITS SHALL OCCUR WHEN THE TEFFECTIVENESS OF SEDIMENT REMOVAL HAS BEEN REDUCED. MAINTENANCE SHALL CONSIST OF REMOVING SEDIMENT AND CLEANING THE MATERIALS OR PLACING ADDITIONAL MATERIAL (SLASH MULCH OR CRUSHED ROCK) OVER SEDIMENT FILLED MATERIAL TO RESTORE EFFECTIVENESS.

7 EROSION STABILIZATION MATS
NO SCALE

8. FOLLOW MANUFACTURER'S RECOMMENDATIONS FOR PROPER

RUMBLE PAD CONSTRUCTION EXIT

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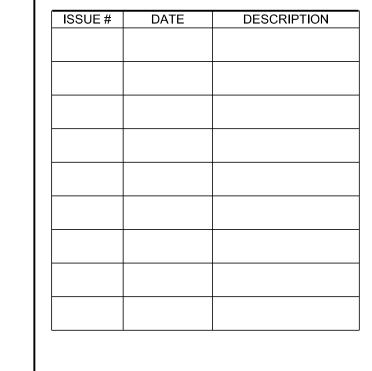
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CONSULTANTS

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PROJECT TITLE

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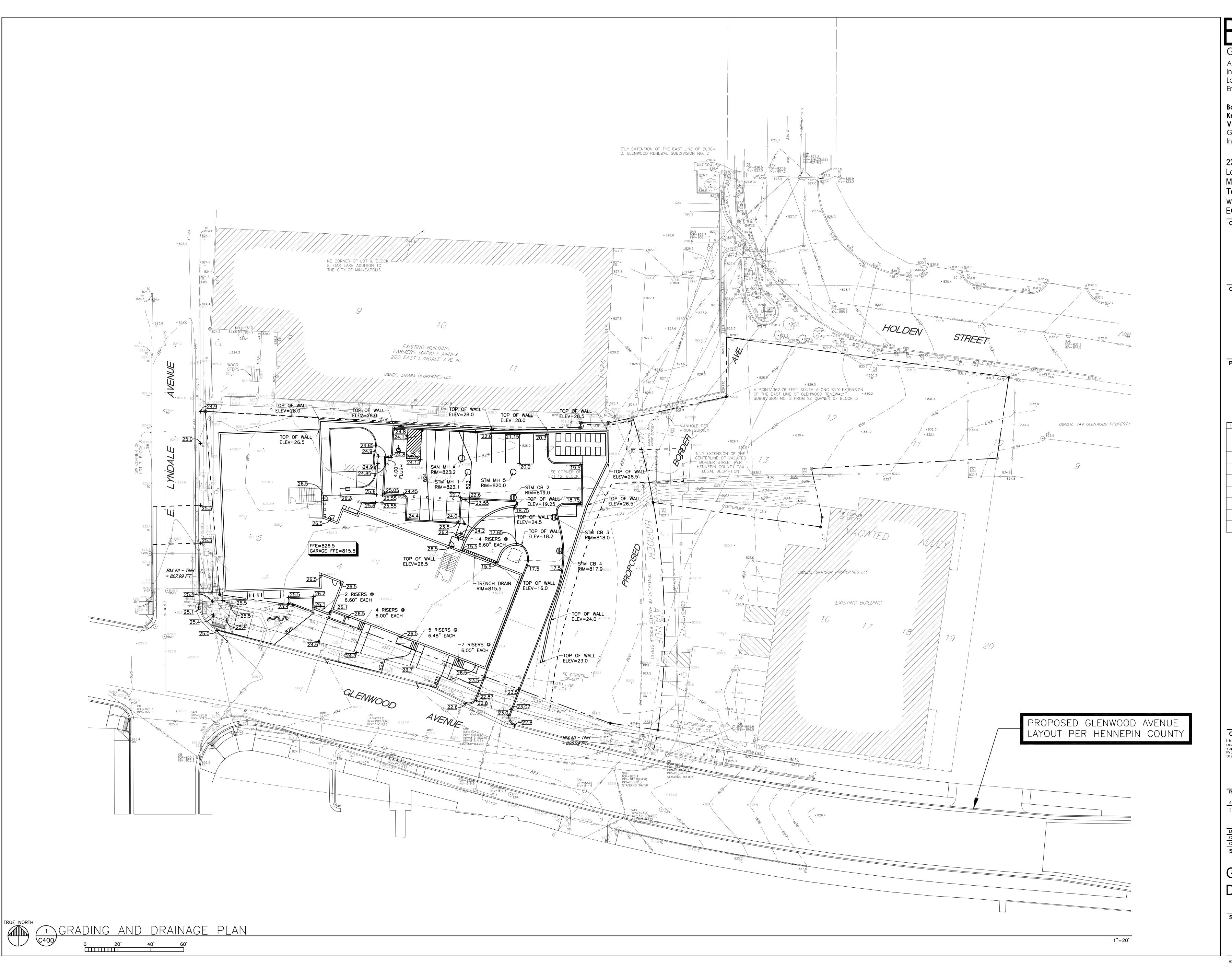
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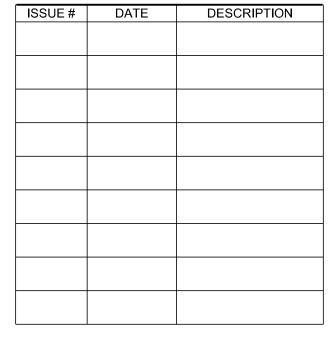
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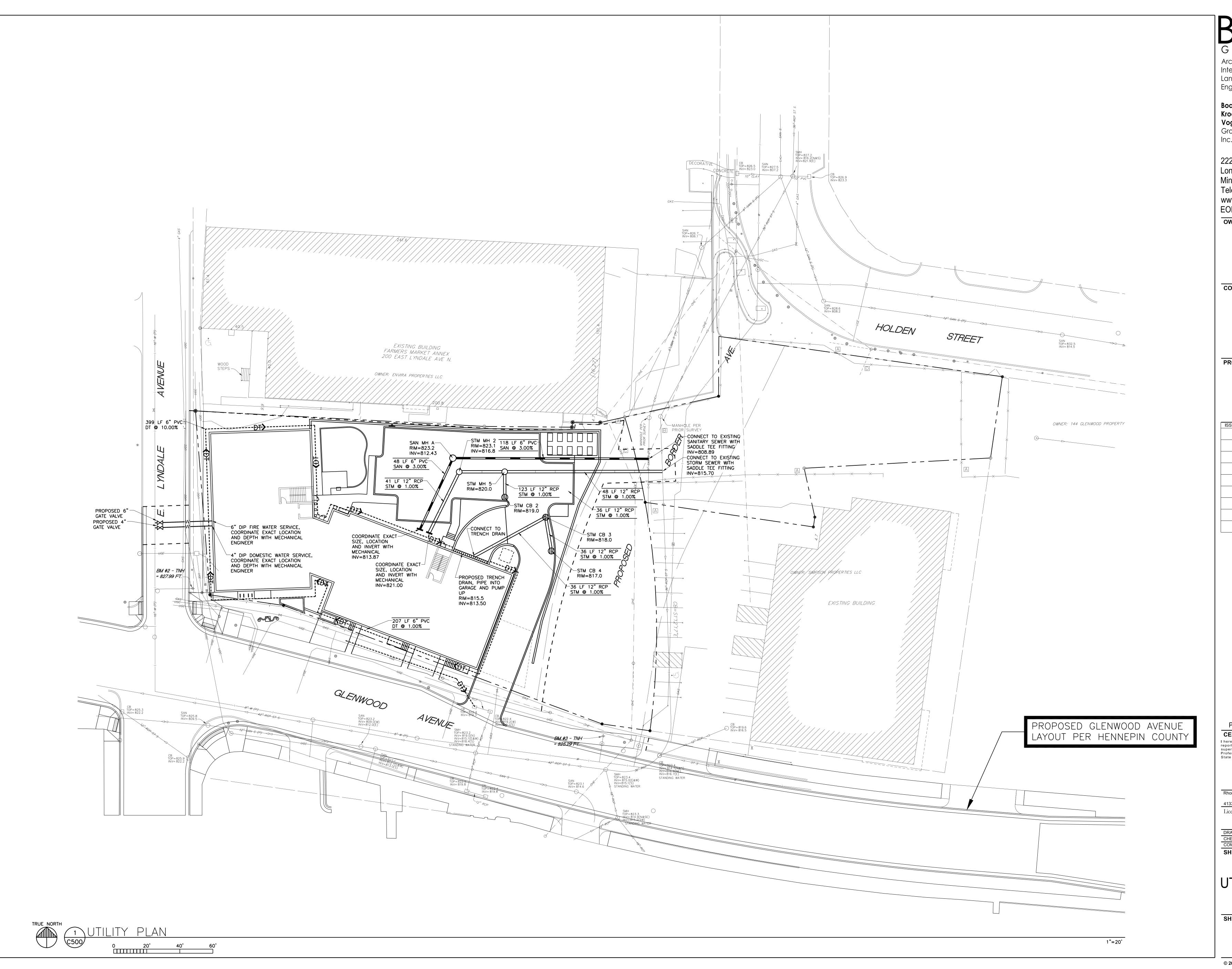
GRADING AND

