
LAND USE APPLICATION SUMMARY

Property Location: 188 ½ Glenwood Avenue and 151 Holden Street North
Project Name: Shelby Commons
Prepared By: Hilary Dvorak, Principal City Planner, (612) 673-2639
Applicant: Woda Cooper Development, Inc.
Project Contact: Mike Krych with BKV Group
Request: To construct a new four-story, 46-unit residential development.

Dwelling Units	46 dwelling units
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Required Applications:

Conditional Use Permit	To allow a 17-space surface parking lot located in the DP Downtown Parking Overlay District.
Site Plan Review	For a new four-story, 46-unit residential building.
Preliminary and Final Plat	To create a new lot and a new outlot.

SITE DATA

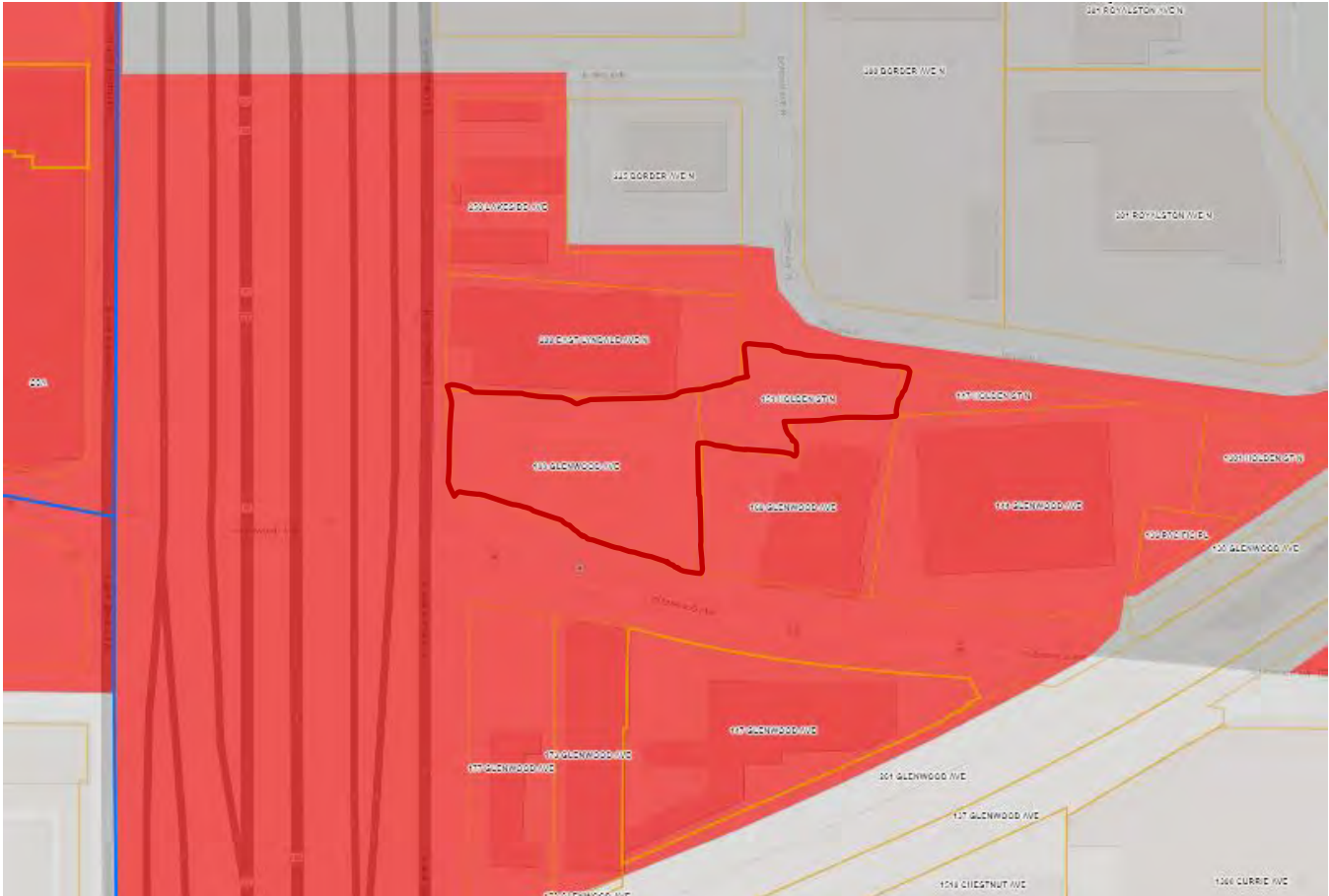
Existing Zoning	C3A Community Activity Center District DP Downtown Parking Overlay District
Lot Area	30,226 square feet / .69 acres
Ward(s)	5
Neighborhood(s)	North Loop
Future Land Use	Community Mixed use
Goods and Services Corridor	Glenwood Avenue
Built Form	Transit 30

Date Application Deemed Complete	October 15, 2020	Date Extension Letter Sent	Not applicable
End of 60-Day Decision Period	December 14, 2020	End of 120-Day Decision Period	Not applicable

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The site is located on the northeast corner of Glenwood Avenue and East Lyndale Avenue North. The site is vacant. It has historically been used for open storage.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The site is surrounded by a mixture of commercial and residential properties. The site is located in the North Loop neighborhood.



PROJECT DESCRIPTION. The applicant is proposing to construct a new four-story, 46-unit residential development on the site. This will be an affordable housing project. There will be a 17-space surface parking lot located towards the back of the building and 33 parking spaces will be provided underneath the building. Access to the site will be from Glenwood Avenue. There will be an outdoor play area for children located towards the back of the building in addition to a community garden space. The exterior building materials include corrugated metal, fiber cement panel, brick and glass.

The site is located in the DP Downtown Parking Overlay District. Accessory surface parking lots require a conditional use permit in the overlay district. The applicant is proposing to have a 17-space surface parking lot on the site.

Site plan review is required as the proposed development contains more than 10 dwelling units.

The current site also includes the property located at 151 Holden Street North. In the future, the City of Minneapolis would like to extend Border Avenue North to Glenwood Avenue. In order to do this, the property boundaries of 188 ½ Glenwood Avenue and 155 Holden Street North would need to be modified. As part of this

application, the properties will be replatted to accommodate the future needs for the street. There will be one new lot and one new outlot created through the plat.

When Border Avenue North is extended to Glenwood Avenue, the hope is that the proposed driveway leading to Glenwood Avenue would be eliminated and a new curb cut would be constructed to Border Avenue North. CPED and the City's Public Works Department is in conversations with the applicant about this.

RELATED APPROVALS. Not applicable.

PUBLIC COMMENTS. No public comment letters have been submitted for this development. Any correspondence received prior to the public meeting will be forwarded on to the Planning Commission for consideration.

ANALYSIS

CONDITIONAL USE PERMIT

The Department of Community Planning and Economic Development has analyzed the application to allow a 17-space surface parking lot located in the DP Downtown Parking Overlay District based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The establishment of a surface parking lot will not be detrimental to or endanger the public health, safety, comfort or general welfare. The site has historically been used for open storage. The proposed surface parking lot will be located towards the back of the building. If and when Border Avenue North is extended to Glenwood Avenue, the parking lot will be buffered from it by a community garden and landscaping.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The establishment of a surface parking lot will not be injurious to the use and enjoyment of other property in the vicinity or impede the normal or orderly development and improvement of surrounding property. The proposed surface parking lot will be located towards the back of the building. The proposed surface parking lot will be screened by the building itself, a community garden and landscaping

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

The Public Works Department has reviewed the preliminary plans and will review the final plans for compliance with standards related to access and circulation, drainage, and sewer/water connections. The applicant will be required to continue to work closely with the Public Works Department, the Plan Review Section of CPED and the various utility companies during the duration of the development to ensure that all procedures are followed and that the development complies with all city and other applicable requirements. The applicant is aware that the final plans are expected to incorporate any applicable comments or modifications as required by the Public Works Department.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

The parking requirement for the development is one space per dwelling unit or 46 spaces. However, the site is located within a quarter-mile from a bus transit stop with midday service headways of 15 minutes or less. Given this, the development qualifies for a 50 percent parking reduction. The parking requirement for the development is 23 spaces. The applicant is proposing to provide 47 parking spaces within the building.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

The proposed use would be consistent with the applicable guidance and policies of *Minneapolis 2040 (2020)*:

Future Land Use	Guidance	Staff Comment
Community Mixed Use	Large-scale mixed use development is encouraged throughout these areas, with commercial uses fronting on major streets. Commercial retail spaces are typically smaller in order to generate pedestrian activity, and are often a destination for customers coming from outside of the market area. Active uses that are accessible to the general public such as office, food service, retail, or medical establishments are required at the street level; therefore single-use residential development is not permitted. Contiguous expansion of commercial zoning is allowed.	The applicant is proposing a single-use residential development. The City's new comprehensive plan, <i>Minneapolis 2040</i> , went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not have a base zoning district that matches the height and density called for on the Built Form map. The City will address any conflicts by amending the zoning code with the adoption of Built Form Overlay Districts. These overlay districts will contain performance standards that match the scale of development called for in the adopted Built Form Map. This work is currently underway and is expected to be complete by the end of 2020.
Goods and Services Corridor	Guidance	Staff Comment
Glenwood Avenue	Goods and Services Corridors serve two purposes: 1) To indicate where commercial uses should front in relation to properties guided for commercial future land uses, and 2) In addition to the guidance for the mixed use land use categories found in this section, Goods and Services Corridors identify where the establishment or expansion of commercial uses can be considered. Properties immediately adjacent to a Goods and Services Corridor may be considered for commercial activity, allowing for uses similar in scale and scope to the Neighborhood and Corridor Mixed Use categories.	The site is zoned C3A Community Activity Center District. No commercial uses are proposed in the development. The City's new comprehensive plan, <i>Minneapolis 2040</i> , went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not

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Built Form Guidance	Guidance	Staff Comment
<p>Transit 30</p>	<p>New and remodeled buildings in the Transit 30 district should reflect a variety of building types on both moderate and large sized lots. Upper floors of taller buildings should be set back to increase access to light and air. Building heights should be 10 to 30 stories. Building heights should be at least 10 stories in order to best take advantage of the access to transit, jobs, and goods and services provided by the Transit 30 district. Requests to exceed 30 stories will be evaluated on the basis of whether or not a taller building is a reasonable means for further achieving Comprehensive Plan goals.</p>	<p>The applicant is proposing to construct a four-story building on the site. The building would be short of the minimum requirement of 10 stories in the Transit 30 built form district. The City’s new comprehensive plan, <i>Minneapolis 2040</i>, went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not have a base zoning district that matches the height and density called for on the Built Form map. The City will address any conflicts by amending the zoning code with the adoption of Built Form Overlay Districts. These overlay districts will contain performance standards that match the scale of development called for in the adopted Built Form Map. This work is currently underway and is expected to be complete by the end of 2020.</p>

The following goals from *Minneapolis 2040 (2020)* apply to this proposal:

- Goal 3. Affordable and accessible housing: In 2040, all Minneapolis residents will be able to afford and access quality housing throughout the city.
- Goal 6. High-quality physical environment: In 2040, Minneapolis will enjoy a high-quality and distinctive physical environment in all parts of the city.
- Goal 9. Complete neighborhoods: In 2040, all Minneapolis residents will have access to employment, retail services, healthy food, parks, and other daily needs via walking, biking, and public transit.

The following policies and action steps from *Minneapolis 2040 (2020)* apply to this proposal:

Policy 6. Pedestrian-Oriented Building and Site Design: Regulate land uses, building design, and site design of new development consistent with a transportation system that prioritizes walking first, followed by bicycling and transit use, and lastly motor vehicle use.

- p. Discourage the establishment of and minimize the size of surface parking lots. Mitigate the negative effect of parking lots through screening, landscaping, minimizing curb cuts, sufficient number of down-cast, glare-free light fixture, and other measures.
- q. Encourage the design of parking areas in ways that minimize their contribution to the urban heat island.
6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use applications are approved, the proposal will comply with all provisions of the C3A Community Activity Center District and the DP Downtown Parking Overlay District.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required findings and applicable standards in the site plan review chapter:

Applicable Standards of Chapter 530, Site Plan Review

BUILDING PLACEMENT AND DESIGN

Building placement – Requires alternative compliance

- The first floor of the building is located within eight feet of the front property line along both Glenwood Avenue and East Lyndale Avenue North except the entryway is set back 23 feet from the front property line along Glenwood Avenue. Alternative compliance is required.
- The placement of the building reinforces the street wall, maximizes natural surveillance and visibility, and facilitates pedestrian access and circulation.
- The area between the building and the front property lines includes amenities such as landscaping and a placeholder for public art.
- All on-site accessory parking is located to the rear of the site.

Principal entrances – Meets requirements

- The proposed project would comply with the principal entrances standards as the principal entrance faces Glenwood Avenue.

Visual interest – Meets requirements

- The proposed project would comply with the visual interest standards.

Exterior materials – Meets requirements

- The applicant is proposing brick, glass, metal panel, fiber cement ($\leq 5/8''$) and fiber cement siding as the building’s primary exterior materials. Exterior material changes at a later date may require review by the Planning Commission and an amendment to the site plan review.
- Each elevation would comply with the City’s durability standards for exterior materials.
- The application is consistent with the City’s policy of allowing no more than three exterior materials per elevation, excluding windows, doors, and foundation materials.
- Plain face concrete block is not proposed along any public streets, sidewalks, or adjacent to a residence or office residence district.
- The exterior materials and appearance of the rear and side walls of the building are similar to and compatible with the front of the building.

Percentage of Exterior Materials per Elevation

Material	Allowed Max	North	South	East	West
Brick (face)	100%	2%	3%	6%	0%
Glass	100%	32%	33%	26%	30%
Metal Panel	75%	54%	47%	36%	54%
Fiber Cement ($\leq 5/8''$)	30%	0%	0%	25%	15%
Fiber Cement Siding	30%	12%	17%	7%	1%

Windows – Meets requirements

- The proposed project would comply with the minimum window requirements.

Window Requirements for Residential Uses

Floor	Code		Proposed	
1st floor facing Glenwood Avenue	20% minimum	264 sq. ft.	45%	594 sq. ft.
2nd floor and above facing Glenwood Avenue	10% minimum	182 sq. ft.	More than 10%	
1st floor facing East Lyndale Avenue North	20% minimum	155 sq. ft.	49%	384 sq. ft.
2nd floor and above facing East Lyndale Avenue North	10% minimum	107 sq. ft.	More than 10%	
1st floor facing the on-site surface parking lot	20% minimum	206 sq. ft.	37%	378 sq. ft.
2nd floor and above facing the on-site surface parking lot	10% minimum	142 sq. ft.	More than 10%	

Ground floor active functions – Meets requirements

- The proposed project would comply with the ground floor active functions requirements as 100 percent of the ground floor facing both Glenwood Avenue and East Lyndale Avenue North contains active functions.

Roof line – Meets requirements

- The principal roof line of the building would be flat which is similar to that of surrounding buildings.

Parking garages – Meets requirements

- The proposed parking garage would comply with the applicable site plan review standards as it is located underneath the building.