DRAINAGE PLAN

SHEET NUMBER

C400

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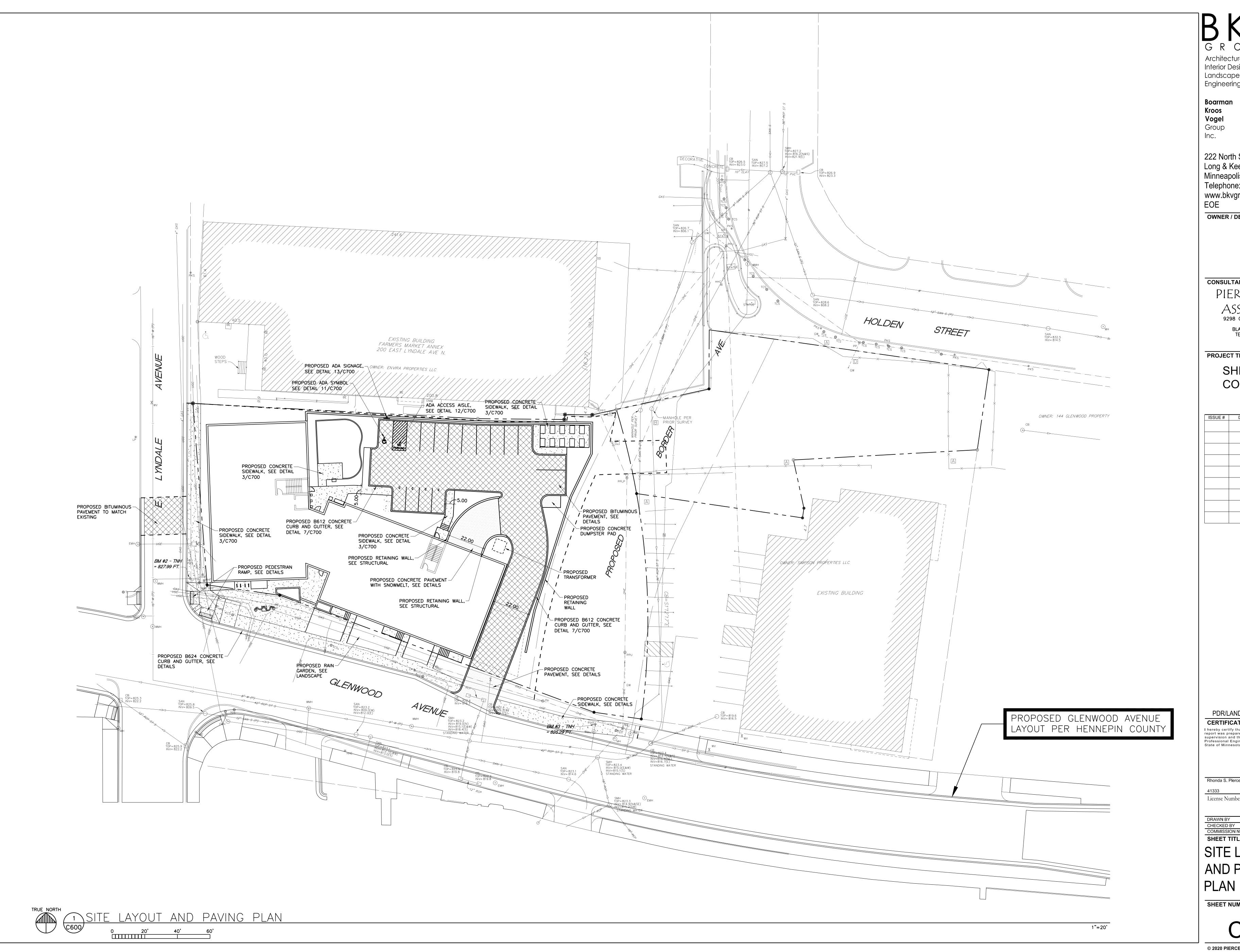
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SHEET TITLE

UTILITY PLAN

SHEET NUMBER

C500

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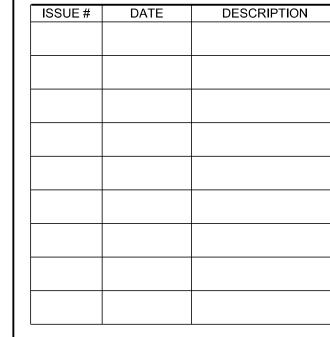
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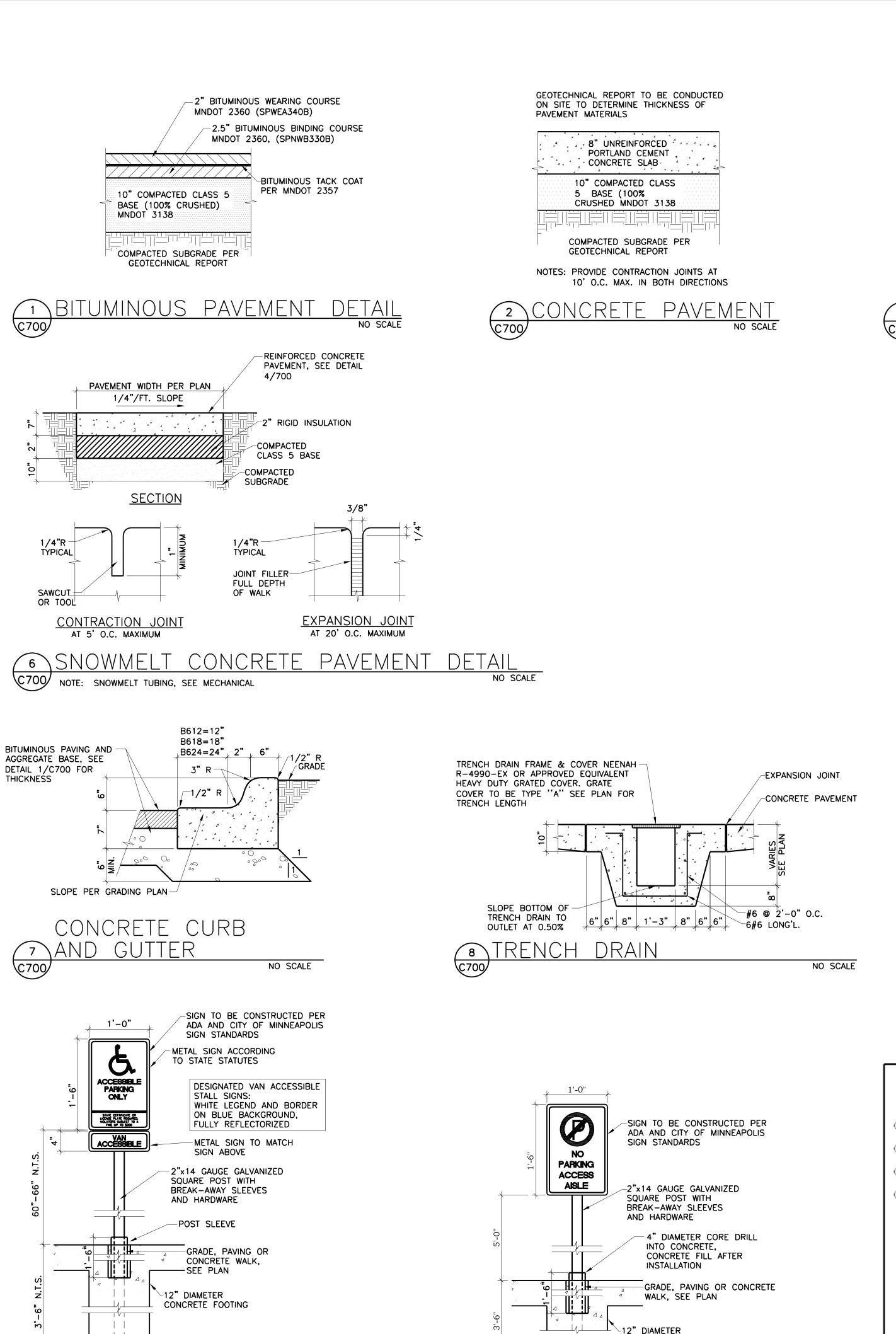
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SITE LAYOUT AND PAVING

SHEET NUMBER

C600

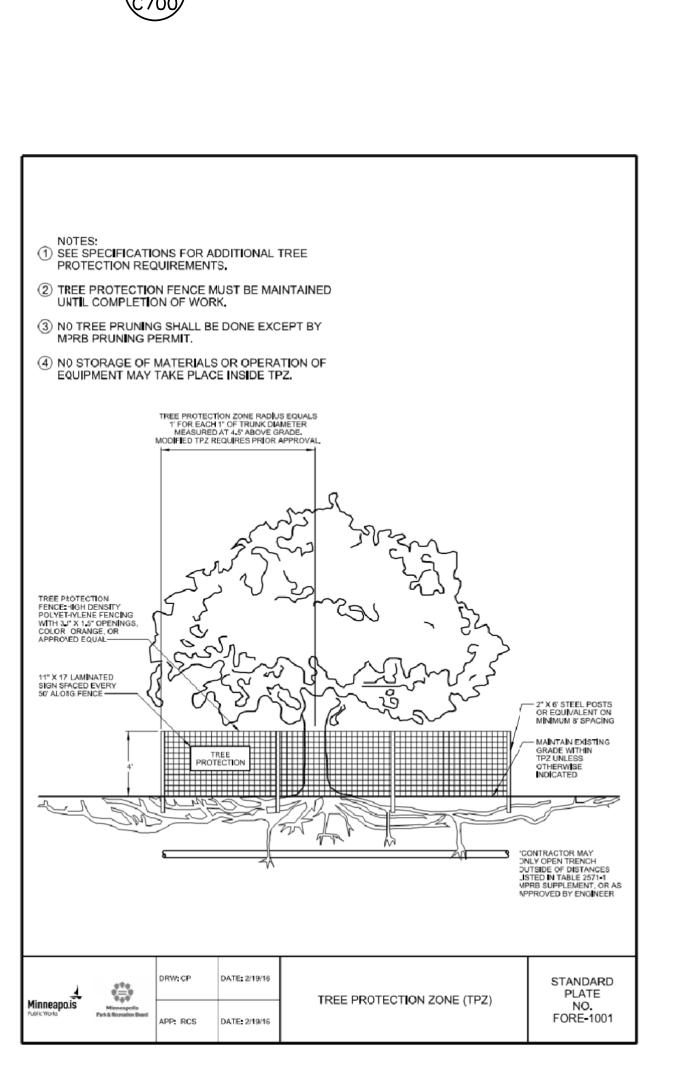


ACCESSIBLE PARKING

NO SCALE

(13) STALL SIGN

CONCRETE FOOTING



REINFORCED PORTLAND CEMENT WALK WIDTH PER PLAN 1/4"/FT. SLOPE

TYPICAL

CONTRACTION JOINT

AT 5' O.C. MAXIMUM

U.N.O.

-COMPACTED

-COMPACTED

SUBGRADE

TYPICAL

OF WALK

JOINT FILLER

FULL DEPTH

CLASS 5 BASE

TOP OF CASTING TO BE FLUSH WITH

ABOVE FINISH GRADE IN LANDSCAPED

-ATTACH TRACER WIRE

FOR ALL WIRES

CASTING FRAME, TYPICAL

OUTSIDE OF RINGS

-2" ADJUSTING RINGS AS REQ'D.

(2-MIN, 4-MAX.) FULL BED OF MORTAR BETWEEN RINGS-MORTAR

MANHOLE FRAME AND COVER, SEE SCHEDULE, WITH MACHINED BEARING SURFACES AND 2 CONCEALED PICK HOLES OR APPROVED

EQUIVALENT. SANITARY MANHOLE CASTING

PRECAST CONCRETE MANHOLE

MANHOLES MUST HAVE INTERNAL

OR EXTERNAL SEALS EXTENDING

-ALL BUTTED JOINTS BETWEEN SECTIONS SHALL HAVE "O"

-INSTALL TRACER WIRE ON ALL PIPES AND SECURE TO TOP OF PIPE, TYPICAL

-STEPS-NEENAH R-1980E

ALUMINUM @ 16"O.C.

RING RUBBER GASKETS

PER ASTM C478. SANITARY

FROM CASTING TO CONE

(DOWNSTREAM)

SEE PLAN FOR PIPE

-SLOPE 2" PER

B" PRECAST OR

C.I.P. BASE

1. MANHOLES 8' OR DEEPER - THE PRECAST SECTION IMMEDIATELY BELOW THE

4. PROVIDE SNAP-IN BOOT AT ALL SANITARY SEWER CONNECTIONS TO MANHOLES.

9 STANDARD SEWER MANHOLE

5. ALL STORM SEWER PIPING RUNS SHALL USE RESILIENT RUBBER JOINTS AT PIPE

2. PROVIDE STEPS IN MANHOLES OVER 4-0" IN DEPTH.
3. REINFORCING SHALL BE A MINIMUM OF A SINGLE LINE WIRE FABRIC HAVING AN AREA

CONNECTIONS TO CATCH BASINS FOR WATERTIGHT CONNECTION PER PLUMBING CODE.

CONCRETE SECTION SHALL BE 1'-0" HIGH.

OF NOT LESS THAN 0.12 SQ. IN. PER FOOT OF HEIGHT.

ASSEMBLIES SHALL CONFORM TO CITY OF

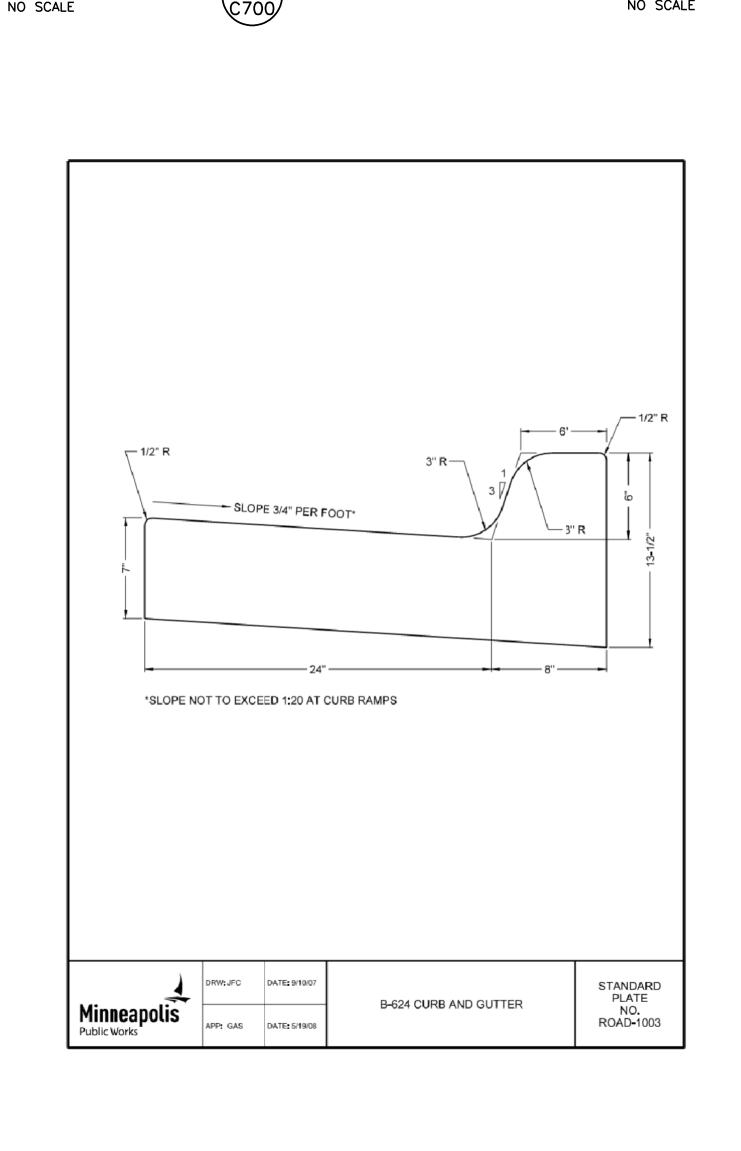
CHAMPLIN STANDARD DETAIL

FINISH GRADE AT PAVED AREAS: 1"

3/8"

EXPANSION JOINT

AT 20' O.C. MAXIMUM



SEE MECHANICAL FOR ATTACHING

1/2" WIRE SPACING

∠8" PAVEMENT 6 X 6 W1.4 X W1.4

NOTE: THE PIPE IS BEDDED IN COMPACTED GRANULAR

BEDDING MATERIAL BELOW THE PIPE IS A MINIMUM OF 3"

SIZES. THE REMAINING EMBANKMENT MATERIAL IS LIGHTLY

MATERIAL UP TO A HEIGHT EQUAL TO ONE SIXTH THE

OUTSIDE DIAMETER OF THE PIPE. THE DEPTH OF THE

FOR 27" DIAMETER AND SMALLER PIPE, 6" FOR 66" DIAMETER AND LARGER PIPE, AND 4" FOR INTERMEDIATE

1.25 Bc MINIMUM

GRANULAR FOUNDATION—CLASS C

(11) ADA PARKING SYMBOL

PAVEMENT DETAIL

COMPACTED.

1/6 B

REINFORCED CONCRETE

OR 3" MAX -

SNOWMELT SYSTEM TO REINFORCEMENT

PAVEMENT JOINT

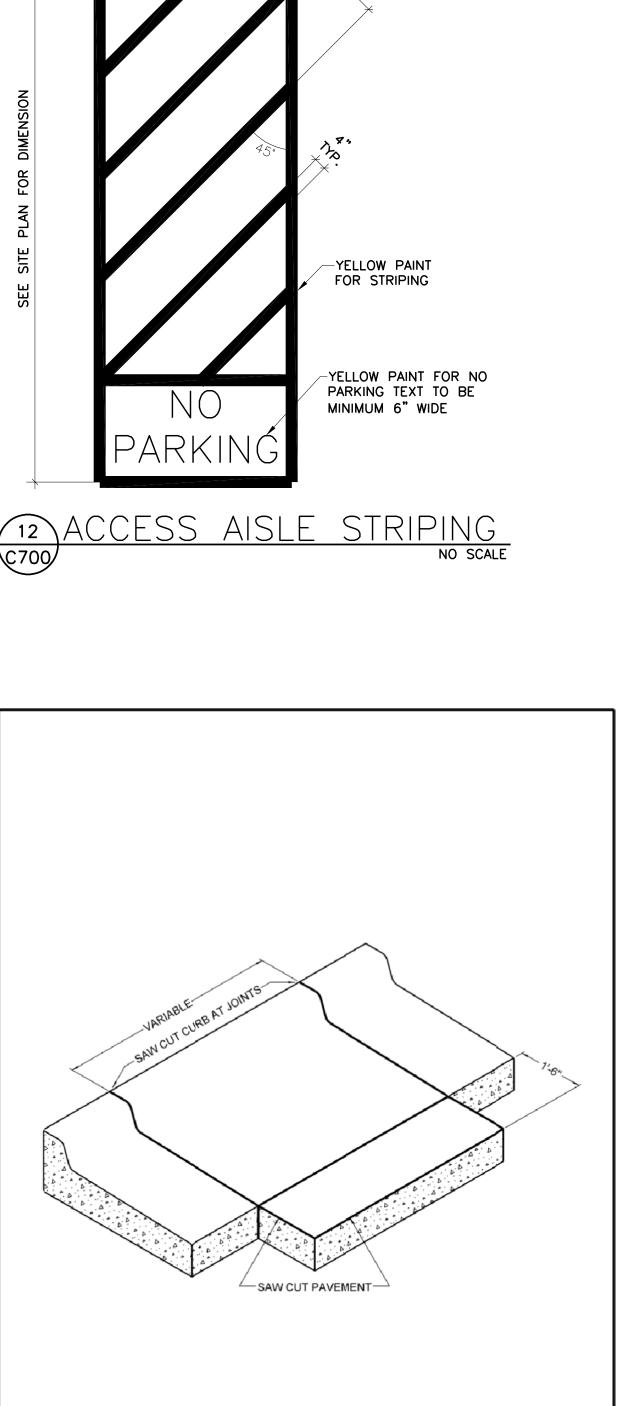
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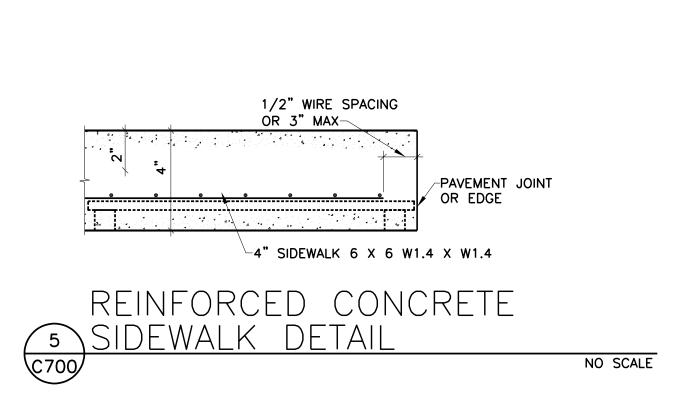
OR EDGE