ACCESS AND CIRCULATION

Pedestrian access – Meets requirements

- There would be clear and well-lit walkways at least four feet in width connecting building entrances to the adjacent public sidewalk and on-site parking facilities.

Transit access – Not applicable

- No transit shelters are proposed as part of this development.

Vehicular access – Meets requirements

- The proposed project would comply with the vehicular access requirements. As proposed, all vehicle traffic will enter and exit the site from a curb cut along Glenwood Avenue.

LANDSCAPING AND SCREENING

General landscaping and screening – Meets requirements

- The proposed project would comply with the general landscaping and screening requirements.
- The remainder of the required landscaped area is covered with turf grass, native grasses, perennial flowering plants and ornamental trees.

Landscaping and Screening Requirements

Requirement	Required	Proposed
Lot Area	--	30,226 sq. ft.
Building Footprint	--	12,954 sq. ft.
Area Not Covered by Buildings	--	17,272 sq. ft.
Landscaped Area	3,455 sq. ft.	6,611 sq. ft.
Canopy Trees (1:500 sq. ft.)	7 trees	8 trees
Shrubs (1:100 sq. ft.)	35 shrubs	178 shrubs

Parking and loading landscaping and screening – Meets requirements

- The proposed project would comply with the applicable parking and loading landscaping and screening requirements.

Additional landscaping requirements – Meets requirements

- The project appears to comply with the additional landscaping requirements in sections 530.180, 530.190, 530.200, and 530.210 of the zoning code.

ADDITIONAL STANDARDS

Concrete curbs and wheel stops – Meets requirements

- The parking lot would be defined by a six-inch by six-inch continuous concrete curb.

Site context – Meets requirements

- The proposed project would comply with the site context requirements.

Crime prevention through environmental design – Meets requirements

- The proposed project would comply with crime prevention through environmental design (CPTED) standards.

Historic preservation – Not applicable

- This site is neither historically designated or located in a designated historic district, nor has it been determined to be eligible for designation.

Applicable Regulations of the Zoning Ordinance

The proposed use is permitted in the C3A Community Activity Center District. However, a surface parking lot is a conditional use in the DP Downtown Parking Overlay District.

Off-street Parking and Loading – Meets requirements

- The proposed project complies with the applicable vehicle parking, bicycle parking, and loading requirements.

Vehicle Parking Requirements Per Use (Chapter 541)

Use	Minimum	Reductions	Minimum	Maximum	Proposed
Residential Dwellings	46	Transit Incentives (23)	23	78	47
	46	(23)	46	78	47

Bicycle Parking Requirements (Chapter 541)

Use	Minimum	Short-Term	Long-Term	Proposed
Residential Dwellings	23	--	Not less than 90%	21 Long-Term 16 Short-Term
	23	--	21	21 Long-Term 16 Short-Term

Loading Requirements (Chapter 541)

Use	Loading Requirement	Loading Spaces	Proposed
Residential Dwellings	None	None	None

	None	None	None
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Building Bulk and Height – Meets requirements

- The proposed project would meet the applicable bulk and height requirements.

Building Bulk and Height Requirements

Requirement	Code	Bonuses	Total	Proposed
Lot Area	--	--	--	30,226 sq. ft. / .69 acres
Gross Floor Area	--	--	--	53,227 sq. ft.
Max. Floor Area Ratio	2.7	--	2.7	1.76
Max. Building Height	4 stories or 56 ft., whichever is less	--	4 stories or 56 ft., whichever is less	4 stories or 49 ft.

Lot and Residential Unit Requirements – Meets requirements

- The proposed project would meet the applicable lot and residential unit requirements.
- The proposed dwelling units meet the minimum gross floor area requirement of 500 square feet per unit.
- The application is subject to the delayed phase-in for smaller projects with 20-49 units and the project is not subject to the requirements of Inclusionary Zoning per section 535.90(c)(1) of the zoning code.

Lot and Residential Unit Requirements Summary

Requirement	Code	Proposed
Lot Area	5,000 sq. ft. min.	30,226 sq. ft.
Lot Width	40 ft. min.	206 ft. on Glenwood Avenue 115 ft. on East Lyndale Avenue North
Impervious Surface Area	100% max.	78%
Lot Coverage	100% max.	46%
Dwelling Units (DU)	--	46 DUs
Net Residential Area	--	43,124 sq. ft.

Yard Requirements – Meets requirements

- The project would meet the applicable yard requirements.

Minimum Yard Requirements

Setback	Zoning District	Overriding Regulations	Total Requirement	Proposed
Front (Glenwood Avenue)	0 ft.	--	0 ft.	Between 2 and 23 ft.
Front (East Lyndale Avenue North)	0 ft.	--	0 ft.	Between 3 and 7 ft.

Interior Side (North)	0 ft.	--	0 ft.	Between 7 and 59 ft.
Interior Side (East)	0 ft.	--	0 ft.	Between 32 and 42 ft.

Signs – Meets requirements with Conditions of Approval

- All signs are subject to Chapter 543, On-Premise Signs. The applicant will be required to submit a separate sign permit application for any signage that is proposed.
- In the C3A Community Activity Center District there can be one square foot of signage for every one linear foot of primary building wall. Wall signs are limited to 180 square feet in size. Projecting signs are limited to 48 square feet in size. There is no height limitation for either wall signs or projecting signs. Freestanding monument signs are limited to 80 square feet in size and can be no taller than 8 feet. The zoning code limits the number of freestanding signs on a zoning lot to one.
- The applicant has indicated that they would like to install a monument sign towards the corner of Glenwood Avenue and East Lyndale Avenue North and a wall sign on the building; however, no specific signs have been proposed.

Screening of Mechanical Equipment – Meets requirements

- Mechanical equipment is subject to the screening requirements of Chapter 535, Regulations of General Applicability.
- The applicant is proposing to locate a transformer towards the back of the building. The transformer will be screened from the public street by the building itself.

Refuse Screening – Meets requirements

- Refuse and recycling storage containers are subject to the screening requirements in Chapter 535, Regulations of General Applicability.
- All refuse and recycling storage containers are located within the building.

Lighting – Meets requirements

- Existing and proposed lighting must comply with Chapter 535, Regulations of General Applicability.
- The project is consistent with the applicable lighting requirements.

Fences – Not applicable

- Fences must comply with the requirements in Chapter 535, Regulations of General Applicability.
- The applicant is not proposing to have any fencing on the site.

Specific Development Standards – Not applicable

- The uses within the development are not subject to specific development standards in Chapter 536, Specific Development Standards.

DP Downtown Parking Overlay District Standards – Requires conditional use permit

- The site is located in the DP Downtown Parking Overlay District. Accessory surface parking lots require a conditional use permit in the overlay district. The applicant is proposing to have a 17-space surface parking lot on the site. The conditional use permit has been evaluated above.

Applicable Policies of the Comprehensive Plan

The proposed use would be consistent with the applicable guidance and policies of *Minneapolis 2040 (2020)*:

Future Land Use	Guidance	Staff Comment
Community Mixed Use	Large-scale mixed use development is encouraged throughout these areas, with commercial uses fronting on major streets. Commercial retail spaces are typically smaller in order to generate pedestrian activity, and are often a destination for customers coming from outside of the market area. Active uses that are accessible to the general public such as office, food service, retail, or medical establishments are required at the street level; therefore single-use residential development is not permitted. Contiguous expansion of commercial zoning is allowed.	The applicant is proposing a single-use residential development. The City's new comprehensive plan, <i>Minneapolis 2040</i> , went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not have a base zoning district that matches the height and density called for on the Built Form map. The City will address any conflicts by amending the zoning code with the adoption of Built Form Overlay Districts. These overlay districts will contain performance standards that match the scale of development called for in the adopted Built Form Map. This work is currently underway and is expected to be complete by the end of 2020.
Goods and Services Corridor	Guidance	Staff Comment
Glenwood Avenue	Goods and Services Corridors serve two purposes: 1) To indicate where commercial uses should front in relation to properties guided for commercial future land uses, and 2) In addition to the guidance for the mixed use land use categories found in this section, Goods and Services Corridors identify where the establishment or expansion of commercial uses can be considered. Properties immediately adjacent to a Goods and Services Corridor may be considered for commercial activity, allowing for uses similar in scale and scope to the Neighborhood and Corridor Mixed Use categories.	The site is zoned C3A Community Activity Center District. No commercial uses are proposed in the development. The City's new comprehensive plan, <i>Minneapolis 2040</i> , went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not have a base zoning district that

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Built Form	Guidance	Staff Comment
Transit 30	<p>New and remodeled buildings in the Transit 30 district should reflect a variety of building types on both moderate and large sized lots. Upper floors of taller buildings should be set back to increase access to light and air. Building heights should be 10 to 30 stories. Building heights should be at least 10 stories in order to best take advantage of the access to transit, jobs, and goods and services provided by the Transit 30 district. Requests to exceed 30 stories will be evaluated on the basis of whether or not a taller building is a reasonable means for further achieving Comprehensive Plan goals.</p>	<p>The applicant is proposing to construct a four-story building on the site. The building would be short of the minimum requirement of 10 stories in the Transit 30 built form district. The City's new comprehensive plan, <i>Minneapolis 2040</i>, went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not have a base zoning district that matches the height and density called for on the Built Form map. The City will address any conflicts by amending the zoning code with the adoption of Built Form Overlay Districts. These overlay districts will contain performance standards that match the scale of development called for in the adopted Built Form Map. This work is currently underway and is expected to be complete by the end of 2020.</p>

The following policies and action steps from *Minneapolis 2040 (2020)* apply to this proposal:

Policy 1. Access to Housing: Increase the supply of housing and its diversity of location and types.

- a. Allow housing to be built in all areas of the city, except in Production and Distribution areas.

- c. Allow multifamily housing on public transit routes, with higher densities along high-frequency routes and near METRO stations.
- f. Encourage inclusion of units that can accommodate families in new and rehabilitated multifamily housing developments.

Policy 5. Visual Quality of New Development: Ensure a high-quality and distinctive physical environment in all parts of the city through building and site design requirements for both large and small projects.

- a. Allow and encourage a variety of architectural styles.
- c. Ensure that exterior building materials are durable, sustainable, create a lasting addition to the built environment, and contribute positively to the public realm and reflect existing context.
- d. Require that the appearance and materials of the rear and side walls of new buildings are similar to and compatible with the front of the building.
- e. Require adequate distribution of windows and architectural features in order to create visual interest.
- h. Promote an attractive environment by minimizing visual clutter and confusion caused by a proliferation of signage; ensuring that signage is appropriately scaled to the pedestrian experience.
- j. Require the screening of utilities, communication, transformers, and other service connections to buildings. Burying connections and lines is encouraged.
- o. Regulate setbacks, orientation, pattern, materials, height, and scale of small-scale residential buildings to ensure consistency with built-form guidance and existing context.
- s. Balance visual quality of new development, including articulation of buildings, with energy efficiency of new buildings.

Alternative Compliance

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

Standard	Description	Staff Recommendation
Building Placement	The first floor of the building is located within eight feet of the front property line along both Glenwood Avenue and East Lyndale Avenue North except the entryway is set back 23 feet from the front property line along Glenwood Avenue.	The building has been designed to follow the curb line along Glenwood Avenue. Where the street jogs; so, does the building. This pushes the entryway back from the property line. The applicant is proposing to have landscaping and bike racks located in this space. <u>Staff recommends granting alternative compliance.</u>

PRELIMINARY/FINAL PLAT

The Department of Community Planning and Economic Development has analyzed the application for a preliminary and final plat, based on the following findings:

1. *The subdivision is in conformance with these land subdivision regulations, the applicable regulations of the zoning ordinance and policies of the comprehensive plan.*

Subdivision Regulations:

The subdivision is in conformance with the design requirements of the land subdivision regulations.

Zoning Ordinance:

The subdivision is in conformance with the applicable regulations of the zoning ordinance.

Comprehensive Plan:

The proposed project would be consistent with the applicable guidance and policies of Minneapolis 2040 (2020):

Future Land Use	Guidance	Staff Comment
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	<p>section, Goods and Services Corridors identify where the establishment or expansion of commercial uses can be considered. Properties immediately adjacent to a Goods and Services Corridor may be considered for commercial activity, allowing for uses similar in scale and scope to the Neighborhood and Corridor Mixed Use categories.</p>	<p>1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not have a base zoning district that matches the height and density called for on the Built Form map. The City will address any conflicts by amending the zoning code with the adoption of Built Form Overlay Districts. These overlay districts will contain performance standards that match the scale of development called for in the adopted Built Form Map. This work is currently underway and is expected to be complete by the end of 2020.</p>
Built Form	Guidance	Staff Comment
<p>Transit 30</p>	<p>New and remodeled buildings in the Transit 30 district should reflect a variety of building types on both moderate and large sized lots. Upper floors of taller buildings should be set back to increase access to light and air. Building heights should be 10 to 30 stories. Building heights should be at least 10 stories in order to best take advantage of the access to transit, jobs, and goods and services provided by the Transit 30 district. Requests to exceed 30 stories will be evaluated on the basis of whether or not a taller building is a reasonable means for further achieving Comprehensive Plan goals.</p>	<p>The applicant is proposing to construct a four-story building on the site. The building would be short of the minimum requirement of 10 stories in the Transit 30 built form district. The City's new comprehensive plan, <i>Minneapolis 2040</i>, went into effect on January 1, 2020. The new comprehensive plan contains parcel-specific guidance for Land Use and Built Form for the entire City. The Built Form map guides the scale of development for every parcel in the city, independent of the uses allowed on the site. In some cases, the current zoning code does not have a base zoning district that matches the height and density called for on the Built Form map. The City will address any conflicts by amending the zoning code with the adoption of Built Form Overlay Districts. These overlay districts will contain</p>

		<p>performance standards that match the scale of development called for in the adopted Built Form Map. This work is currently underway and is expected to be complete by the end of 2020.</p>
--	--	---

The following policies and action steps from Minneapolis 2040 (2020) apply to this proposal:

Policy 10. Street Grid: Restore and maintain the traditional street and sidewalk grid.

- a. Explore options to restore the street grid or add new streets and sidewalks to larger blocks or tracts of land as part of new development or redevelopment.
- d. Maintain the City’s ownership of streets in order to preserve the network of city streets and arterials.
- e. Restore the street grid when appropriate, taking advantage of opportunities as they arise in conjunction with new development and through capital projects. Explore options to proactively dedicate capital improvement funds to reconnect the street grid, so resources are already available when opportunities arise. Where restoring auto access is no longer feasible or aligned with other City plans, the City will pursue restoring streets for use by pedestrians, bicyclists, or transit users.

- 2. *The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.*

The applicant is proposing to combine several platted lots into one platted lot and one outlot. The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.

- 3. *All land intended for building sites can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.*

The land intended for building can be used safely without endangering the users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.

- 4. *The lot arrangement is such that there will be no foreseeable difficulties, for reasons of topography or other conditions, in securing building permits and in providing driveway access to buildings on such lots from an approved street. Each lot created through subdivision is suitable in its natural state for the proposed use with minimal alteration.*

The parcels created by this application presents no foreseeable difficulties for development. No significant alterations to the land appear necessary.