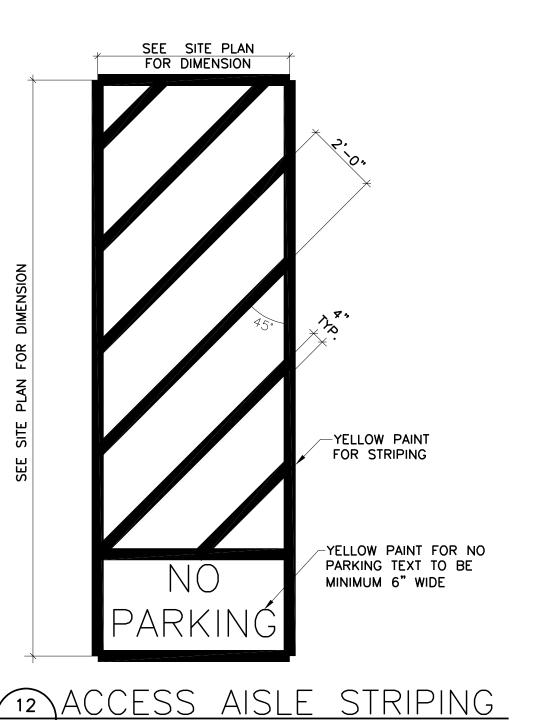
-COMPACTED

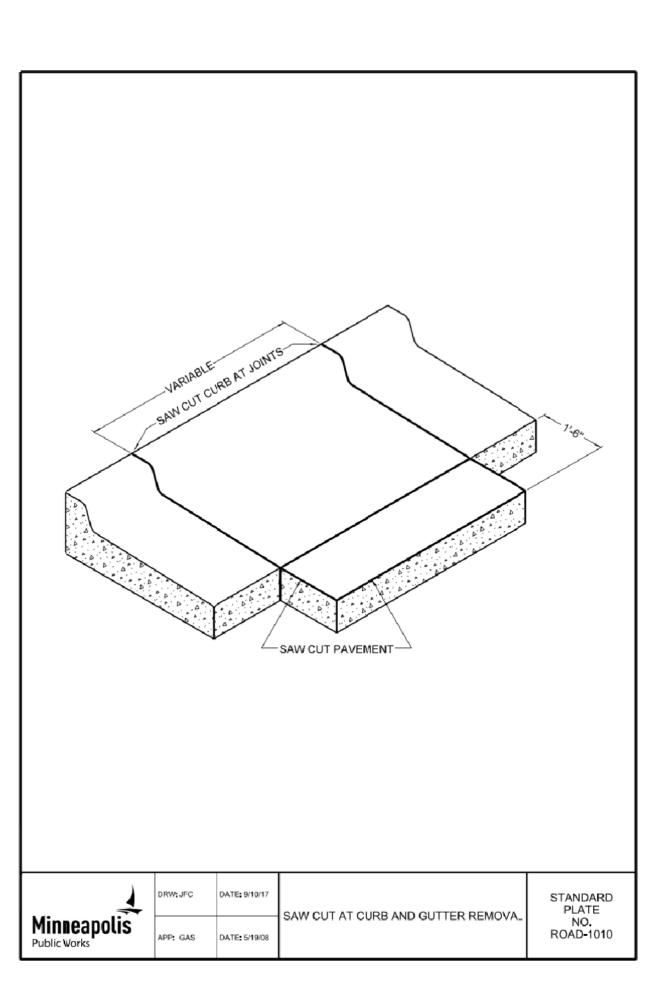
-BLUE SYMBOL

GRANULAR











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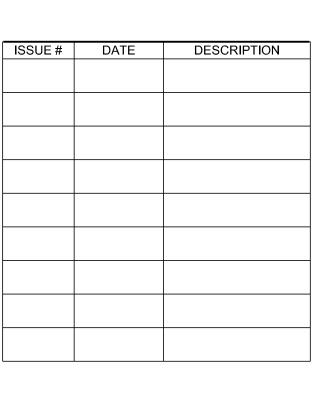
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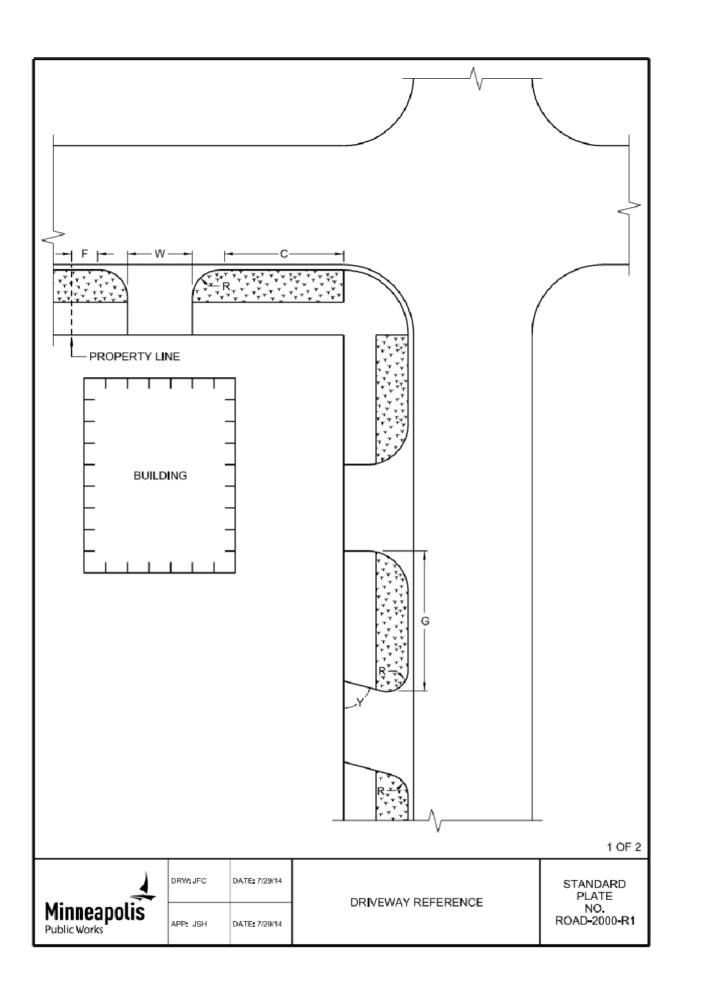
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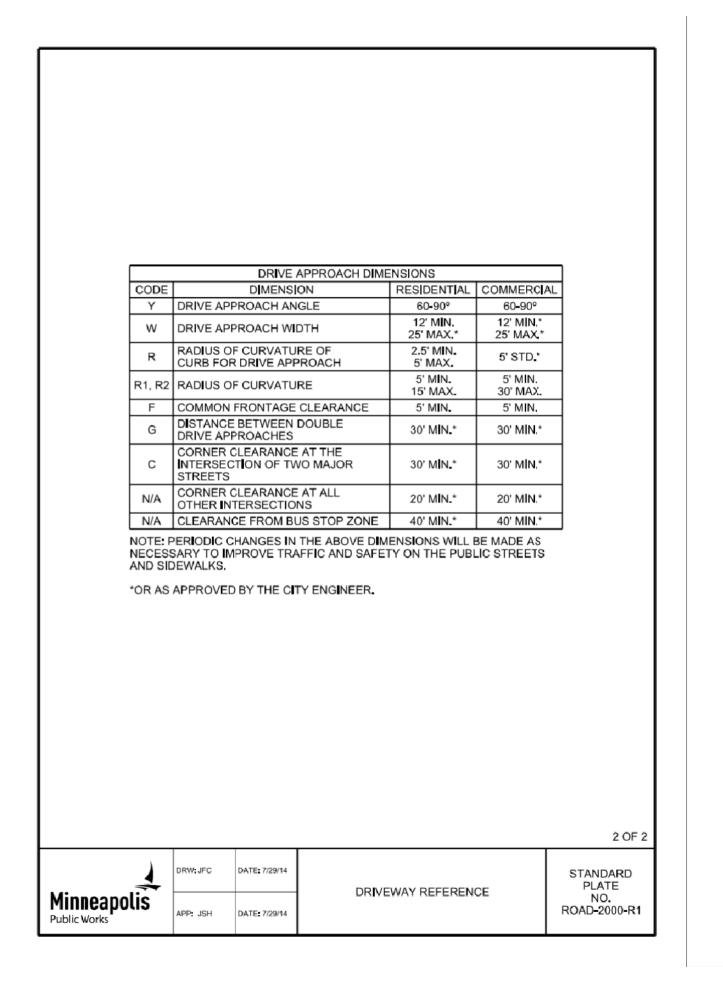
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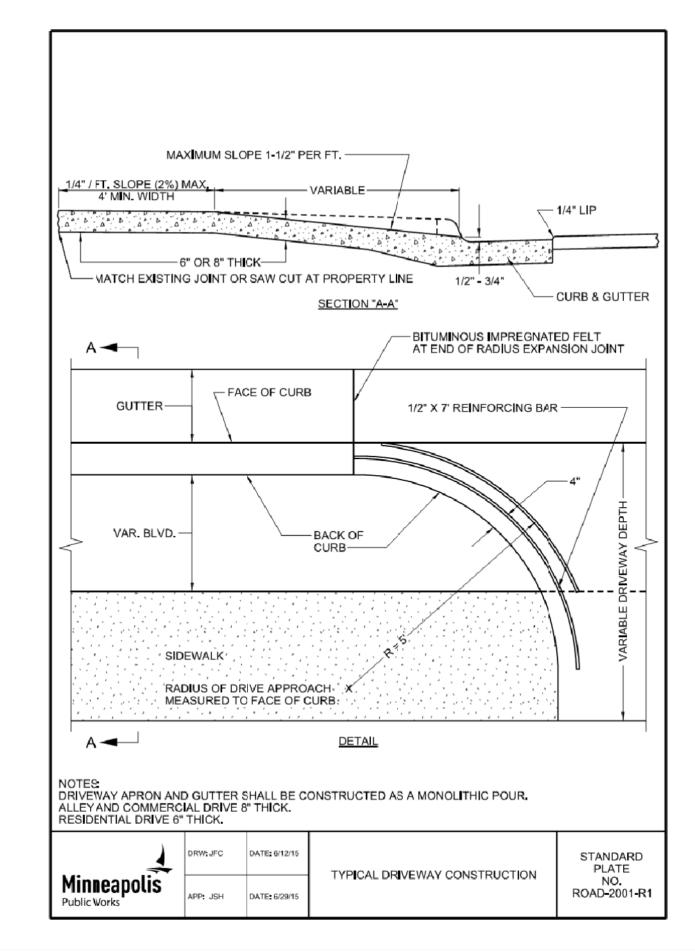
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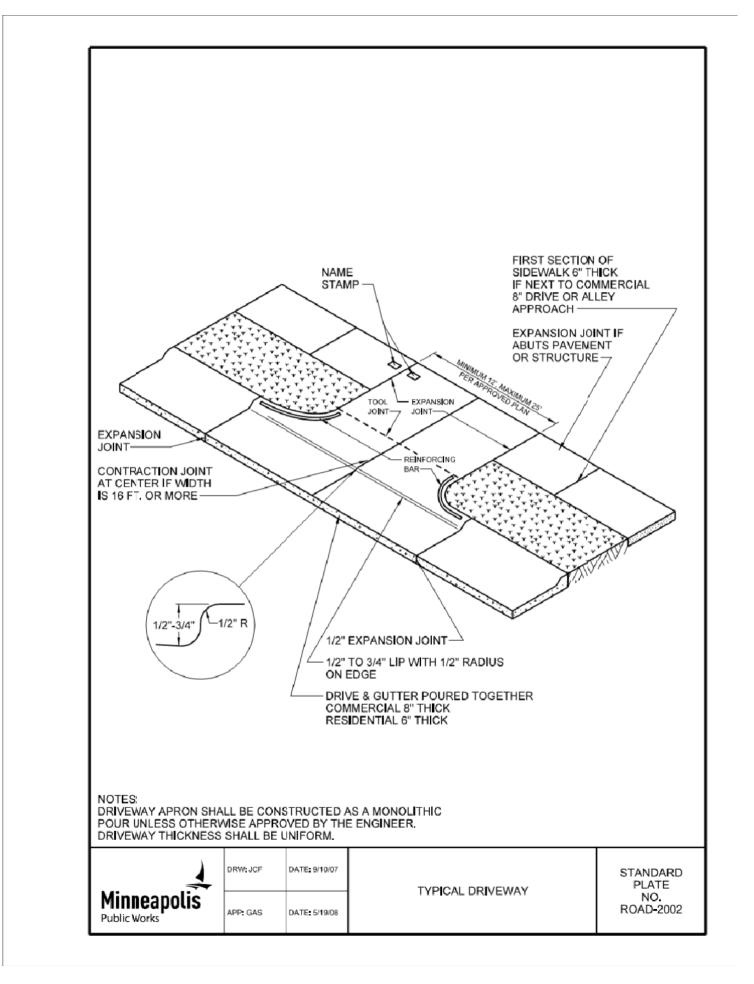
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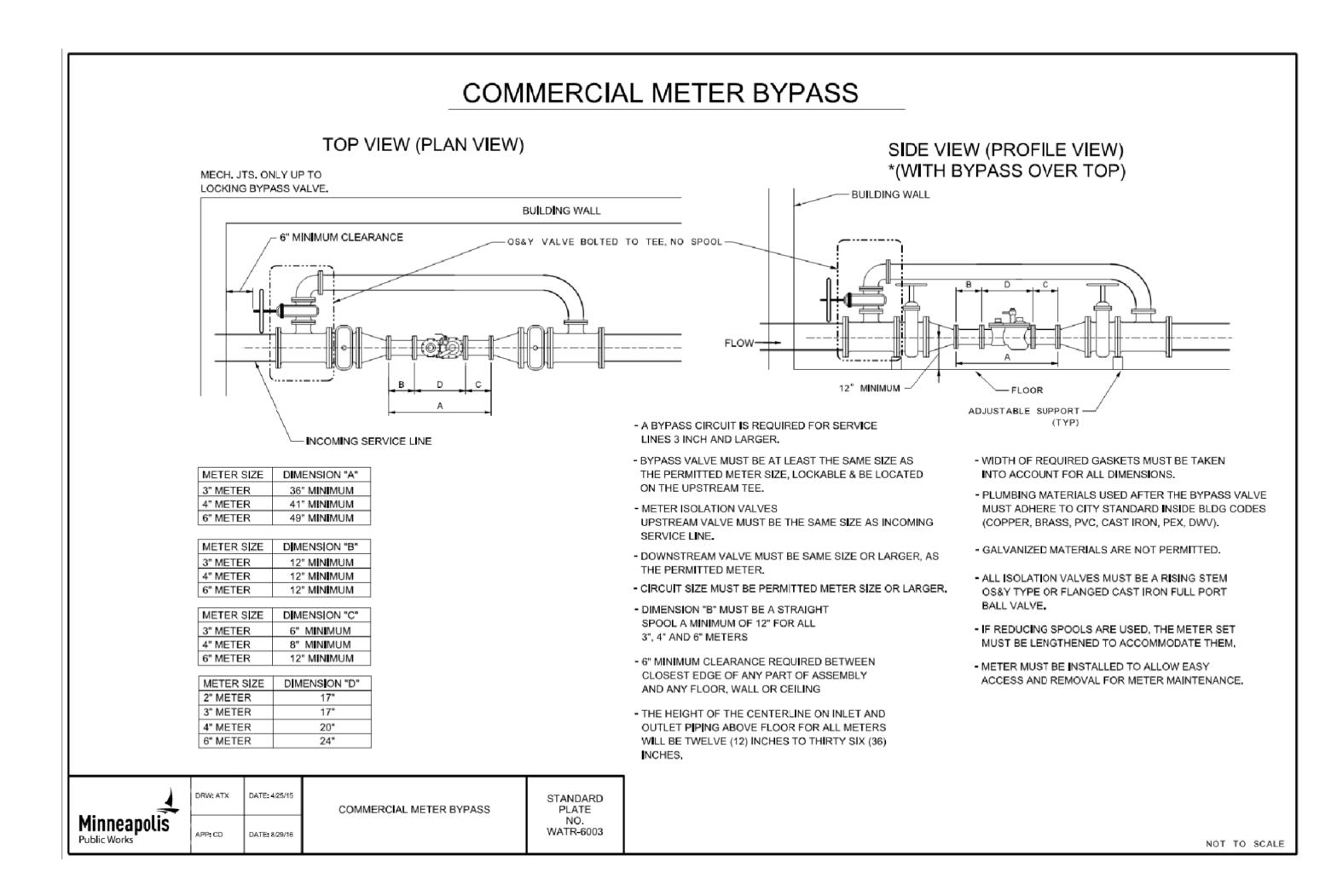
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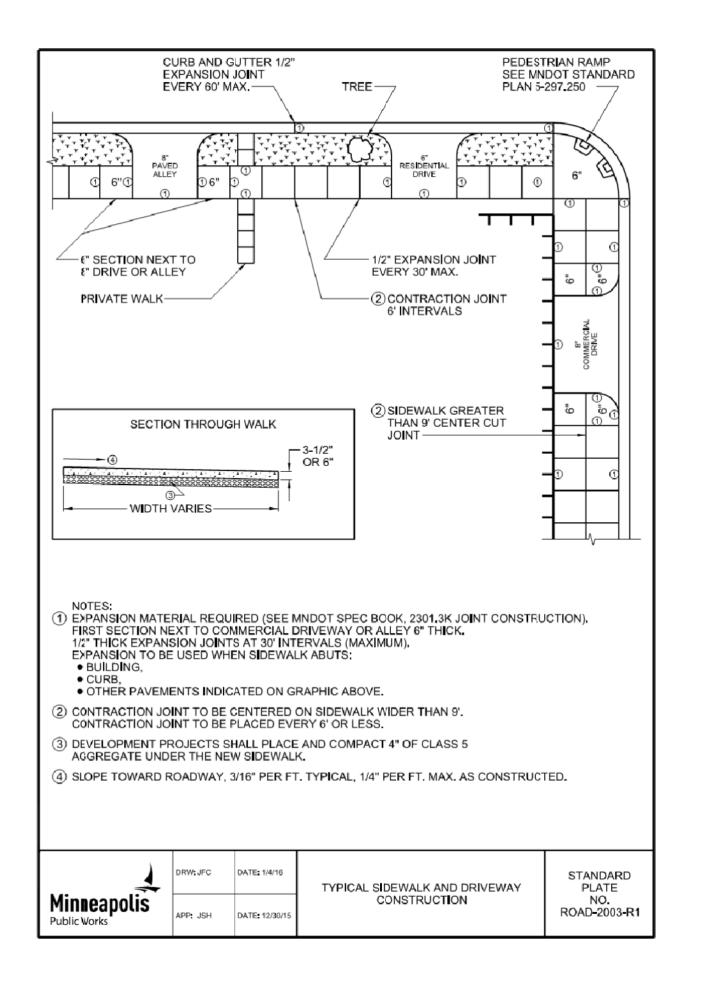












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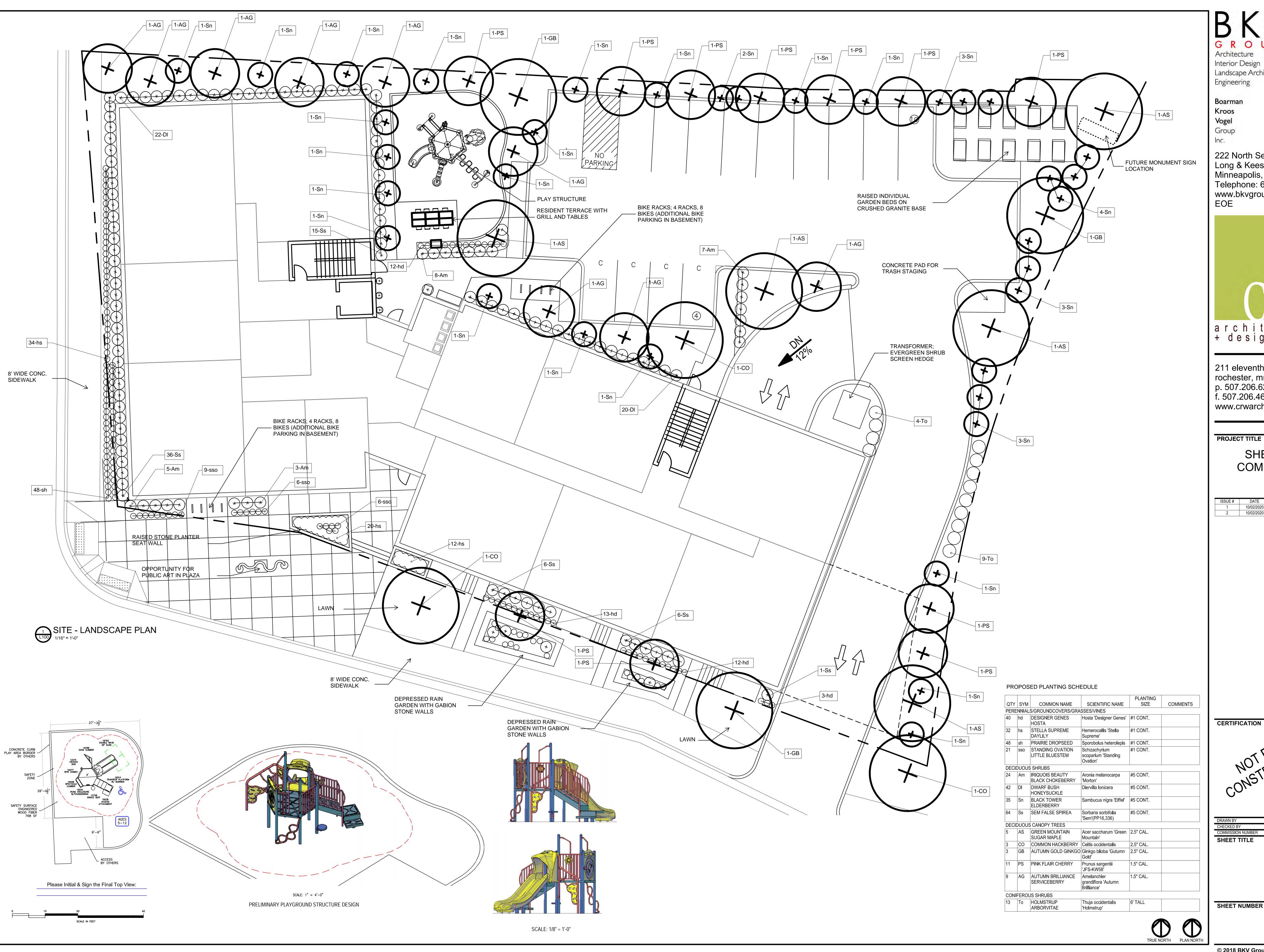
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SHEET NUMBER