- 5. *The subdivision makes adequate provision for stormwater runoff, and temporary and permanent erosion control in accordance with the rules, regulations and standards of the city engineer and the requirements of these land subdivision regulations. To the extent practicable, the amount of stormwater runoff from the site after development will not exceed the amount occurring prior to development.*

The Public Works Department has reviewed the preliminary plans and will review the final plans for compliance with standards related to access and circulation, drainage, and sewer/water connections. The applicant will be required to continue to work closely with the Public Works Department, the Plan Review

Section of CPED and the various utility companies during the duration of the development to ensure that all procedures are followed and that the development complies with all city and other applicable requirements. The applicant is aware that the final plans are expected to incorporate any applicable comments or modifications as required by the Public Works Department.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by Woda Cooper Development, Inc. for the properties located at 188 ½ Glenwood Avenue and 151 Holden Street North:

A. Conditional Use Permit.

Recommended motion: **Approve** the conditional use permit to allow a 17-space surface parking lot located in the DP Downtown Parking Overlay District, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.

B. Site Plan Review.

Recommended motion: **Approve** the site plan review for a new four-story, 46-unit residential building, subject to the following conditions:

1. All site improvements shall be completed by November 16, 2022, unless extended by the Zoning Administrator, or the permit may be revoked for non-compliance.
2. CPED staff shall review and approve the final site, elevation, landscaping, and lighting plans before building permits may be issued.
3. All signs shall comply with Chapter 543, On-Premise Signs of the zoning code. All signage requires a separate permit from CPED.

C. Preliminary/Final Plat.

Recommended motion: **Approve** the application for a preliminary and final plat.

ATTACHMENTS

1. Written description and findings submitted by applicant
2. Civil and architectural plans
3. Correspondence



October 14, 2020

RE: Statement of Proposed Use of All Land to be Subdivided and Description of the Project

Woda Cooper Development, Inc. is submitting a Land Use Application for a proposed family development located at 188 ½ Glenwood Avenue, Minneapolis, MN 55405 in the North Loop neighborhood. The Land Use Application is for a 4-story apartment building, known as Shelby Commons, an affordable housing development. Shelby Commons will be developed by Woda Cooper Development, Inc. with Project for Pride in Living, Inc. (PPL) as a co-developer. This will provide for the development of 46 low-income housing units, all affordable. These units will consist of 11 one-bedroom units, 23 two-bedroom units, and 12 three-bedroom units.

PPL will be providing supportive services, including on-site case management, for 12 units. These supportive services are provided as opportunities to residents at their own will. Residents are not required to use these services. 5 of the supportive housing units will be one-bedroom units for persons with disability. PPL will use its Housing Supports contract to help subsidize the rents of these units. The remaining 7 supportive units will be set aside for high priority homeless with 4 of these units being one-bedroom units, and 3 being two-bedroom units. Shelby Commons has been awarded 12 project-based section 8 vouchers from the Minneapolis Public Housing Authority and is currently awaiting a response from Minnesota Housing Finance Agency and the Minneapolis Community Planning and Economic Development on its applications for 9% low income housing tax credits. Shelby Commons will use 7 of the 12 vouchers to subsidize the rents of the supportive units set aside for high priority homeless. The remaining 5 vouchers will be used on general occupancy units, 1 on a two-bedroom unit, and 4 on three-bedroom units.

Units will be available to residents at varying income levels, with some units set aside at 30%, 50%, 60%, and 80% of the HUD Multifamily Tax Subsidy Projects (MTSP) income limits. In addition to this, all units will be affordable to households at the 60% HUD MTSP income limits. This presents a great opportunity for low income households at or below the 80% income limit to find quality living at a rate which would be very affordable to such households.

Other amenity spaces Shelby Commons has to offer include a community room, community laundry, and a supportive services office. There will also be an outdoor playground for children along with community garden plots. Along Glenwood Avenue, Shelby Commons will feature a unique urban rain garden, and there will be an opportunity for public art, both on the sidewalk/entry plaza and on select portions of the building exterior facade. Because Shelby Commons is a large family development, we have provided parking at a ratio just over 1 to 1, with 47 total parking spaces, 32 of which are in an underground garage, and 15 of which are in a surface lot located at the rear of the building.

Woda Cooper Development is seeking to replat the two tax parcels at 188 ½ Glenwood Avenue. These parcels have tax identification numbers of 22-029-24-33-0053, and 22-029-24-33-0058. The city of Minneapolis is hoping to construct a Border Avenue Extension project to extend the existing

Border Avenue south to Glenwood Avenue. Woda Cooper Development intends to replat the land to the west of this as Lot 1, and the remaining land, the land of the proposed Border Avenue Extension and the land to the east, as Outlot A. As described previously, Lot 1 will be used for the Shelby Commons development. Outlot A will be designated as an outlot for the time being until the future proposed roadway extension proceeds, at which time it will be final platted.

Sincerely,

A handwritten signature in black ink that reads "Parker Zee". The signature is written in a cursive, flowing style.

Parker Zee

Assistant Vice President of Development and Data Manager





October 1, 2020

RE: Statement on the Conditional Use Permit for a 15-Space Accessory Surface Parking Lot

Woda Cooper Development, Inc. is submitting a Land Use Application for a proposed family development located at 188 ½ Glenwood Avenue, Minneapolis, MN 55405 in the North Loop neighborhood. The Land Use Application is for a 4-story apartment building, known as Shelby Commons, an affordable housing development. Shelby Commons will be developed by Woda Cooper Development, Inc. with Project for Pride in Living, Inc. (PPL) as a co-developer. This will provide for the development of 46 low-income housing units, all affordable. These units will consist of 11 one-bedroom units, 23 two-bedroom units, and 12 three-bedroom units.

Woda Cooper Development respectfully requests a Conditional Use Permit for a 15-space accessory surface parking lot at the Shelby Commons development. This development is located in the C3A Community Activity Center District as well as the DP Downtown Parking Overlay District. The DP district requires a Conditional Use Permit for any accessory parking lot. The DP district has two specific provisions that any accessory parking lot must adhere to:

1. The parking lot shall be located on the same zoning lot as the principal use served.
2. The number of parking spaces shall not exceed twenty (20) spaces.

The 15-space accessory surface parking lot meets both of these requirements and is not in violation of the underlying C3A zoning district.

This surface parking lot is located on the interior of the property, behind the proposed apartment building, so this will not be detrimental to public health, safety, comfort, or general welfare. This surface lot will provide a benefit to the community. Shelby Commons is a large multifamily development with 46 total units. With this lot, there will be a total of 47 parking spaces (32 will be in an underground garage) that may be used by residents. Despite being near convenient transportation options in a large city, it is anticipated that many low-income residents will need cars and will benefit from the inclusion of these spaces. Some households may not have cars, but there will also be households that require 2 cars for their commuting needs. If residents have more convenient parking available just outside their home, they will not be forced to search for monthly parking spaces in other areas within the city of Minneapolis, and residents will not need to park on streets. This will prove to be very important, with many residents in the labor force who will require convenient access to parking.

Being a small, private surface lot only available to residents at Shelby Commons, this use will not impact the enjoyment of other property in the vicinity of the site. This use also will not intrude on the ability to develop adjacent property.

This development will have adequate utilities, access roads, drainage, and other facilities to provide for the surface parking lot. Access will already be provided to give access into the underground parking garage, and this will easily be extended right into the surface parking lot. Additionally, there

will be enough exterior lighting provided to ensure this surface parking lot is safe for all residents. No other facilities are needed for this use.

Adequate measures have been taken to minimize traffic congestion in the public streets. No access to the development has changed as a result of adding this surface parking lot, but the inclusion of extra spaces in the lot allows for more residents to park in this private lot. By parking at the development, these residents will not need to park on the street or in other lots, and residents will not have a need to temporarily stop on Glenwood Avenue for loading/unloading, which could result in a traffic lane being blocked.

Although this surface parking lot may detract from the need to use public transit, this conditional use is consistent with the Minneapolis 2040 plan. The comprehensive plan seeks to provide more affordable and accessible housing. This lot will be used for affordable housing and is beneficial to the large families that will live in Shelby Commons. The comprehensive plan also focuses on the need for jobs. Being able to use the spaces in this lot will allow residents to drive to jobs that may have been more difficult to access by walking, biking, or public transit. Being able to drive their own cars will significantly increase opportunities to find employment anywhere in Minneapolis.

Another goal of the comprehensive plan is to eliminate disparities. Residents at Shelby Commons are disadvantaged from an income standpoint and will be a very diverse community. By providing additional parking, more residents will have the ability thrive with access to amenities or jobs nearby that are most easily accessible by car. This will give all residents an opportunity to improve their wealth, helping to eliminate this income disparity.

For the required findings presented along with the adherence to the Downtown Parking Overlay District provisions, Woda Cooper Development believes that a 15-space surface parking lot will prove to be beneficial to the community as a whole, but will most significantly benefit the residents of Shelby Commons.

Thank you for your consideration of this Conditional Use Permit request.

Sincerely,



Parker Zee
Assistant Vice President of Development and Data Manager

SHELBY COMMONS

188 1/2 GLENWOOD AVE, MINNEAPOLIS, MN 55401
 LAND USE APPLICATION/PDR SUBMITTAL - 10/02/2020

Boorman
 Kroos
 Vogel
 Group
 Inc.

222 North Second Street
 Long & Kees Bldg, Suite 101
 Minneapolis, MN 55401
 Telephone: 612.339.3752
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 EOE



211 eleventh avenue nw
 rochester, mn 55901
 p. 507.206.6201
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 www.crwarchitecture.com

PROJECT TITLE

SHELBY COMMONS

ISSUE #	DATE	DESCRIPTION
1	10/02/2020	LAND USE APPLICATION

PROJECT RENDERING



PROJECT MAP



SHEET INDEX

File #	Discipline	Sheet Number	Sheet Name	2020-10-02 LAND USE APPLICATION	2020-10-02 PDR SUBMITTAL
001	0- GENERAL	G1-0	COVER / SHEET INDEX	X	X
002	1- SURVEYOR	1	ALTA LAND TITLE SURVEY	X	X
003	1- SURVEYOR	1 of 1	PRELIMINARY PLAT	X	X
004	2- CIVIL	C100	GENERAL NOTES AND LEGEND	X	X
005	2- CIVIL	C200	SITE DEMOLITION PLAN	X	X
006	2- CIVIL	C300	SWPPP GENERAL INFORMATION	X	X
007	2- CIVIL	C301	SWPPP - NARRATIVE	X	X
008	2- CIVIL	C302	SWPPP - EXISTING CONDITIONS	X	X
009	2- CIVIL	C303	SWPPP - PROPOSED CONDITIONS	X	X
010	2- CIVIL	C304	SWPPP DETAILS	X	X
011	2- CIVIL	C400	GRAZING AND DRAINAGE PLAN	X	X
012	2- CIVIL	C500	UTILITY PLAN	X	X
013	2- CIVIL	C600	SITE LAYOUT AND PAVING PLAN	X	X
014	2- CIVIL	C700	CIVIL DETAILS	X	X
015	2- CIVIL	C701	CIVIL DETAILS	X	X
016	3- LANDSCAPE	L100	SITE LANDSCAPE PLAN	X	X
017	4- ARCH	A1-0	SITE PLAN	X	X
018	4- ARCH	A2-0-0	GARAGE PLAN	X	X
019	4- ARCH	A2-0-1	LEVEL 1	X	X
020	4- ARCH	A2-0-2	LEVEL 2	X	X
021	4- ARCH	A2-0-3	LEVEL 3	X	X
022	4- ARCH	A2-0-4	LEVEL 4	X	X
023	4- ARCH	A2-0-5	ROOF PLAN	X	X
024	4- ARCH	A3-0	ELEVATIONS	X	X
025	4- ARCH	A4-0	RENDERINGS	X	X
026	4- ARCH	A5-0	SHADOW STUDY	X	X
027	5- ELEC	E-011	ELECTRICAL SITE PHOTOMETRIC PLAN	X	X

PROJECT TEAM

OWNER / APPLICANT: Woda Cooper Development, Inc.
 500 S Front Street, 10th Floor
 Columbus, OH 43215
 Phone: 540.351.7877
 Contact: Parker Zee

Architect:
 Boorman Kroos Vogel Group, Inc.
 222 North Second Street
 Minneapolis, Minnesota 55401
 Phone: 612.339.3752
 Fax: 612.339.6212
 Contact: Michael Krych

Civil:
 Pierce Pini + Associates, Inc.
 9290 Central Avenue NE Suite 312
 Blaine, MN, 55434
 Phone: 763.537.1311
 Contact: Rhonda Pierce

Landscape:
 Boorman Kroos Vogel Group, Inc.
 222 North Second Street
 Minneapolis, Minnesota 55401
 Phone: 612.339.3752
 Fax: 612.339.6212
 Contact: Brady Halverson

CERTIFICATION

NOT FOR CONSTRUCTION

DRAWN BY	Author
CHECKED BY	Checker
COMMISSION NUMBER	2385 03

SHEET TITLE

COVER / SHEET INDEX

SHEET NUMBER

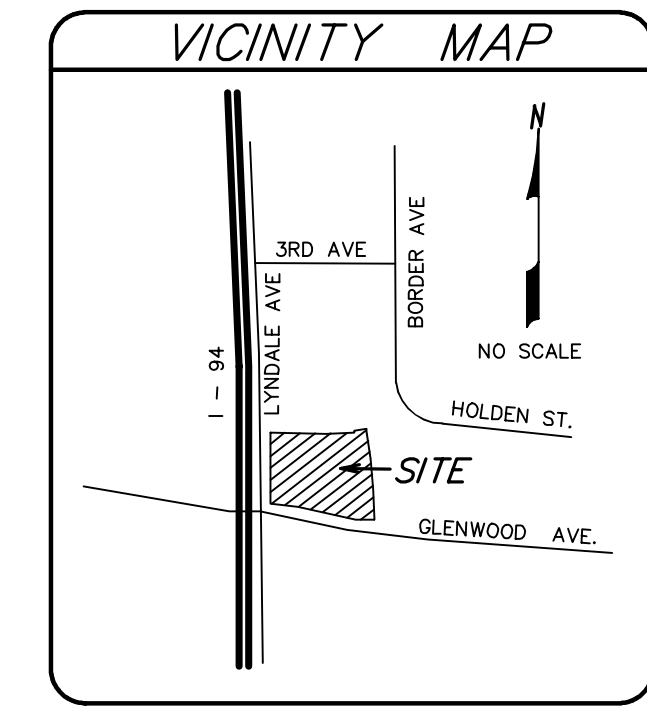
G1-0



- BENCHMARKS (BM)**
(NGVD 29)
- 1.) Top of top nut of fire hydrant northeast quadrant of East Lyndale Avenue and 3rd Avenue N.
Elevation = 825.42 feet.
 - 2.) Top of top nut of fire hydrant northeast quadrant of East Lyndale Avenue and Glenwood Avenue.
Elevation = 827.99 feet.
 - 3.) Top of top nut of fire hydrant 250 feet +/- east of East Lyndale Avenue on north side of Glenwood Avenue.
Elevation = 825.29 feet.

- LEGEND**
- Denotes iron monument set marked with P.L.S. No. 44890
 - Denotes found PLS 15480 iron monument
 - ⊙ Denotes 1.17 inch diameter copper magnetized markers and disc cap affixed stamped LS-44890 set

- BE Denotes building entrance
- BTL Denotes beaver tail curb
- CB Denotes catch basin
- CMH Denotes communication manhole
- COL Denotes building column
- EM Denotes electric meter
- EMH Denotes electric manhole
- EO Denotes electric outlet
- G Denotes concrete gutter
- GAS V Denotes gas valve
- GM Denotes gas meter
- GP Denotes guard post
- GW Denotes guy wire
- HCPB Denotes handicap crosswalk push button
- HCR Denotes disabled ramp
- HCS Denotes disabled sign
- HHC Denotes communication handhole
- HHE Denotes electric handhole
- HHF Denotes fiber optic handhole
- HYD Denotes fire hydrant
- INV Denotes structure invert
- LA Denotes landscaping
- LD Denotes loading dock
- LP Denotes light pole
- MC Denotes metal cover
- MH Denotes manhole
- OD Denotes overhead door
- OHE Denotes overhead electric line
- OHU Denotes overhead utility lines
- (P) Denotes per plan
- PKCS Denotes parking sign
- PP Denotes power pole
- PPLP Denotes power and light pole
- PPT Denotes power pole with transformer
- PPU Denotes power pole with underground utilities
- PVC Denotes polyvinylchloride pipe
- RCP Denotes reinforced concrete pipe
- SAN Denotes sanitary manhole
- SAN S Denotes sanitary sewer
- SMH Denotes storm manhole
- ST S Denotes storm sewer
- TS Denotes top of concrete curb
- TCS Denotes traffic control sign
- TL Denotes traffic light
- UGC Denotes underground communication line
- UGE Denotes underground electric line
- W Denotes water line
- WM Denotes water manhole
- WV Denotes water valve
- BAS Denotes Basswood tree
- COT Denotes Cottonwood tree
- EVG Denotes evergreen tree
- MPL Denotes Maple tree



DESCRIPTION OF PROPERTY SURVEYED
(Per Fidelity National Title Insurance Company Commitment for Title Insurance File No. 19-1085, effective date June 22, 2020)

All those portions of the following described parcel of land that lie West of a line that is the Northernly extension of the West line of Lot 20, Block 7, Oak Lake Addition to the City of Minneapolis:

All of Lot 13 and those parts of Lots 5, 6, 7, 8, 9, 10, 11 and 12, Block 7 and those parts of Lots 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11 and 12, Block 8, together with the part of the vacated alley in Blocks 7 and 8 and part of vacated Border Avenue and Holden Street, all in Oak Lake Addition to the City of Minneapolis, Hennepin County, Minnesota described as follows: Commencing at a point 115 feet Easterly of the Northeast corner of Lot 9 in said Block 8; and 155' Northerly of the Southeast corner of Lot 12 in said Block 8; thence Westerly 115 feet to the Northeast corner of said Lot 9; thence Westerly 135 feet to a point 139 feet Northerly of the Southwest corner of Lot 7 in said Block 8; thence Southerly 124.6 feet along a line, which, if extended would intersect the Southwest corner of said Lot 7 to a point hereinafter referred to as Point "A"; thence Easterly deflecting to the left at an angle of 99 degrees 30 minutes along a line hereinafter referred to as Line "A" to an intersection with a line hereinafter referred to as Line "B" and the point of beginning of the tract to be described; thence continuing along the Easterly extension of said Line "A" to a point 203.5 feet Easterly from said Point "A"; thence Easterly deflecting to the left at an angle of 2 degrees 34 minutes a distance of 53 feet; thence deflecting left at an angle of 82 degrees 21 minutes, a distance of 3 feet; thence Easterly deflecting right 82 degrees 21 minutes a distance of 16.27 feet to an intersection with a straight line from the point of commencement to the Southwest corner of Lot 12 in said Block 8; thence Easterly to a point in the Southerly extension of the East Line of Block 3, Glenwood Renewal Subdivision No. 2; distance 362.76 feet Southerly from the Southeast corner of said Block 7; thence Northerly along said Southerly extension to an intersection with the Westerly extension of the North line of Lot 11 in said Block 7; thence Easterly along said Southerly extension and along the North lines of Lots 11, 10, 9, 8, 7, 6 and 5 of said Block 7 to a point in the North line of said Lot 5 which lies 25.58 feet Westerly from the Northeast corner thereof; thence Southwesterly to a point in the West line of Lot 12 in said Block 7, which lies 30.24 feet Northwesterly from the Southwest corner of said Lot 12; thence Southerly along said West line to said Southwest corner; thence Southerly at right angles to the South line of said Lot 12 a distance of 6 feet to the centerline of the vacated alley in said Block 7; thence Westerly along said vacated alley centerline and its Westerly extension to an intersection with the centerline of vacated Border Avenue; thence Southerly along said centerline of vacated Border Avenue to an intersection with the Easterly extension of the South line of Lot 1 in said Block 8; thence Westerly along said Easterly extension to the Southeast corner of said Lot 1; thence Northwesterly to the intersection of the East line of Lot 5 in said Block 8 with a line 80 feet Southerly from and parallel to the North line of said Lot 5; thence Westerly along said parallel line a distance of 45 feet; thence Northerly along a line passing through a point on the South line of Lot 7 in said Block 8 distant 64 feet West of the Southeast corner thereof, said line being the line referred to herein as Line "B"; to an intersection with Line "A" and the point of beginning.

PLAT RECORDING INFORMATION

The plat of Oak Lake Addition to the City of Minneapolis was filed of record on December 15, 1873.

[] Bearings and/or dimensions listed within brackets are per plat or record documents.

TITLE COMMITMENT

Fidelity National Title Insurance Company Commitment for Title Insurance File No. 19-1085, effective date June 22, 2020, was relied upon as to matters of record.

Schedule B Exceptions:

1. Exceptions are indicated on survey with circled numbers unless otherwise noted. Items not listed below are standard exceptions and/or are not survey related.
9. Easement for access granted to the State of Minnesota appearing of record in the deed dated November 29, 1967, and recorded December 15, 1967, as Document No. 3692930 in Deed Book 2618, page 350, Hennepin County records.
10. Easements and rights of access control for trunk highway purposes in favor of the State of Minnesota and conveyed into the County of Hennepin and the City of Minneapolis as follows:
 - a. Deed dated November 17, 1993, and recorded December 9, 1993, as Document No. 6197365, Hennepin County records.
 - b. Deed dated November 17, 1993, and recorded January 13, 1998, as Document No. 6834705, Hennepin County records.
 - c. Deed dated November 18, 1993, and recorded February 19, 1998, as Document No. 6851611, Hennepin County records.

FLOOD ZONE NOTE

1.) The subject property appears to lie within Zone X (Areas determined to be outside the 0.2% annual chance floodplain) per the National Flood Insurance Program, Flood Insurance Rate Map Community Panel No. 2701720356F, dated November 4, 2016. This information was obtained from the FEMA Map Service Center web site.

PARKING

No designated parking stalls observed on site.

AREA

Gross = 54,783 square feet or 1.258 acres
Area of property description lying west of proposed Border Ave. = 30,220 square feet or 0.69 acres +/-

LIST OF POSSIBLE ENCROACHMENTS

- The following list of possible encroachments is only the opinion of this surveyor; should not be interpreted as a legal opinion and should not be interpreted as a complete listing.
- A) Possible encroachments are indicated on survey with boxed letters as listed below.
 - A.) Fences crossing property lines
 - B.) Storm sewer crossing property line
 - C.) Overhead electric over property lines
 - D.) Guywire over property line

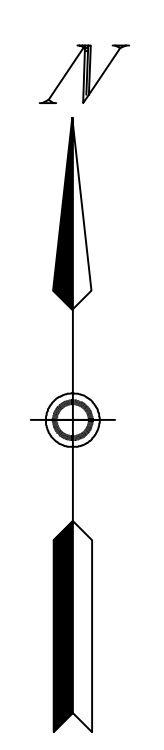
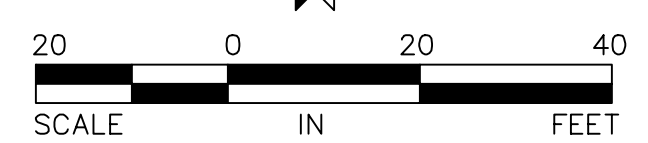
GENERAL NOTES

- 1.) Survey coordinate basis: Hennepin County Coordinate System (NAD83 1986)
- 2.) "No Buildings Observed" on the surveyed property in the process of conducting the fieldwork.
- 3.) Adjoining ownership information shown hereon was obtained from the Hennepin County Property Tax Information web site.
- 4.) There was no evidence of recent earth moving work, building construction or building additions observed in the process of conducting the fieldwork.
- 5.) There is a proposed Border Avenue extension running through the site. There was no evidence of recent street or sidewalk construction or repairs observed in the process of conducting the fieldwork.
- 6.) There were no wetland delineation markers observed in the process of conducting the fieldwork.

UTILITY NOTES

- 1.) Utility information from plans and markings was combined with observed evidence of utilities to develop a view of the underground utilities shown hereon. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. In addition, Gopher State One Call locate requests from surveyors may be ignored or result in an incomplete response. Where additional or more detailed information is required, excavation and/or a private utility locate request may be necessary.
- 2.) Other underground utilities of which we are unaware may exist. Verify all utilities critical to construction or design.
- 3.) Some underground utility locations are shown as marked onsite by those utility companies whose locators responded to our Gopher State One Call, ticket number 202370702.
- 4.) Contact GOPHER STATE ONE CALL at 651-454-0002 (800-252-1166) for precise onsite location of utilities prior to any excavation.

Hennepin County, Minnesota.
Abstract Property



Dated this 10th day of September, 2020.
SUNDE LAND SURVEYING, LLC.
By: *Leonard F. Carlson*
Leonard F. Carlson, P.L.S. Minn. Lic. No. 44890



Revision: _____ By: _____ Date: _____
MAP

Drawing Title:
**ALTA/NSPS LAND TITLE SURVEY FOR:
SHELBY COMMONS LP**
188 1/2 GLENWOOD AVENUE, MINNEAPOLIS, MN

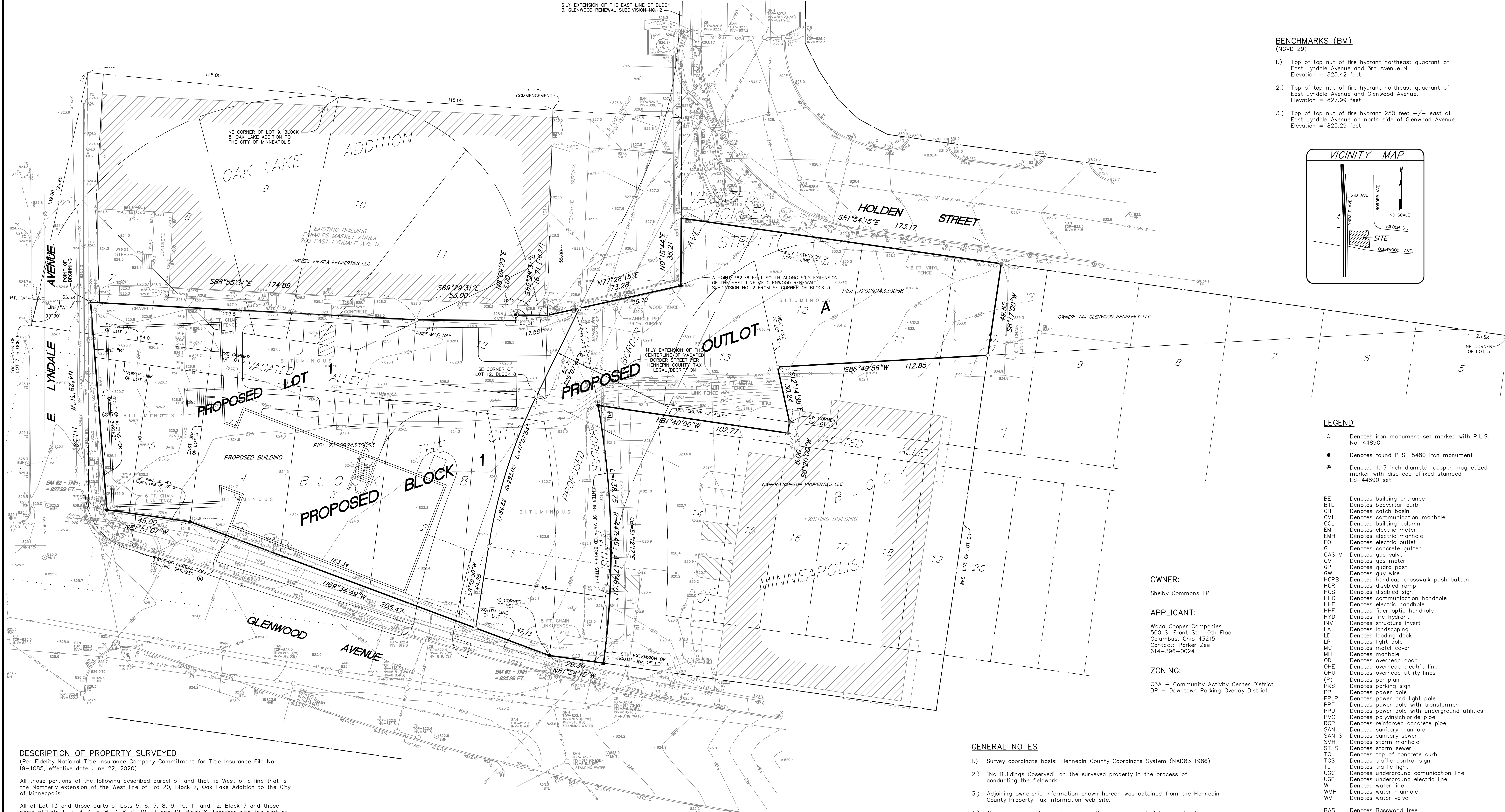
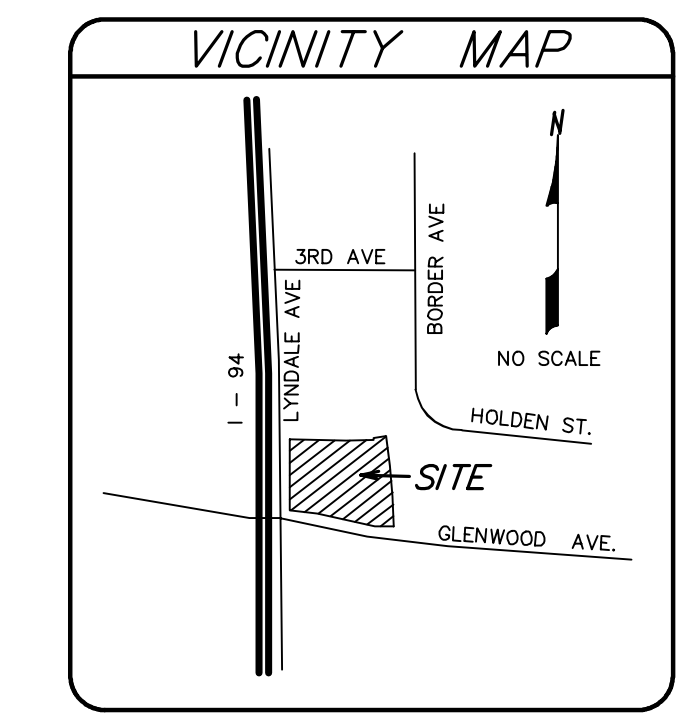
SUNDE LAND SURVEYING
www.sunde.com

9001 East Bloomington Freeway (35W) + Suite 118
Bloomington, Minnesota 55420-3435
952-881-2455 (Fax: 952-888-9828)

Project: 2016-103-A Bk/Pg: 977/83 Date: 9/10/2020
Township: 29 Range: 24 Section: 22
File: 2016103A001.dwg Sheet: 1 of 1

BENCHMARKS (BM) (NGVD 29)

- 1.) Top of top nut of fire hydrant northeast quadrant of East Lyndale Avenue and 3rd Avenue N. Elevation = 825.42 feet.
2.) Top of top nut of fire hydrant northeast quadrant of East Lyndale Avenue and Glenwood Avenue. Elevation = 827.99 feet.
3.) Top of top nut of fire hydrant 250 feet +/- east of East Lyndale Avenue on north side of Glenwood Avenue. Elevation = 825.29 feet.