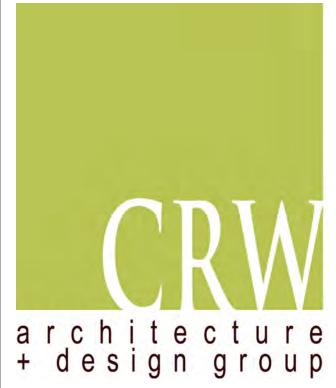
C701

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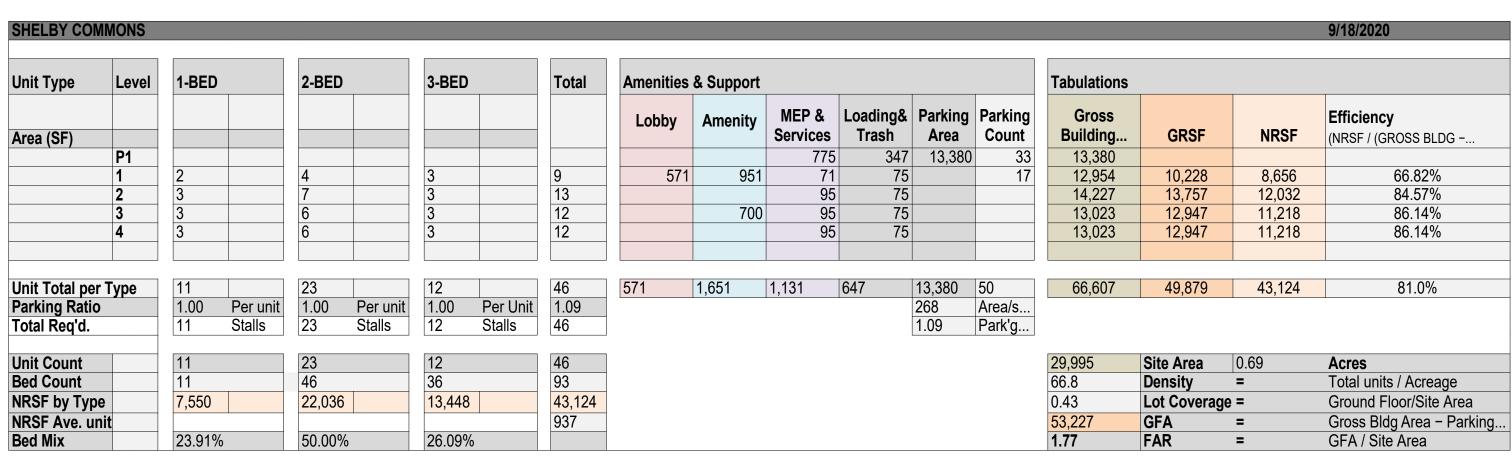
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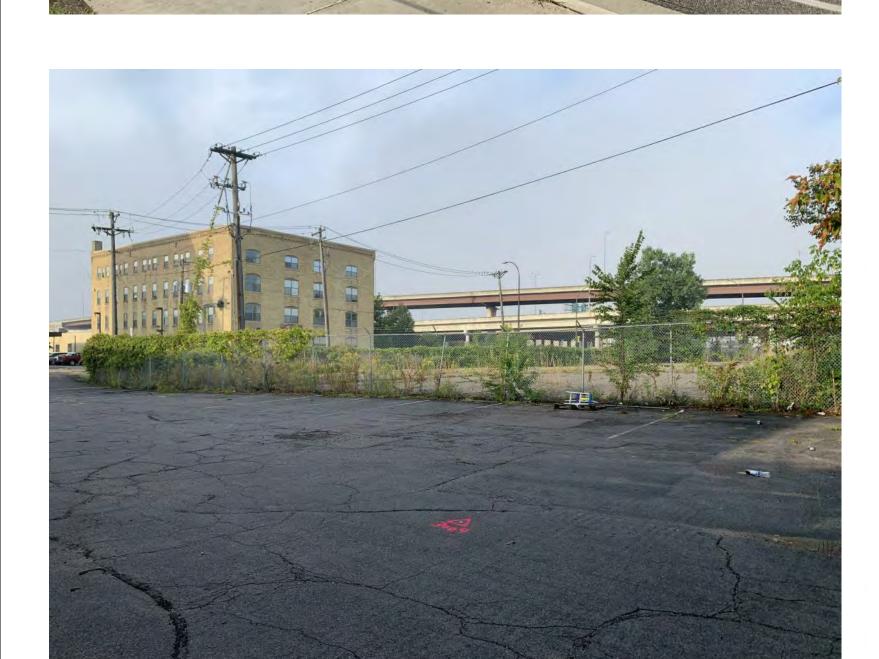
ISSUE#	DATE	DESCRIPTION
1	10/02/2020	LAND USE APPLICATION
2	10/02/2020	PDR SUBMITTAL

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Parcel ID	Parcel area	Zoning Distric Standards	Allowable	Proposed
220 292 433 0053	29995	Unerlying FAR	2.7	
188 1/2 Glenwood Ave		Density bonus for at least 20% units are affordable	0.54	
		Total FAR	3.24	1.77
Neighborhood: North Loop		Building Height	4 Stories / 56 ft	4 Stories
Ward: 5 - Jeremiah Bey Ellison		Lot area minimum per dwelling unit	N/A	
Zoning District: C3A - Community Activity Center		Total Unit Density	n/a	46 Units (67/acre
and Downtown Parking Overlay District		Residential Parking Stalls	1.7 per Unit	17
				33
		Total Parking Stalls	78	50
		Land Use Application Requests		
		1. Subdivision Approval		
Total Lot Area	29995			







1 SITE PLAN
A1-0 1" = 30'-0"

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STAMPING AREA

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RTIFICATION

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COMMISSION NUMBER 2385.03
SHEET TITLE

SITE PLAN

SHEET NUMBER

Δ1_0

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SITE PHOTOS
A1-0 1 1/2" = 1'-0"



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1 10/02/2020 LAND USE APPLICATION

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2385.03
SHEET TITLE

GARAGE PLAN

SHEET NUMBER

A2-0.0

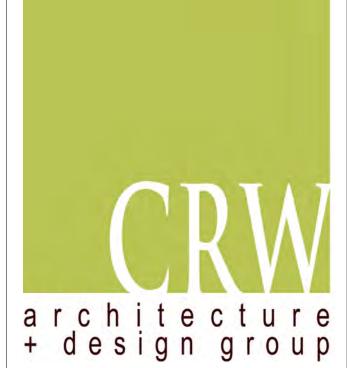


1 LEVEL 1 A2-0.1 1/8" = 1'-0" B V V P Architecture Interior Design Landscape Architecture

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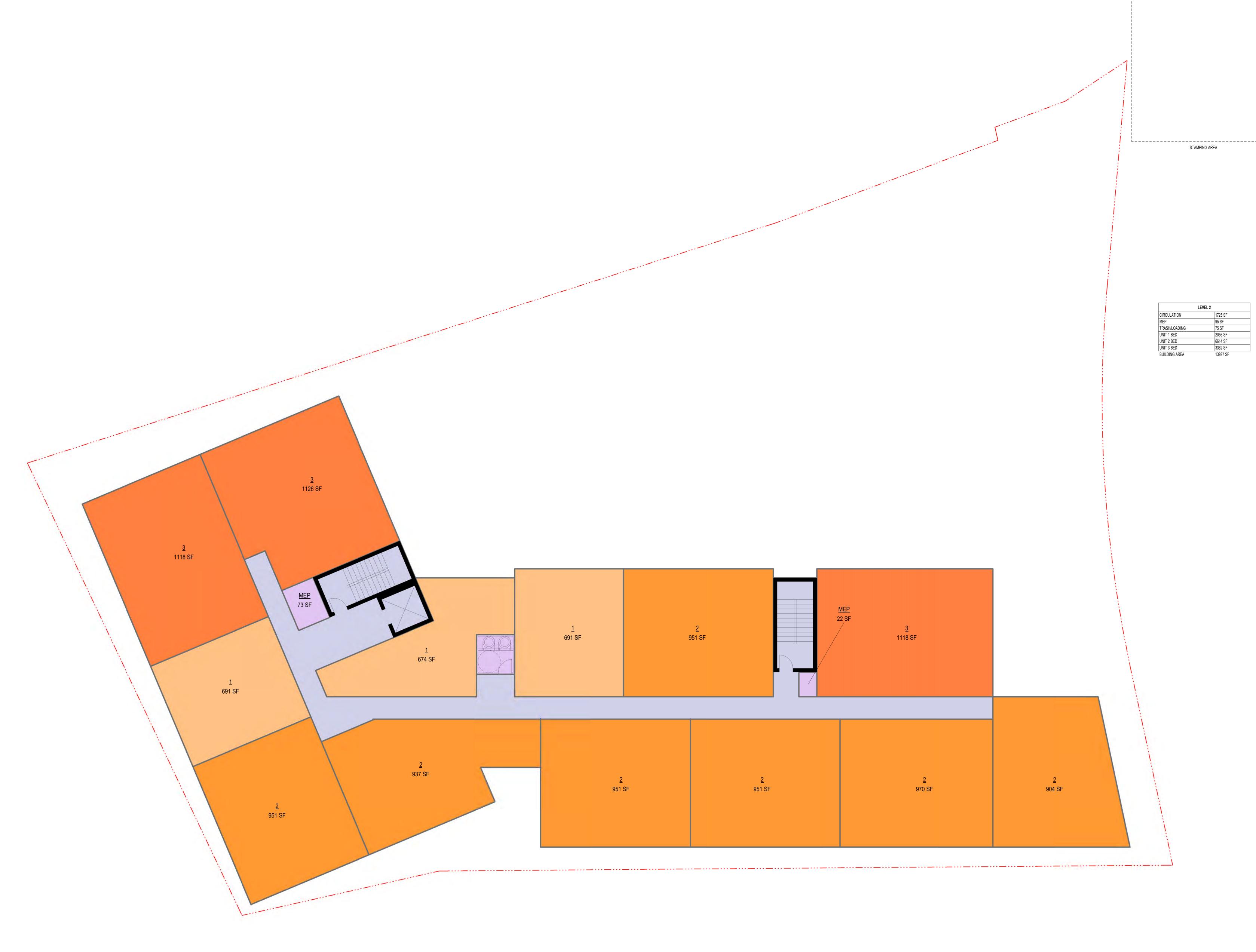
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LEVEL 1

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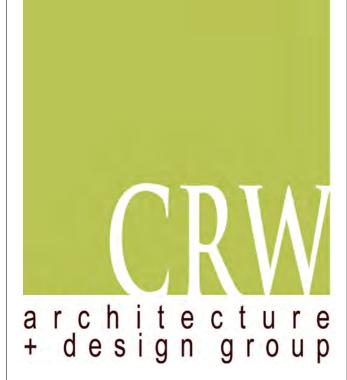