LEGEND table listing symbols for iron monuments, building entrances, catch basins, communication manholes, etc.

OWNER: Shelby Commons LP
APPLICANT: Weda Cooper Companies
ZONING: C3A - Community Activity Center District, DP - Downtown Parking Overlay District

DESCRIPTION OF PROPERTY SURVEYED

(Per Fidelity National Title Insurance Company Commitment for Title Insurance File No. 19-1085, effective date June 22, 2020)

All those portions of the following described parcel of land that lie West of a line that is the Northernly extension of the West line of Lot 20, Block 7, Oak Lake Addition to the City of Minneapolis:

All of Lot 13 and those parts of Lots 5, 6, 7, 8, 9, 10, 11 and 12, Block 7 and those parts of Lots 1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11 and 12, Block 8, together with the part of the vacated alley in Blocks 7 and 8 and part of vacated Border Avenue and Holden Street, all in Oak Lake Addition to the City of Minneapolis, Hennepin County, Minnesota described as follows: Commencing at a point 115 feet Easterly of the Northeast corner of Lot 9 in said Block 8; and 155' Northerly of the Southeast corner of Lot 12 in said Block 8; thence Westerly 115 feet to the Northeast corner of said Lot 9; thence Westerly 135 feet to a point 139 feet Northerly of the Southwest corner of Lot 7 in said Block 8; thence Southerly 124.6 feet along a line, which, if extended would intersect the Southwest corner of said Lot 7 to a point hereinafter referred to as Point "A"; thence Easterly deflecting to the left at an angle of 99 degrees 30 minutes along a line hereinafter referred to as Line "A" to an intersection with a line hereinafter referred to as Line "B" and the point of beginning of the tract to be described; thence continuing along the Easterly extension of said Line "A" to a point 203.5 feet Easterly from said Point "A"; thence Easterly deflecting left at an angle of 2 degrees 34 minutes a distance of 53 feet; thence deflecting left at an angle of 82 degrees 21 minutes, a distance of 3 feet; thence Easterly deflecting right 82 degrees 21 minutes a distance of 16.27 feet to an intersection with a straight line from the point of commencement to the Southwest corner of Lot 12 in said Block 8; thence Southerly to a point in the Southerly extension of the East Line of Block 3, Glenwood Renewal Subdivision No. 2", distance 362.76 feet Southerly from the Southeast corner of said Block 3; thence Northerly along said Southerly extension to an intersection with the Westerly extension of the North line of Lot 11 in said Block 7; thence Easterly along said Westerly extension and along the North lines of Lots 11, 10, 9, 8, 7, 6 and 5 of said Block 7 to a point in the North line of said Lot 5 which lies 25.58 feet Westerly from the Northeast corner thereof; thence Southwesterly to a point in the West line of Lot 12 in said Block 7, which lies 30.24 feet Northwesterly from the Southwest corner of said Lot 12; thence Southerly along said West line to said Southwest corner; thence Southerly at right angles to the South line of said Lot 12 a distance of 6 feet to the centerline of the vacated alley in said Block 7; thence Westerly along said vacated alley centerline and its Westerly extension to an intersection with the centerline of vacated Border Avenue; thence Southerly along said centerline of vacated Border Avenue to an intersection with the Easterly extension of the South line of Lot 1 in said Block 8; thence Easterly along said Easterly extension to the Southeast corner of said Lot 1; thence Northwesterly to the intersection of the East line of Lot 5 in said Block 8 with a line 80 feet Southerly from and parallel to the North line of said Lot 5; thence Westerly along said parallel line a distance of 45 feet; thence Northerly along a line passing through a point on the South line of Lot 7 in said Block 8 distant 64 feet West of the Southeast corner thereof, said line being the line referred to herein as Line "B", to an intersection with Line "A" and the point of beginning.

PLAT RECORDING INFORMATION

The plat of Oak Lake Addition to the City of Minneapolis was filed of record on December 15, 1873.

[] Bearings and/or dimensions listed within brackets are per plat or record documents.

TITLE COMMITMENT

Fidelity National Title Insurance Company Commitment for Title Insurance File No. 19-1085, effective date June 22, 2020, was relied upon as to matters of record.

Schedule B Exceptions:

- 1. Exceptions are indicated on survey with circled numbers unless otherwise noted. Items not listed below are standard exceptions and/or are not survey related.
9. Easement for access granted to the State of Minnesota appearing of record in the deed dated November 29, 1967, and recorded December 18, 1967, as Document No. 3692930 in Deed Book 2618, page 350, Hennepin County records.
10. Easements and rights of access control for trunk highway purposes in favor of the State of Minnesota and conveyed unto the County of Hennepin and the City of Minneapolis as follows:
a. Deed dated November 17, 1993, and recorded December 9, 1993, as Document No. 6197365, Hennepin County records.
b. Deed dated November 17, 1993, and recorded January 13, 1998, as Document No. 6834705, Hennepin County records.
c. Deed dated November 18, 1993, and recorded February 19, 1998, as Document No. 6851611, Hennepin County records.

FLOOD ZONE NOTE

- 1.) The subject property appears to lie within Zone X (Areas determined to be outside the 0.2% annual chance floodplain) per the National Flood Insurance Program, Flood Insurance Rate Map Community Panel No. 2701720356F, dated November 4, 2016. This information was obtained from the FEMA Map Service Center web site.

PARKING

No designated parking stalls observed on site.

AREAS

Lot 1, Block 1 = 30,226 square feet or 0.694 acres
Outlot A = 24,557 square feet or 0.564 acres
Gross = 54,783 square feet or 1.258 acres

GENERAL NOTES

- 1.) Survey coordinate basis: Hennepin County Coordinate System (NAD83 1986)
2.) "No Buildings Observed" on the surveyed property in the process of conducting the fieldwork.
3.) Adjoining ownership information shown hereon was obtained from the Hennepin County Property Tax Information web site.
4.) There was no evidence of recent earth moving work, building construction or building additions observed in the process of conducting the fieldwork.
5.) There is a proposed Border Avenue extension running through the site. There was no evidence of recent street or sidewalk construction or repairs observed in the process of conducting the fieldwork.
6.) There were no wetland delineation markers observed in the process of conducting the fieldwork.

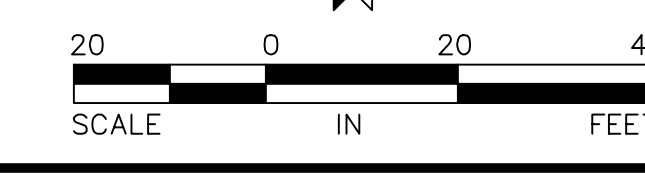
UTILITY NOTES

- 1.) Utility information from plans and markings was combined with observed evidence of utilities to develop a view of the underground utilities shown hereon. However, lacking excavation, the exact location of underground features cannot be accurately, completely and reliably depicted. In addition, Gopher State One Call requests from surveyors may be ignored or result in an incomplete response. Where additional or more detailed information is required, excavation and/or a private utility locate request may be necessary.
2.) Other underground utilities of which we are unaware may exist. Verify all utilities critical to construction or design.
3.) Some underground utility locations are shown as marked onsite by those utility companies whose locators responded to our Gopher State One Call, ticket number 202370702.
4.) Contact GOPHER STATE ONE CALL at 651-454-0002 (800-252-1166) for precise onsite location of utilities prior to any excavation.

I hereby certify that this survey, plan, or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.

Dated this 30th day of September, 2020
SUNDE LAND SURVEYING, LLC.
By: Leonard F. Carlson, P.L.S. Minn. Lic. No. 44890

Revision table, Drawing Title: PRELIMINARY PLAT OF SHELBY COMMONS, 188 1/2 GLENWOOD AVENUE, MINNEAPOLIS, MN, SUNDE LAND SURVEYING logo and contact info, Project: 2016-103-A, Date: 9/24/2020, Sheet: 1 of 1



Hennepin County, Minnesota. Abstract Property

PROJECT TITLE

SHELBY COMMONS

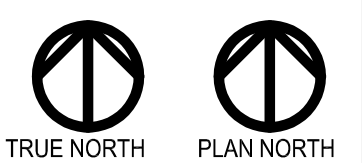
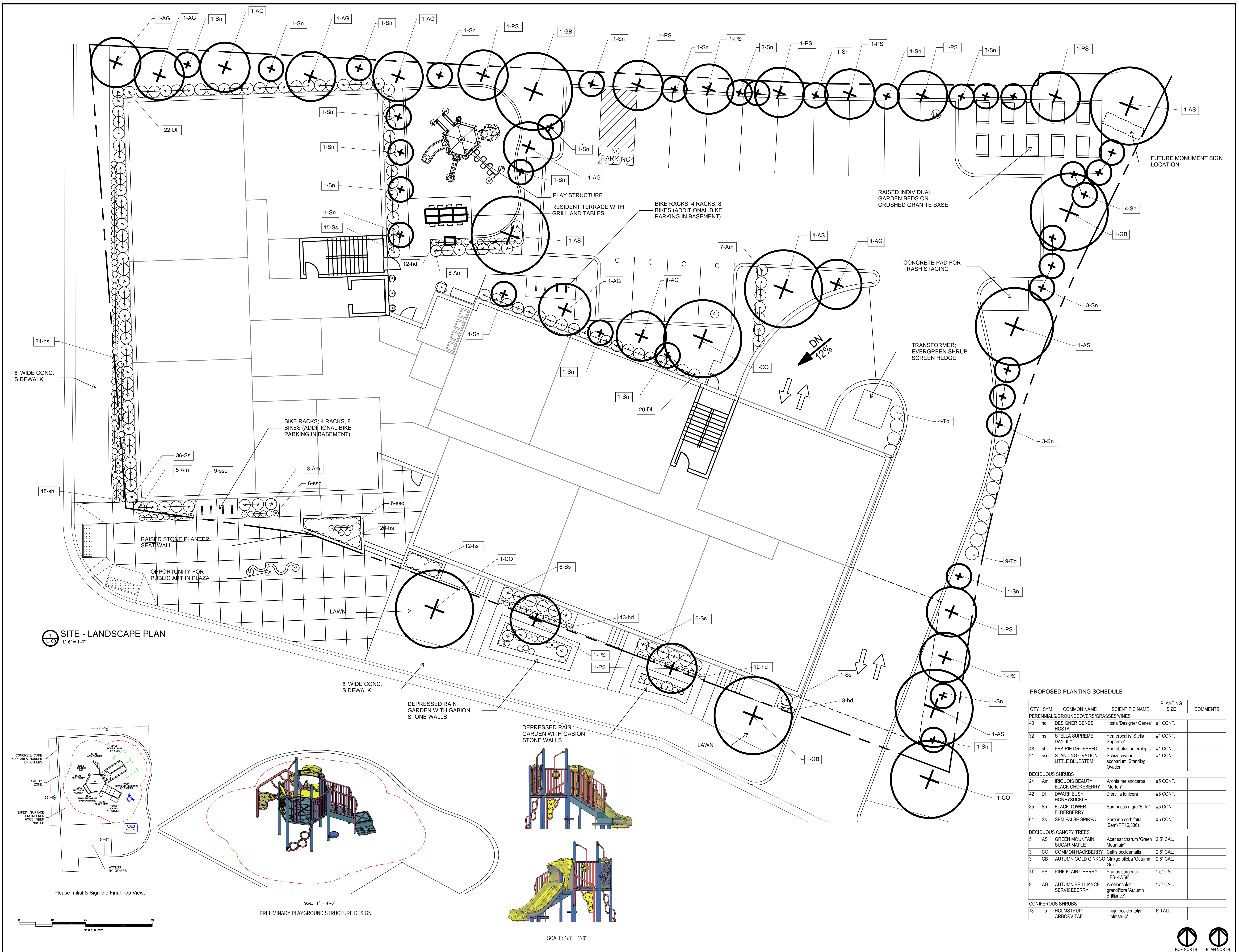
ISSUE #	DATE	DESCRIPTION
1	10/02/2020	LAND USE APPLICATION
2	10/02/2020	PDR SUBMITTAL

CERTIFICATION

NOT FOR CONSTRUCTION

DRAWN BY	
CHECKED BY	
COMMISSION NUMBER	
SHEET TITLE	

SHEET NUMBER





SHELBY COMMONS										9/18/2020					
Unit Type	Level	1-BED	2-BED	3-BED	Total	Amenities & Support				Tabulations					
						Lobby	Amenity	MEP & Services	Loading & Trash	Parking Area	Parking Count	Gross Building...	GRSF	NRSF	Efficiency (NRSF / (GROSS BLDG ...))
Area (SF)	P1											13,380			
	1	2	4	3	9	571	951	775	347	13,380	33	12,954	10,228	8,656	66.82%
	2	3	7	3	13			95	75			14,227	13,757	12,032	84.57%
	3	3	6	3	12		700	95	75			13,023	12,947	11,218	86.14%
	4	3	6	3	12			95	75			13,023	12,947	11,218	86.14%
Unit Total per Type	11	23	12	46	571	1,651	1,131	647	13,380	50	29,995	66,607	49,879	43,124	81.0%
Parking Ratio	1.00	1.00	1.00	1.09					268	Area/s...					
Total Req'd.	11	23	12	46					1.09	Parkg...					
Unit Count	11	23	12	46											
Bed Count	11	46	36	93											
NRSF by Type	7,550	22,036	13,448	43,124											
NRSF Ave. unit				937											
Bed Mix	23.91%	50.00%	26.09%												

Parcel ID	Parcel area	Zoning Distric Standards	Allowable	Proposed
220 292 433 0053 188 1/2 Glenwood Ave	29995	Underlying FAR	2.7	
		Density bonus for at least 20% units are affordable	0.54	
		Total FAR	3.24	1.77
		Building Height	4 Stories / 56 ft	4 Stories
		Lot area minimum per dwelling unit	N/A	
		Total Unit Density	n/a	46 Units (67/acre)
		Residential Parking Stalls	1.7 per Unit	17
				33
		Total Parking Stalls	78	50
		Land Use Application Requests		
		1. Subdivision Approval		
Total Lot Area	29995			



PROJECT TITLE

SHELBY COMMONS

ISSUE #	DATE	DESCRIPTION
1	10/02/2020	LAND USE APPLICATION
2	10/02/2020	PDR SUBMITTAL

CERTIFICATION

NOT FOR CONSTRUCTION

DRAWN BY	Author
CHECKED BY	Checker
COMMISSION NUMBER	2385 03

SHEET TITLE

SITE PLAN

SHEET NUMBER

A1-0



PROJECT TITLE

**SHELBY
COMMONS**

ISSUE #	DATE	DESCRIPTION
1	10/22/2020	LAND USE APPLICATION

CERTIFICATION

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CONSTRUCTION

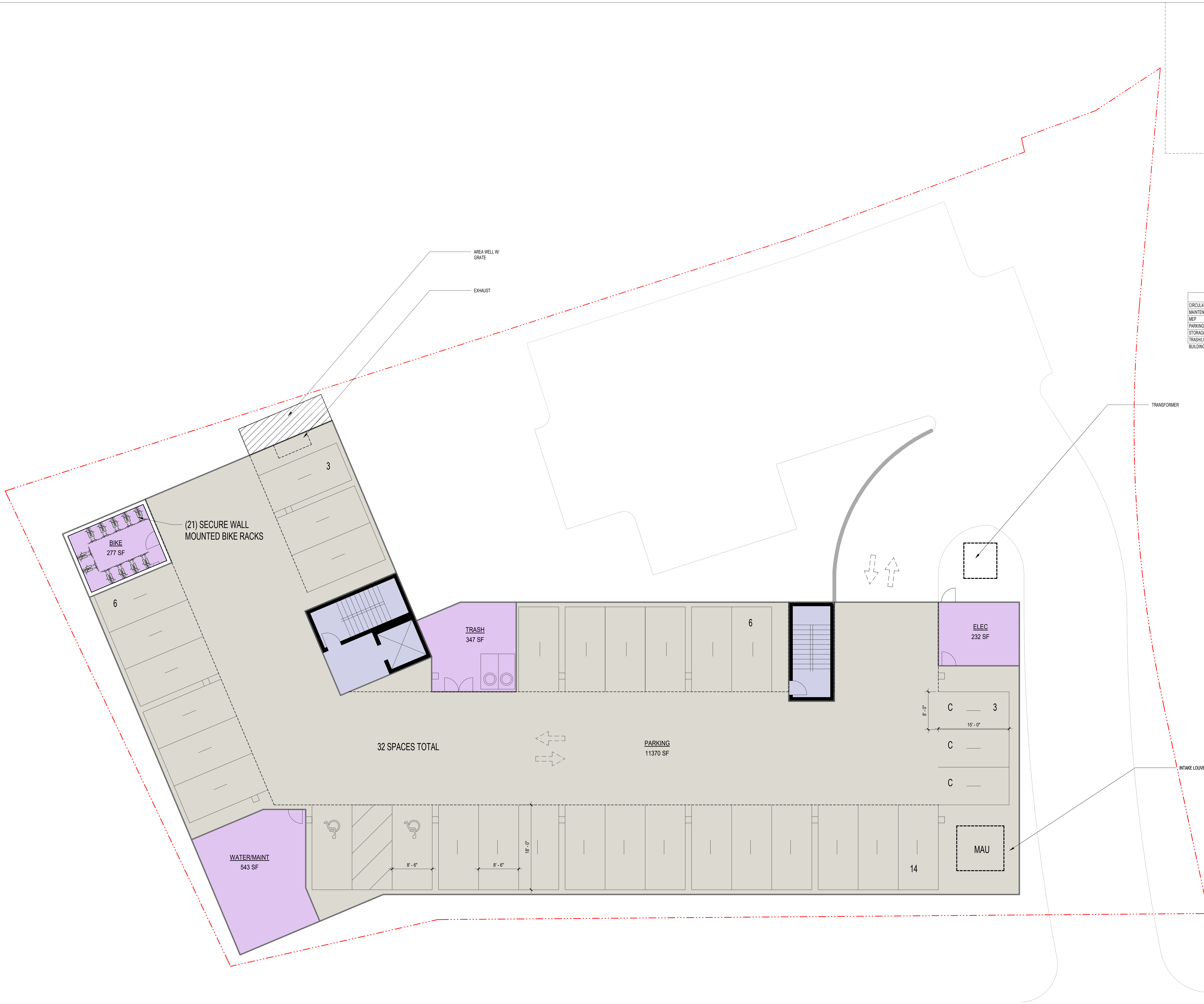
DRAWN BY	Author
CHECKED BY	Checker
COMMISSION NUMBER	2385 03

SHEET TITLE

GARAGE PLAN

SHEET NUMBER

A2-0.0



LEVEL P1 - OPTION 3

CIRCULATION	610 SF
MAINTENANCE	543 SF
MEP	232 SF
PARKING	11370 SF
STORAGE	277 SF
TRASH/LOADING	347 SF
BUILDING AREA	13380 SF

PROJECT TITLE

**SHELBY
COMMONS**

ISSUE #	DATE	DESCRIPTION

CERTIFICATION

NOT FOR
CONSTRUCTION

DRAWN BY	Author
CHECKED BY	Checker
COMMISSION NUMBER	2385-03

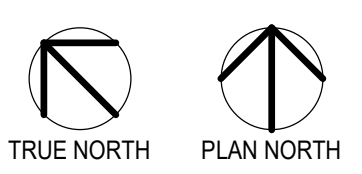
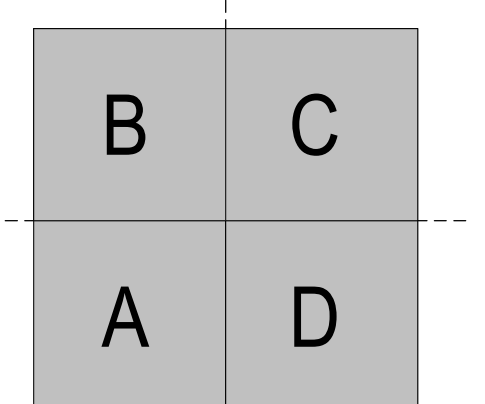
SHEET TITLE

LEVEL 1

SHEET NUMBER

A2-0.1

LEVEL 1	
AMENITY	951 SF
AMENITY / LAUNDRY	127 SF
AMENITY / MAIL	230 SF
CIRCULATION	1672 SF
LEASING	350 SF
LOBBY	571 SF
MEP	71 SF
SERVICES	350 SF
TRASHLOADING	75 SF
UNIT 1 BED	1362 SF
UNIT 2 BED	2912 SF
UNIT 3 BED	3362 SF
BUILDING AREA	12954 SF



PROJECT TITLE

**SHELBY
COMMONS**

ISSUE #	DATE	DESCRIPTION

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DRAWN BY	Author
CHECKED BY	Checker
COMMISSION NUMBER	2385-03

SHEET TITLE

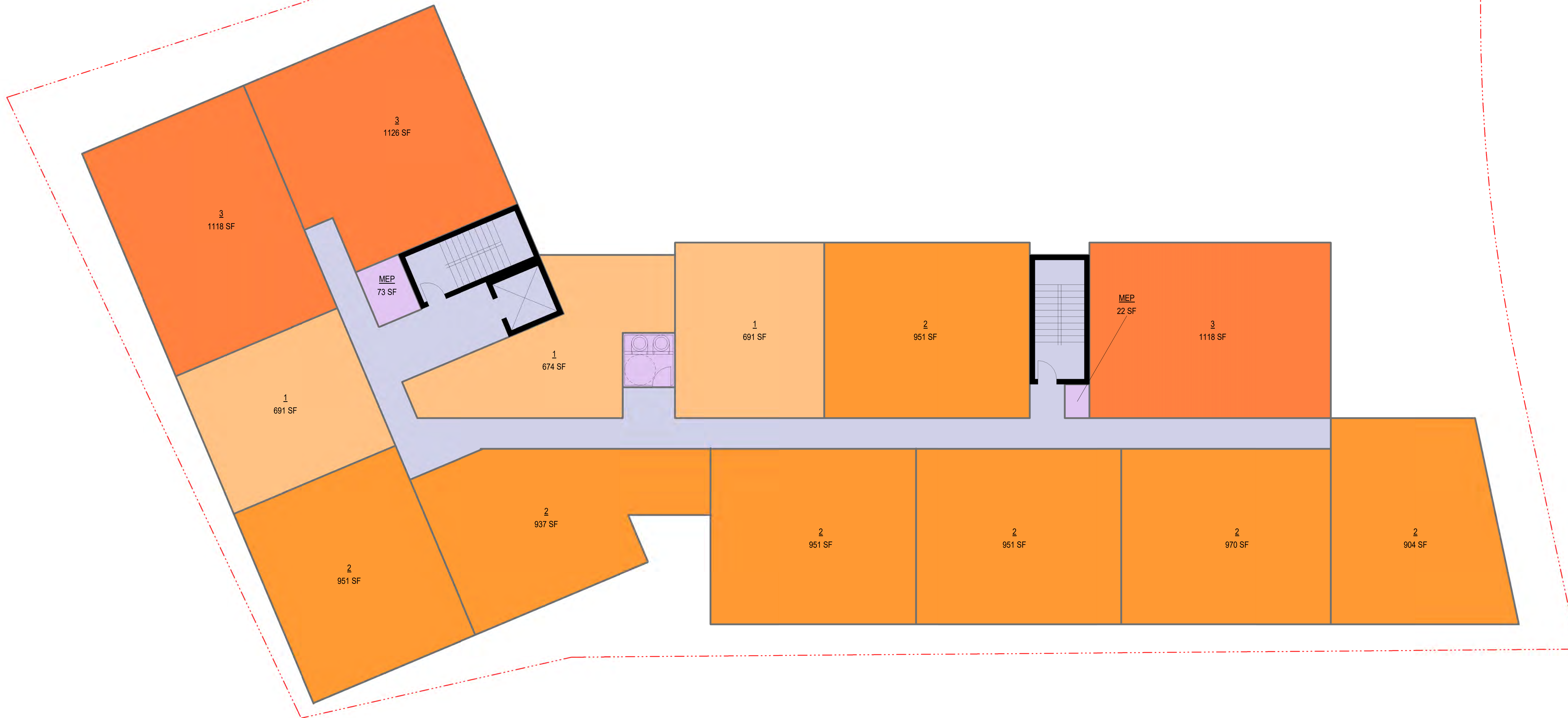
LEVEL 2

SHEET NUMBER

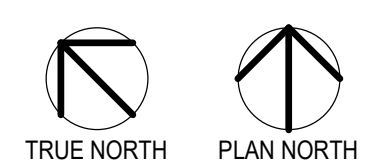
A2-0.2

STAMPING AREA

LEVEL 2	
CIRCULATION	1775 SF
MEP	95 SF
TRASHLOADING	75 SF
UNIT 1 BED	2056 SF
UNIT 2 BED	6614 SF
UNIT 3 BED	3382 SF
BUILDING AREA	13227 SF



1 LEVEL 2
A2-0.2 1/8" = 1'-0"



PROJECT TITLE

**SHELBY
COMMONS**

ISSUE #	DATE	DESCRIPTION

CERTIFICATION

NOT FOR
CONSTRUCTION

DRAWN BY	Author
CHECKED BY	Checker
COMMISSION NUMBER	2385 03

SHEET TITLE

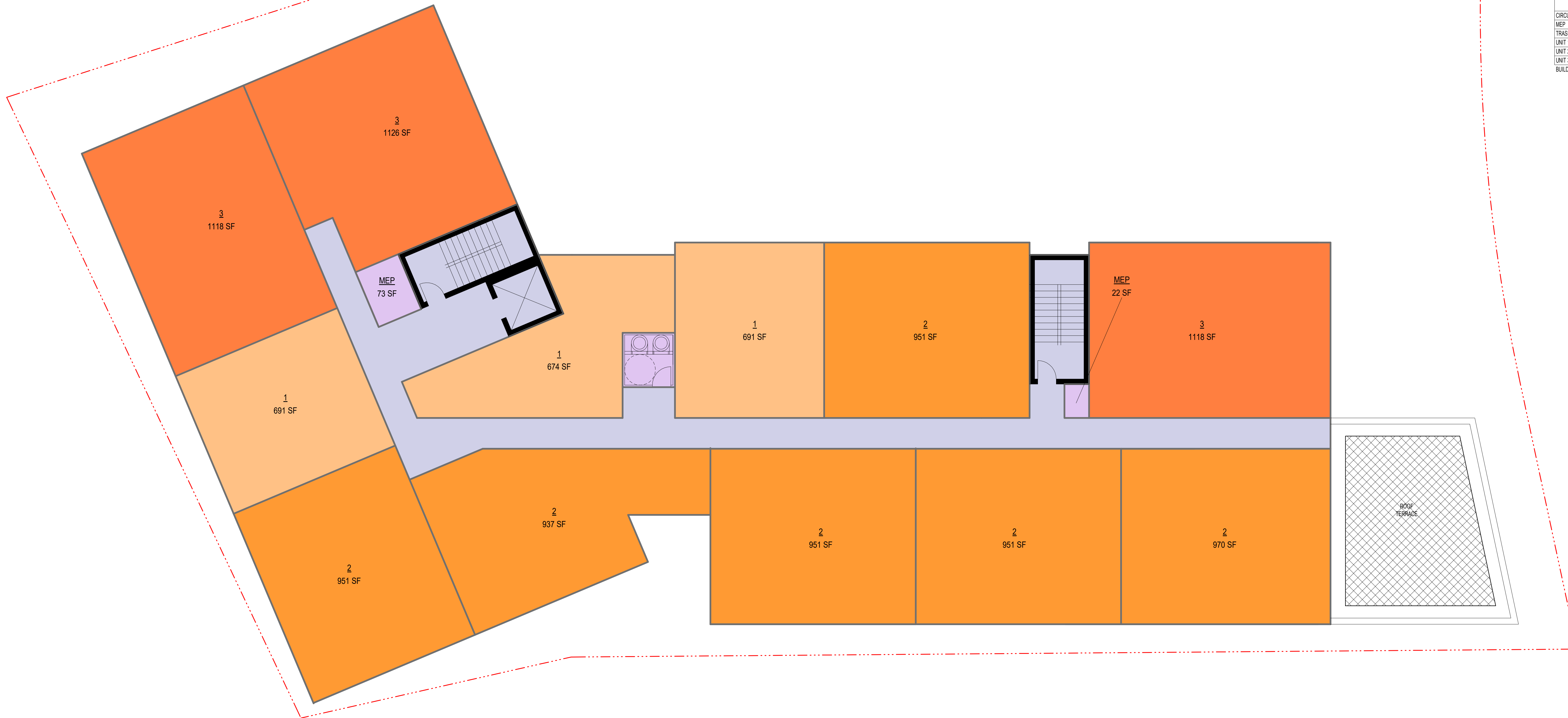
LEVEL 3

SHEET NUMBER

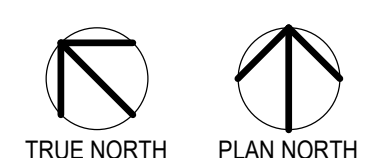
A2-0.3

STAMPING AREA

LEVEL 3	
CIRCULATION	1775 SF
MEP	95 SF
TRASHLOADING	75 SF
UNIT 1 BED	2056 SF
UNIT 2 BED	5710 SF
UNIT 3 BED	1302 SF
BUILDING AREA	13023 SF