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CONSTRUCTION

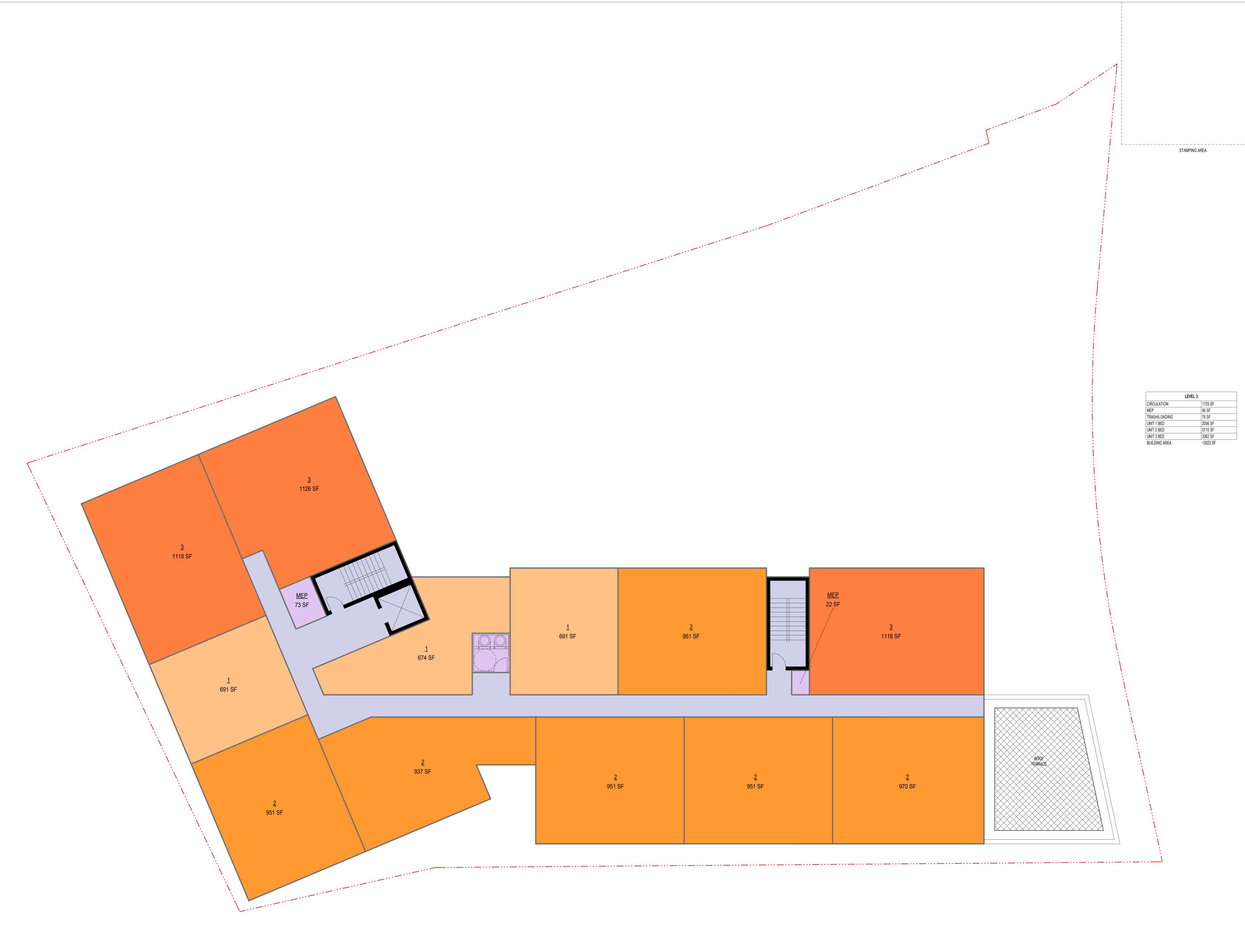
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SHEET TITLE

LEVEL 2

OUEET NUMBER

A2-0.2

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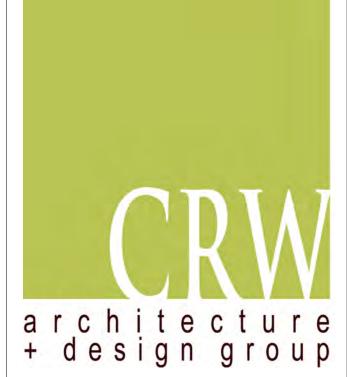


1 LEVEL 3 A2-0.3 1/8" = 1'-0" B V V P Architecture Interior Design Landscape Architecture

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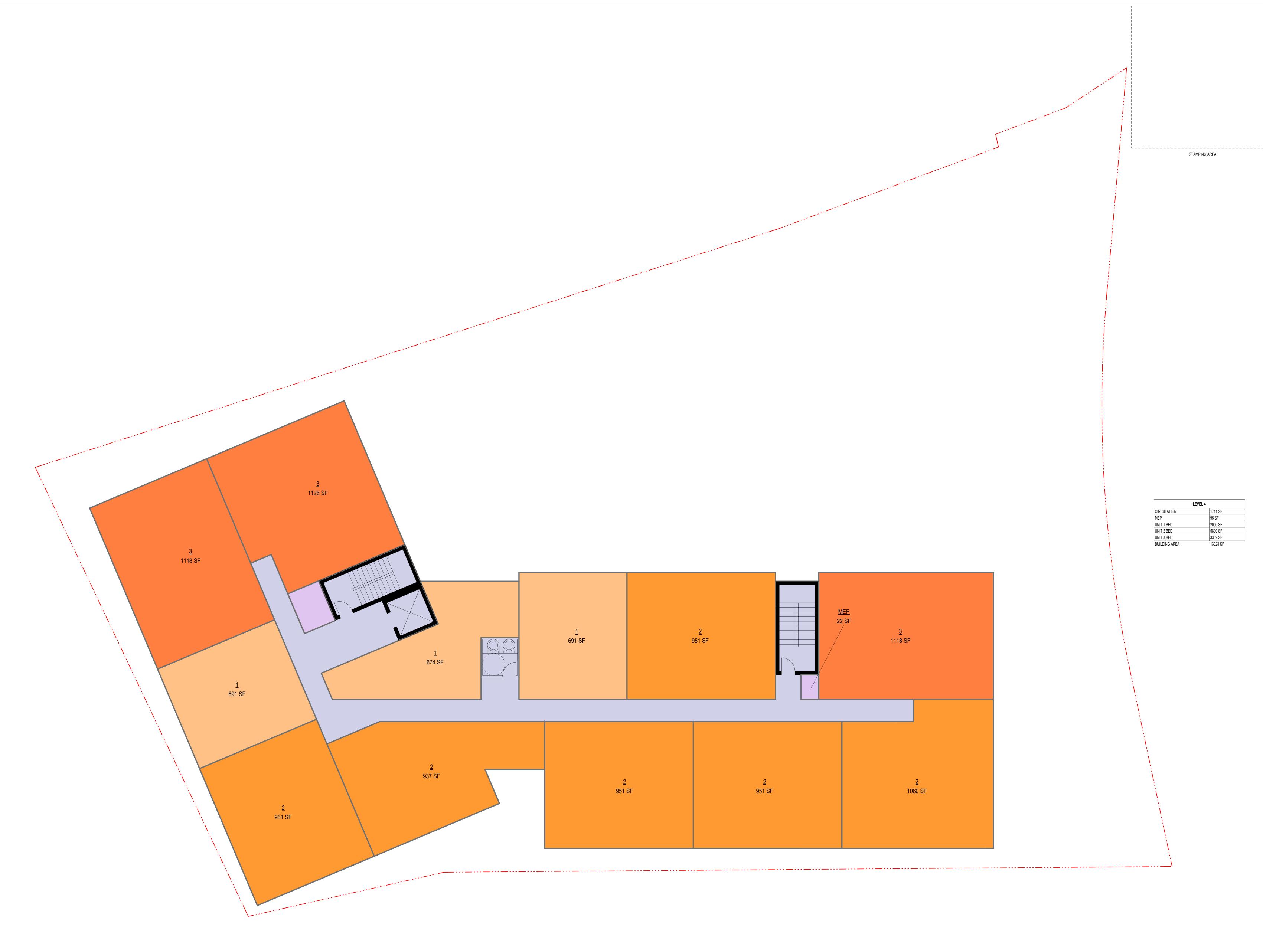
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LEVEL 3

SHEET NUMBER

A2-0.3



1 LEVEL 4
A2-0.4 1/8" = 1'-0"

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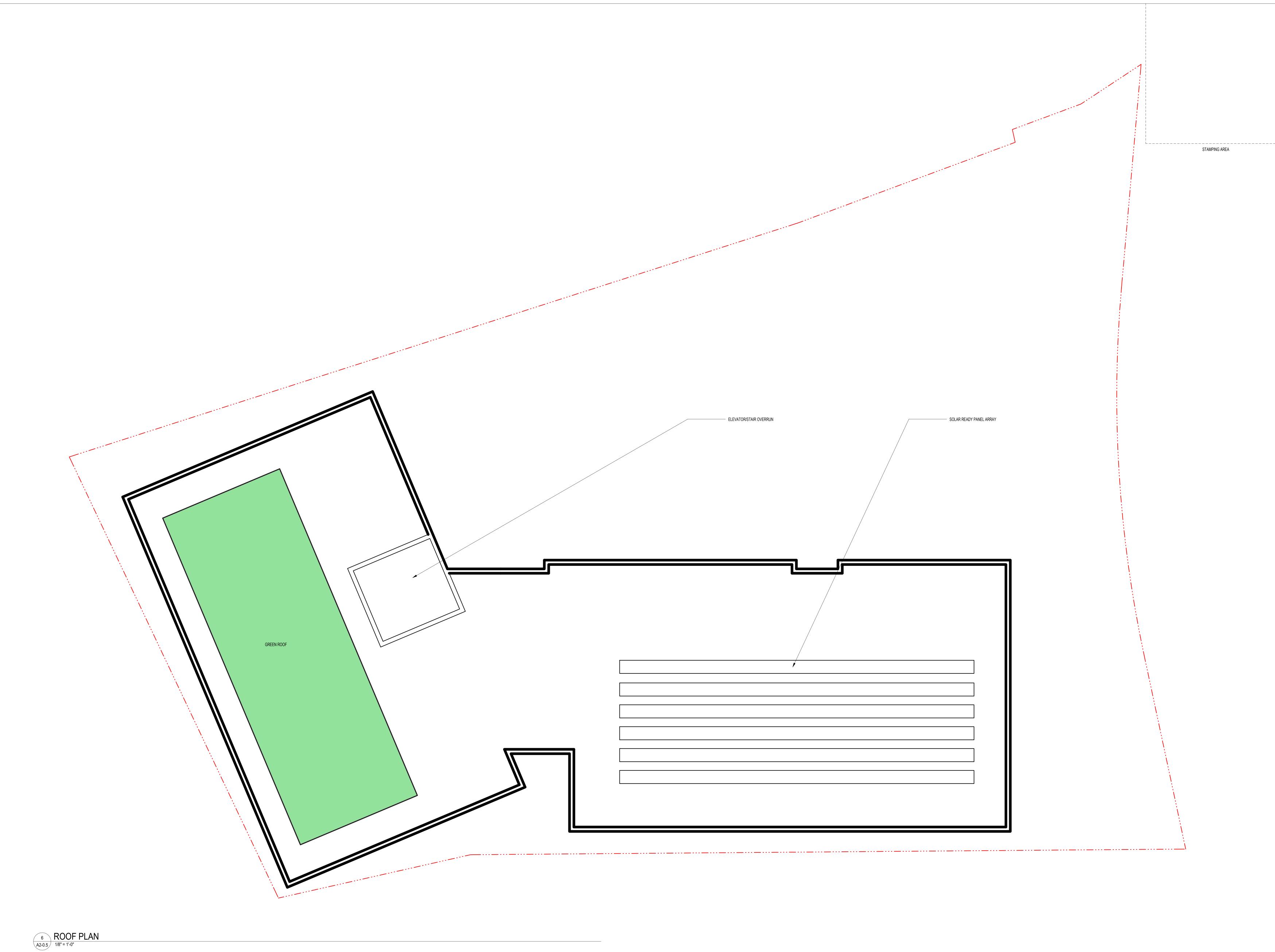
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SHEET TITLE