1 LEVEL 3
A2-0.3 1/8" = 1'-0"

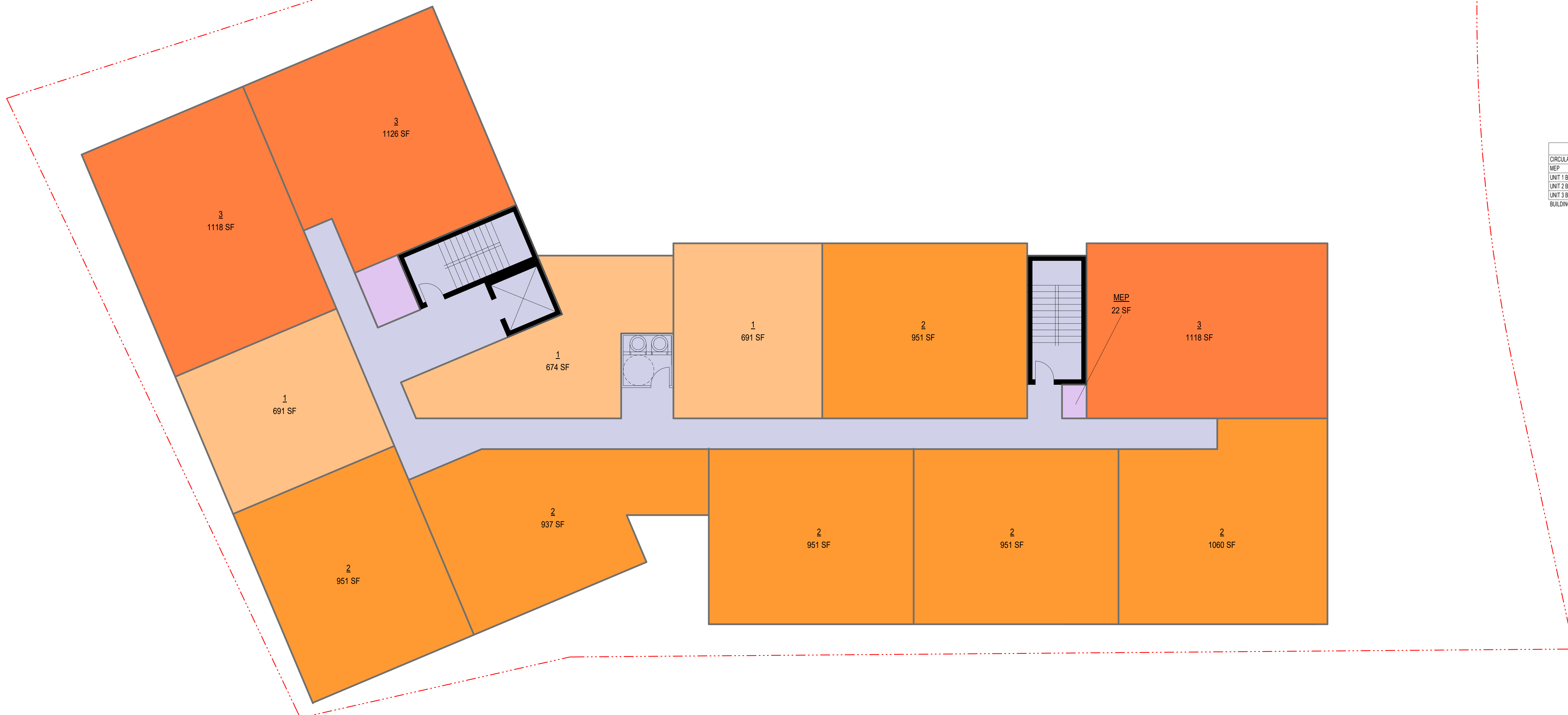


PROJECT TITLE

**SHELBY
COMMONS**

ISSUE #	DATE	DESCRIPTION

LEVEL 4	
CIRCULATION	1711 SF
MEP	95 SF
UNIT 1 BED	2056 SF
UNIT 2 BED	3600 SF
UNIT 3 BED	1382 SF
BUILDING AREA	13023 SF



1
A2-0.4
LEVEL 4
1/8" = 1'-0"

CERTIFICATION

NOT FOR
CONSTRUCTION

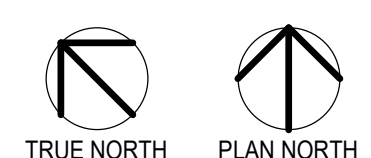
DRAWN BY	Author
CHECKED BY	Checker
COMMISSION NUMBER	2385 03

SHEET TITLE

LEVEL 4

SHEET NUMBER

A2-0.4



PROJECT TITLE

**SHELBY
COMMONS**

ISSUE #	DATE	DESCRIPTION
1	10/02/2020	LAND USE APPLICATION

CERTIFICATION

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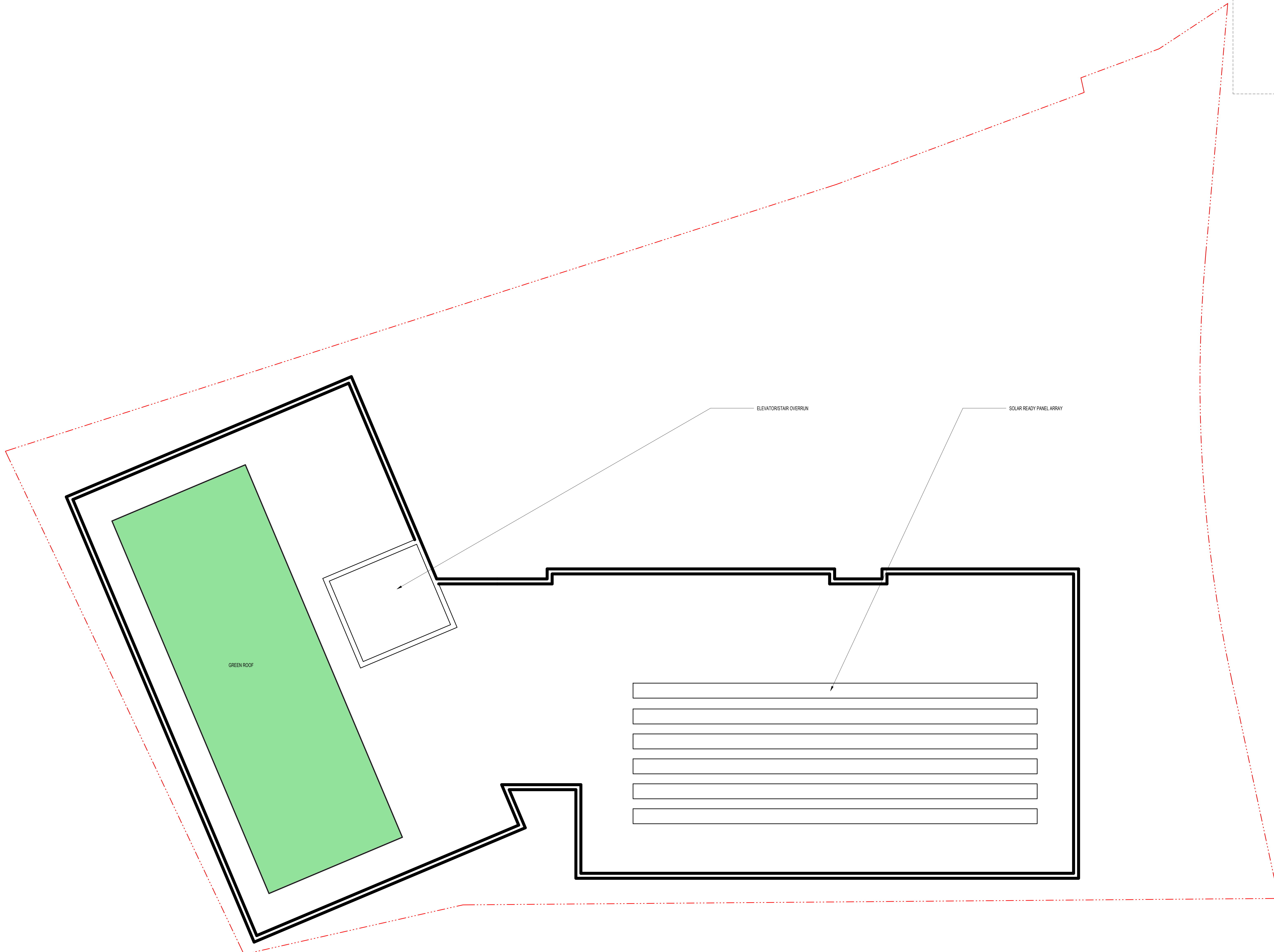
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COMMISSION NUMBER	2385-03

SHEET TITLE

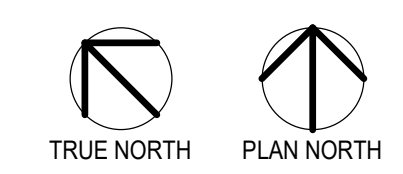
ROOF PLAN

SHEET NUMBER

A2-0.5



6 ROOF PLAN
A2-0.5 1/8" = 1'-0"



PROJECT TITLE

**SHELBY
COMMONS**

ISSUE #	DATE	DESCRIPTION
1	10/02/2020	LAND USE APPLICATION
2	10/02/2020	PDR SUBMITTAL

CERTIFICATION

NOT FOR CONSTRUCTION

DRAWN BY	Author
CHECKED BY	Checker
COMMISSION NUMBER	2385 03
SHEET TITLE	
ELEVATIONS	
SHEET NUMBER	
A3-0	

STAMPING AREA



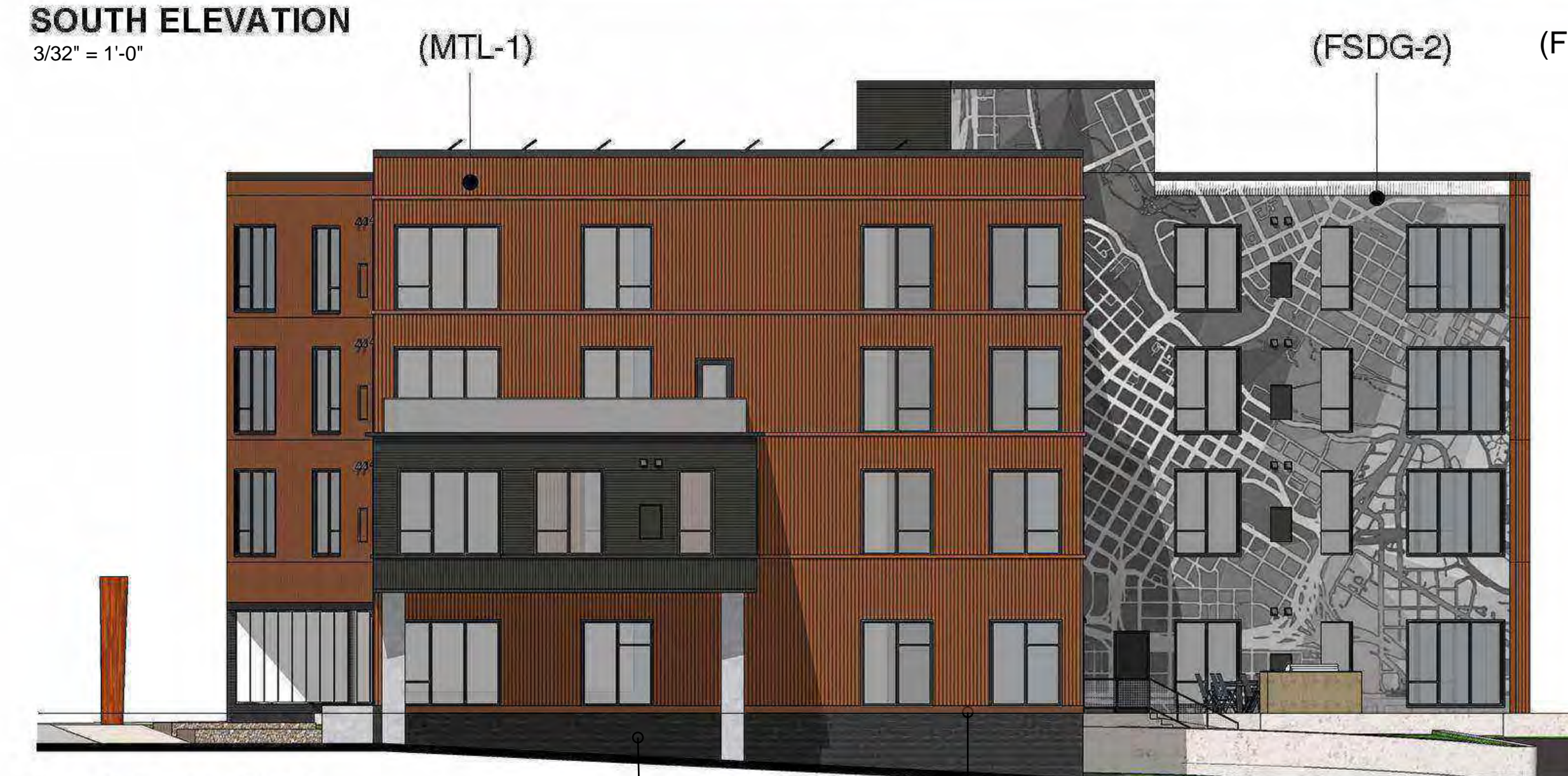
South Elevation

Overall Area	8612
Glass	33%
FSDG - Lap	17%
FSDG - Panel	0%
Brick	3%
Metal	47%
Total	100%

West Elevation

Overall Area	5352
Glass	30%
FSDG - Lap	1%
FSDG - Panel	15%
Brick	0%
Metal	54%
Total	100%

SOUTH ELEVATION
3/32" = 1'-0"



EAST ELEVATION
3/32" = 1'-0"

East Elevation

Overall Area	5132
Glass	26%
FSDG - Lap	7%
FSDG - Panel	25%
Brick	6%
Metal	36%
Total	100%

North Elevation

Overall Area	9288
Glass	32%
FSDG - Lap	12%
FSDG - Panel	0%
Brick	2%
Metal	54%
Total	100%



WEST ELEVATION
3/32" = 1'-0"



NORTH ELEVATION
3/32" = 1'-0"

PROJECT TITLE

**SHELBY
COMMONS**

ISSUE #	DATE	DESCRIPTION
1	10/22/2020	LAND USE APPLICATION

CERTIFICATION

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CONSTRUCTION

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CHECKED BY	Checker
COMMISSION NUMBER	2385.03

SHEET TITLE

RENDERINGS

SHEET NUMBER

A4-0

STAMPING AREA



COR-TEN PAINTED
CORRUGATED METAL
(MTL-1)

FIBER CEMENT LAP
(FSDG-1)
PRODUCT: JAMES HARDIE
HARDIEPLANK OR SIMILAR
(MINIMUM LAP EXPOSURE)

FIBER CEMENT PANEL MURAL
(FSDG-2)
PRODUCT: JAMES HARDIE
HARDIEPANEL OR SIMILAR
(FLUSH JOINTS)