LEVEL 4

OUEET NUMBER

Δ2_0 /

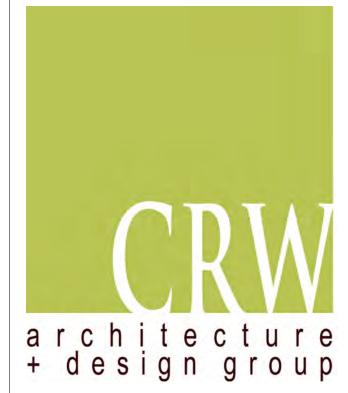


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1 10/02/2020 LAND USE APPLICATION

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SHEET TITLE

ROOF PLAN

SHEET NUMBER

A2-0.5

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2 10/02/2020 PDR SUBMITTAL	1	10/02/2020	LAND USE APPLICATION	
10/02/2020	2	10/02/2020	PDR SUBMITTAL	

CERTIFICATION



SHEET TITLE	
COMMISSION NUMBER	2385.03
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DRAWN BY	Author

ELEVATIONS

SHEET NUMBER

A3-0

			STAMPING AREA
		(FSDG-1)	South Elevation Overall Area 8612 Glass 33% FSDG - Lap 17% FSDG - Panel 0% Brick 3% Metal 47% Total 100% West Elevation Overall Area 5352 Glass 30% FSDG - Lap 1% FSDG - Panel 15% Brick 0% Metal 54% Total 100%
SOUTH ELEVATION 3/32" = 1'-0" (MTL-1) EAST ELEVATION 3/32" = 1'-0"	(FSDG-2) (FB-1)	(CMU-1) (MTL-1) (FSDG-1) WEST ELEVATION 3/32" = 1'-0"	(FSDG-2)
East Elevation	NORTH ELEVATION 3/32" = 1'-0" (FB-1)	(CMU-1)	

STAMPING AREA









PRODUCT: JAMES HARDIE

HARDIEPANEL OR SIMILAR

(FLUSH JOINTS)

PRODUCT: JAMES HARDIE

HARDIEPLANK OR SIMILAR

(MINIMUM LAP EXPOSURE)

G R O U P Architecture Interior Design Landscape Architecture Engineering

Boarman Kroos Vogel Group

222 North Second Street Long & Kees Bldg, Suite 101 Minneapolis, MN 55401 Telephone: 612.339.3752 www.bkvgroup.com EOE

architecture + design group

211 eleventh avenue nw rochester, mn 55901 p. 507.206.6201 f. 507.206.4621 www.crwarchitecture.com

PROJECT TITLE

SHELBY COMMONS

 ISSUE #
 DATE
 DESCRIPTION

 1
 10/02/2020
 LAND USE APPLICATION

DRAWN BY
CHECKED BY
COMMISSION NUMBER Author Checker 2385.03 SHEET TITLE

RENDERINGS

SHEET NUMBER

DECEMBER 21, 3PM

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G R O U P Architecture

Interior Design Landscape Architecture Engineering

Boarman Kroos Vogel Group

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PROJECT TITLE

SHELBY COMMONS

ISSUE # DATE DESCRIPTION
1 10/02/2020 LAND USE APPLICATION

CERTIFICATION

DRAWN BY
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COMMISSION NUMBER

SHEET TITLE

SHADOW STUDY

DECEMBER 21, 10AM

CalcType	Units	Avg	Max	Min
Illuminance	Fc	1.24	16.2	0.0
Illuminance	Fc	1.89	11.3	0.2
Illuminance	Fc	2.77	14.9	0.5
Illuminance	Fc	0.82	1.5	0.2
Illuminance	Fc	4.53	16.2	0.5
Illuminance	Fc	3.74	10.2	0.9
Illuminance	Fc	0.85	3.3	0.1
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Architecture Interior Design Landscape Architecture Engineering

Boarman Kroos Vogel Group

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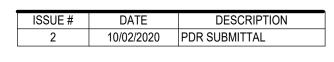
222 North Second Street Long & Kees Bldg, Suite 101 Minneapolis, MN 55401 Telephone: 612.339.3752 www.bkvgroup.com EOE



211 eleventh avenue nw rochester, mn 55901 p. 507.206.6201 f. 507.206.4621 www.crwarchitecture.com

PROJECT TITLE

SHELBY COMMONS



CHECKED BY SHEET TITLE

ELECTRICAL SITE PHOTOMETRIC PLAN

SHEET NUMBER





October 1, 2020

Minneapolis City Council, Ward 5 350 South 5th Street, Room 307 Minneapolis, MN 55415 Attn: Jeremiah Ellison

Dear Mr. Jeremiah Ellison:

Woda Cooper Development, Inc. is submitting a Land Use Application for a proposed family development located at 188 ½ Glenwood Avenue, Minneapolis, MN 55405 in the North Loop neighborhood. The Land Use Application is for a 4-story apartment building, known as Shelby Commons, an affordable housing development. Shelby Commons will be developed by Woda Cooper Development, Inc. with Project for Pride in Living, Inc. (PPL) as a co-developer. This will provide for the development of 46 low income housing units, all affordable. These units will consist of 11 one-bedroom units, 23 two-bedroom units, and 12 three-bedroom units.

PPL will be providing supportive services, including on-site case management, for 12 units. 5 of the supportive housing units will be one-bedroom units for persons with disability. PPL will use its Housing Supports contract to help subsidize the rents of these units. The remaining 7 supportive units will be set aside for high priority homeless with 4 of these units being one-bedroom units, and 3 being two-bedroom units. Shelby Commons has been awarded 12 project-based section 8 vouchers from the Minneapolis Public Housing Authority and is currently awaiting a response from Minnesota Housing Finance Agency and the Minneapolis Community Planning and Economic Development on its applications for 9% low income housing tax credits. Shelby Commons will use 7 of the 12 vouchers to subsidize the rents of the supportive units set aside for high priority homeless. The remaining 5 vouchers will be used on general occupancy units, 1 on a two-bedroom unit, and 4 on three-bedroom units.

Units will be available to residents at varying income levels, with some units set aside at 30%, 50%, 60%, and 80% of the HUD Multifamily Tax Subsidy Projects (MTSP) income limits. In addition to this, all units will be affordable to households at the 60% HUD MTSP income limits. This presents a great opportunity for low income households at or below the 80% income limit to find quality living at a rate which would be very affordable to such households.

Other amenity spaces Shelby Commons has to offer include a community room, community laundry, and a supportive services office. There will also be an outdoor playground for children along with community garden plots. Along Glenwood Avenue, Shelby Commons will feature a unique urban rain garden, and there will be an opportunity for public art, both on the sidewalk/entry plaza and on select portions of the building exterior facade. Because Shelby Commons is a large family development, we have provided parking at a ratio just over 1 to 1, with 47 total parking spaces, 32 of which are in an underground garage, and 15 of which are in a surface lot located at the rear of the building.

As part of the Land Use Application for Shelby Commons, Woda Cooper Development is also submitting a Conditional Use Permit and a Subdivision Application. The Conditional Use Permit is required for the 15-space surface lot. This property is located in the Downtown Parking Overlay District, so any surface parking as an accessory use requires a conditional use. The Subdivision Application will be used to replat the two tax parcels at 188 ½ Glenwood Ave. These parcels have tax identification numbers of 22-029-24-33-0053, and 22-029-24-33-0058. Woda Cooper Development is seeking to replat this as the land for the Shelby Commons development, and the rest as an outlot. The city of Minneapolis is hoping to construct a Border Avenue Extension project that will use this outlot to extend the existing Border Avenue to Glenwood Avenue as part of the reconstruction surrounding the Green Line Extension project. In speaking with the city, Woda Cooper Development was advised to replat as such to accommodate this future road extension.

Thank you for your consideration of our proposed development. Should you have any questions or comments, please do not hesitate to contact me at 614-396-0024 or pzee@wodagroup.com. I can also be reached by mail at 500 S Front St., 10th Floor, Columbus, OH 43215, Attn: Parker Zee.

Sincerely,

Parker Zee

Parker See

Assistant Vice President of Development and Data Manager







October 1, 2020

North Loop Neighborhood Association P.O. Box 580672 Minneapolis, MN 55458 Attn: Tim Bildsoe

Dear Mr. Tim Bildsoe:

Woda Cooper Development, Inc. is submitting a Land Use Application for a proposed family development located at 188 ½ Glenwood Avenue, Minneapolis, MN 55405 in the North Loop neighborhood. The Land Use Application is for a 4-story apartment building, known as Shelby Commons, an affordable housing development. Shelby Commons will be developed by Woda Cooper Development, Inc. with Project for Pride in Living, Inc. (PPL) as a co-developer. This will provide for the development of 46 low income housing units, all affordable. These units will consist of 11 one-bedroom units, 23 two-bedroom units, and 12 three-bedroom units.

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Sincerely,

Parker Zee

Parker Des

Assistant Vice President of Development and Data Manager







October 1, 2020

Heritage Park Neighborhood Association 1000 Olson Memorial Highway Minneapolis, MN 55411 Attn: Victoria Bayerl

Dear Ms. Victoria Bayerl:

Woda Cooper Development, Inc. is submitting a Land Use Application for a proposed family development located at 188 ½ Glenwood Avenue, Minneapolis, MN 55405 in the North Loop neighborhood. The Land Use Application is for a 4-story apartment building, known as Shelby Commons, an affordable housing development. Shelby Commons will be developed by Woda Cooper Development, Inc. with Project for Pride in Living, Inc. (PPL) as a co-developer. This will provide for the development of 46 low income housing units, all affordable. These units will consist of 11 one-bedroom units, 23 two-bedroom units, and 12 three-bedroom units.

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Sincerely,

Parker Zee

Parker her

Assistant Vice President of Development and Data Manager







October 1, 2020

Harrison Neighborhood Association 503 Irving Ave N, Suite 100 Minneapolis, MN 55405 Attn: Nichole Buehler

Dear Ms. Nichole Buehler, Ms. Angela Bonfiglio, and Mr. Denetrick Powers:

Woda Cooper Development, Inc. is submitting a Land Use Application for a proposed family development located at 188 ½ Glenwood Avenue, Minneapolis, MN 55405 in the North Loop neighborhood. The Land Use Application is for a 4-story apartment building, known as Shelby Commons, an affordable housing development. Shelby Commons will be developed by Woda Cooper Development, Inc. with Project for Pride in Living, Inc. (PPL) as a co-developer. This will provide for the development of 46 low income housing units, all affordable. These units will consist of 11 one-bedroom units, 23 two-bedroom units, and 12 three-bedroom units.

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Sincerely,
Parker July

Parker Zee

Assistant Vice President of Development and Data Manager