MASONRY BASE
(CMU-1)

AUTUMN BLEND BRICK
(FB-1)

PROJECT TITLE

**SHELBY
COMMONS**

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COMMISSION NUMBER	2385 03

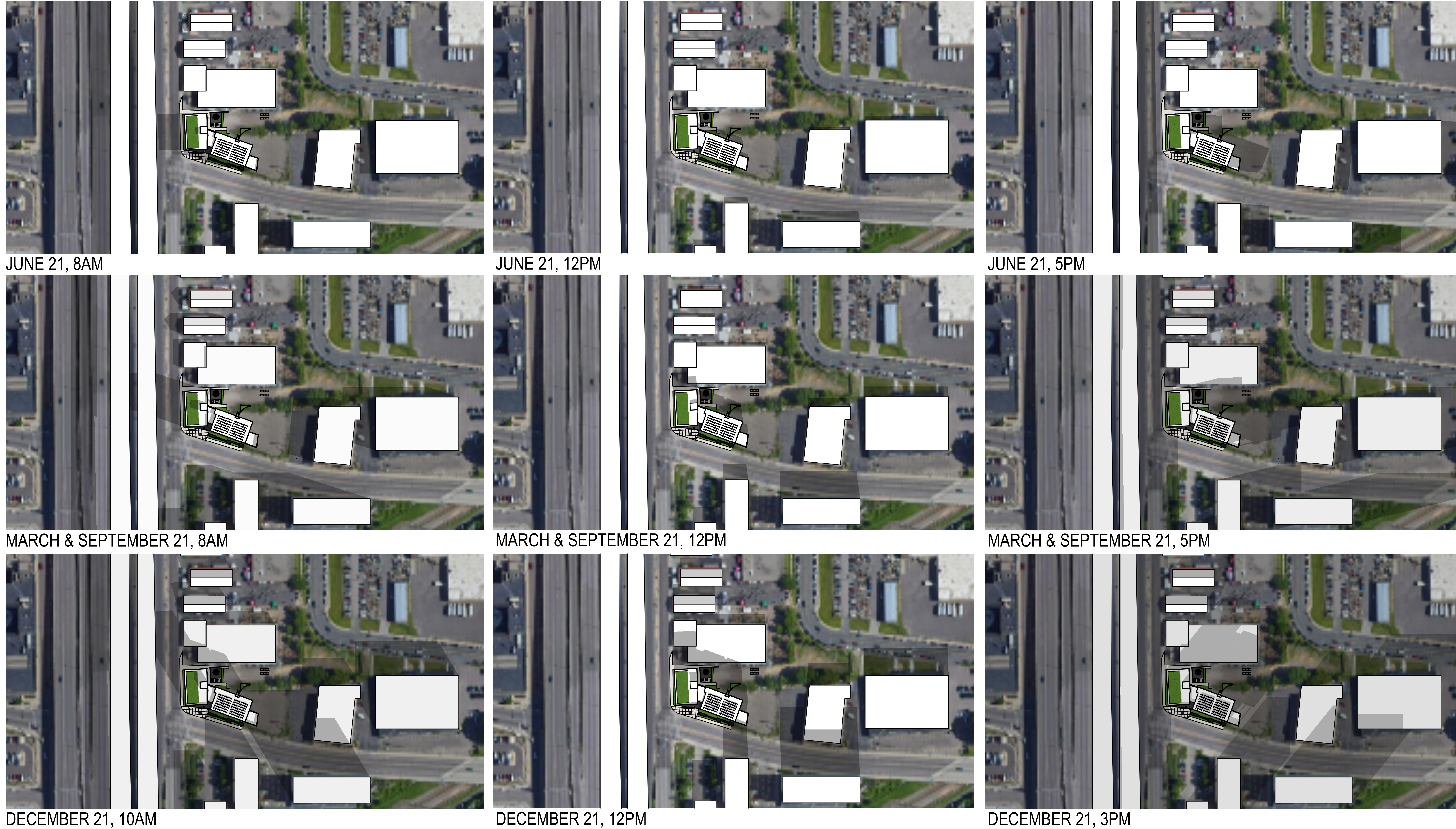
SHEET TITLE

SHADOW STUDY

SHEET NUMBER

A5-0

STAMPING AREA



JUNE 21, 8AM

JUNE 21, 12PM

JUNE 21, 5PM

MARCH & SEPTEMBER 21, 8AM

MARCH & SEPTEMBER 21, 12PM

MARCH & SEPTEMBER 21, 5PM

DECEMBER 21, 10AM

DECEMBER 21, 12PM

DECEMBER 21, 3PM

NOT TO SCALE



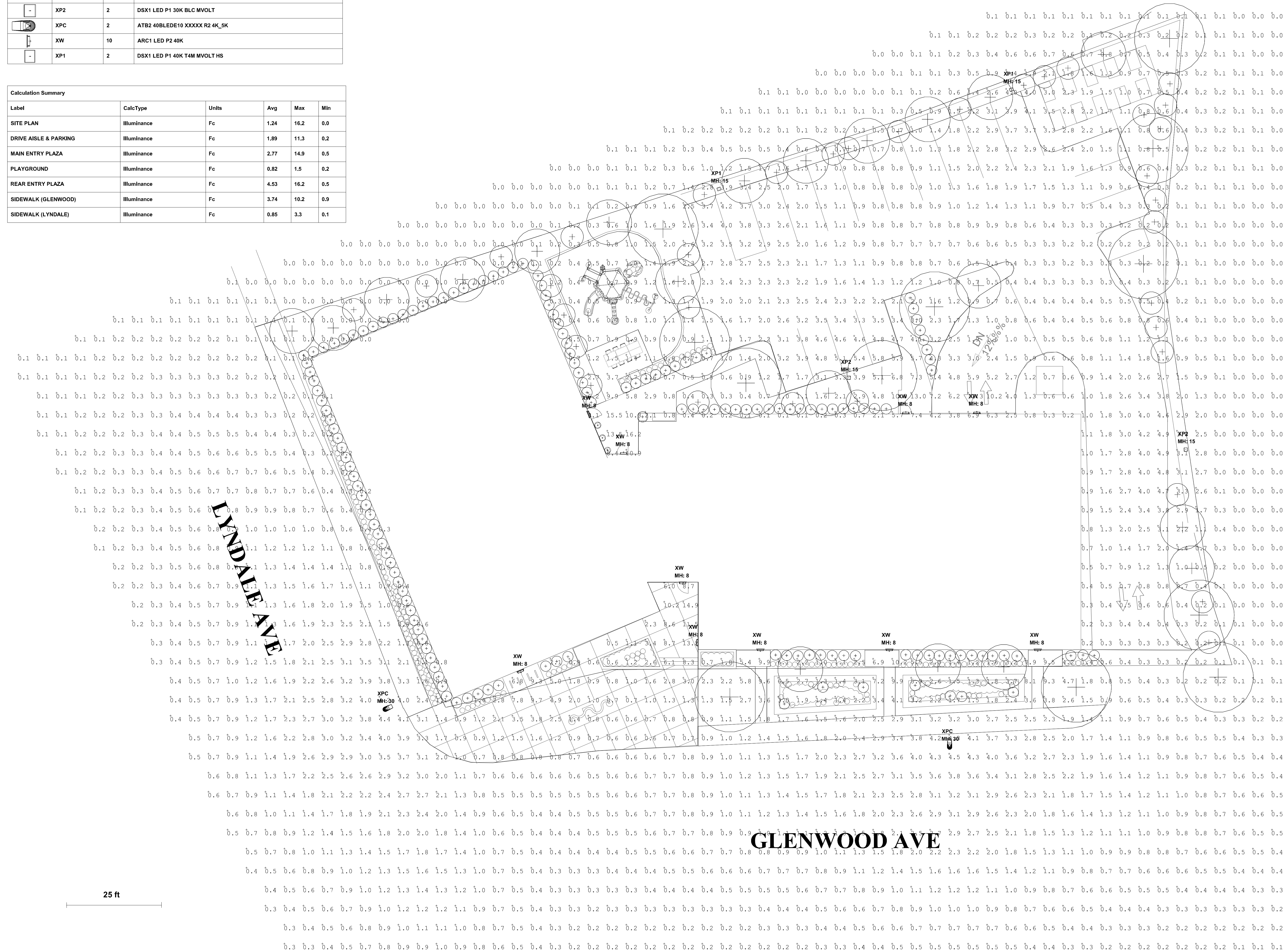
ISSUE #	DATE	DESCRIPTION
2	10/02/2020	PDR SUBMITTAL

NOT FOR
CONSTRUCTION

DRAWN BY	Author
CHECKED BY	Checker
COMMISSION NUMBER	2385 03

Symbol	Tag	Qty	Light Fixture Description
[Symbol]	XP2	2	DSX1 LED P1 30K BLC MVOLT
[Symbol]	XPC	2	ATB2 40BLEDE10 XXXXX R2 4K_SK
[Symbol]	XW	10	ARC1 LED P2 40K
[Symbol]	XP1	2	DSX1 LED P1 40K T4M MVOLTHS

Label	Calc Type	Units	Avg	Max	Min
SITE PLAN	Illuminance	Fc	1.24	16.2	0.0
DRIVE AISLE & PARKING	Illuminance	Fc	1.89	11.3	0.2
MAIN ENTRY PLAZA	Illuminance	Fc	2.77	14.9	0.5
PLAYGROUND	Illuminance	Fc	0.82	1.5	0.2
REAR ENTRY PLAZA	Illuminance	Fc	4.53	16.2	0.5
SIDEWALK (GLENWOOD)	Illuminance	Fc	3.74	10.2	0.9
SIDEWALK (LYNDALE)	Illuminance	Fc	0.85	3.3	0.1





WODA COOPER COMPANIES

500 South Front St
10th Floor
Columbus, Ohio 43215
Office: 614.396.3200
www.wodagroup.com

October 1, 2020

Minneapolis City Council, Ward 5
350 South 5th Street, Room 307
Minneapolis, MN 55415
Attn: Jeremiah Ellison

Dear Mr. Jeremiah Ellison:

Woda Cooper Development, Inc. is submitting a Land Use Application for a proposed family development located at 188 ½ Glenwood Avenue, Minneapolis, MN 55405 in the North Loop neighborhood. The Land Use Application is for a 4-story apartment building, known as Shelby Commons, an affordable housing development. Shelby Commons will be developed by Woda Cooper Development, Inc. with Project for Pride in Living, Inc. (PPL) as a co-developer. This will provide for the development of 46 low income housing units, all affordable. These units will consist of 11 one-bedroom units, 23 two-bedroom units, and 12 three-bedroom units.

PPL will be providing supportive services, including on-site case management, for 12 units. 5 of the supportive housing units will be one-bedroom units for persons with disability. PPL will use its Housing Supports contract to help subsidize the rents of these units. The remaining 7 supportive units will be set aside for high priority homeless with 4 of these units being one-bedroom units, and 3 being two-bedroom units. Shelby Commons has been awarded 12 project-based section 8 vouchers from the Minneapolis Public Housing Authority and is currently awaiting a response from Minnesota Housing Finance Agency and the Minneapolis Community Planning and Economic Development on its applications for 9% low income housing tax credits. Shelby Commons will use 7 of the 12 vouchers to subsidize the rents of the supportive units set aside for high priority homeless. The remaining 5 vouchers will be used on general occupancy units, 1 on a two-bedroom unit, and 4 on three-bedroom units.

Units will be available to residents at varying income levels, with some units set aside at 30%, 50%, 60%, and 80% of the HUD Multifamily Tax Subsidy Projects (MTSP) income limits. In addition to this, all units will be affordable to households at the 60% HUD MTSP income limits. This presents a great opportunity for low income households at or below the 80% income limit to find quality living at a rate which would be very affordable to such households.

Other amenity spaces Shelby Commons has to offer include a community room, community laundry, and a supportive services office. There will also be an outdoor playground for children along with community garden plots. Along Glenwood Avenue, Shelby Commons will feature a unique urban rain garden, and there will be an opportunity for public art, both on the sidewalk/entry plaza and on select portions of the building exterior facade. Because Shelby Commons is a large family development, we have provided parking at a ratio just over 1 to 1, with 47 total parking spaces, 32 of which are in an underground garage, and 15 of which are in a surface lot located at the rear of the building.

OFFICES LOCATED IN GEORGIA, INDIANA, KENTUCKY, MARYLAND, MICHIGAN, OHIO AND VIRGINIA.

WODA COOPER COMPANIES, INC. IS AN ESOP OWNED COMPANY AND PARENT COMPANY OF WODA CONSTRUCTION, INC., WODA MANAGEMENT & REAL ESTATE, LLC, AND WODA COOPER DEVELOPMENT, INC.

As part of the Land Use Application for Shelby Commons, Woda Cooper Development is also submitting a Conditional Use Permit and a Subdivision Application. The Conditional Use Permit is required for the 15-space surface lot. This property is located in the Downtown Parking Overlay District, so any surface parking as an accessory use requires a conditional use. The Subdivision Application will be used to replat the two tax parcels at 188 ½ Glenwood Ave. These parcels have tax identification numbers of 22-029-24-33-0053, and 22-029-24-33-0058. Woda Cooper Development is seeking to replat this as the land for the Shelby Commons development, and the rest as an outlot. The city of Minneapolis is hoping to construct a Border Avenue Extension project that will use this outlot to extend the existing Border Avenue to Glenwood Avenue as part of the reconstruction surrounding the Green Line Extension project. In speaking with the city, Woda Cooper Development was advised to replat as such to accommodate this future road extension.

Thank you for your consideration of our proposed development. Should you have any questions or comments, please do not hesitate to contact me at 614-396-0024 or pzee@wodagroup.com. I can also be reached by mail at 500 S Front St., 10th Floor, Columbus, OH 43215, Attn: Parker Zee.

Sincerely,



Parker Zee
Assistant Vice President of Development and Data Manager





WODA COOPER COMPANIES

500 South Front St
10th Floor
Columbus, Ohio 43215
Office: 614.396.3200
www.wodagroup.com

October 1, 2020

North Loop Neighborhood Association
P.O. Box 580672
Minneapolis, MN 55458
Attn: Tim Bildsoe

Dear Mr. Tim Bildsoe:

Woda Cooper Development, Inc. is submitting a Land Use Application for a proposed family development located at 188 ½ Glenwood Avenue, Minneapolis, MN 55405 in the North Loop neighborhood. The Land Use Application is for a 4-story apartment building, known as Shelby Commons, an affordable housing development. Shelby Commons will be developed by Woda Cooper Development, Inc. with Project for Pride in Living, Inc. (PPL) as a co-developer. This will provide for the development of 46 low income housing units, all affordable. These units will consist of 11 one-bedroom units, 23 two-bedroom units, and 12 three-bedroom units.

PPL will be providing supportive services, including on-site case management, for 12 units. 5 of the supportive housing units will be one-bedroom units for persons with disability. PPL will use its Housing Supports contract to help subsidize the rents of these units. The remaining 7 supportive units will be set aside for high priority homeless with 4 of these units being one-bedroom units, and 3 being two-bedroom units. Shelby Commons has been awarded 12 project-based section 8 vouchers from the Minneapolis Public Housing Authority and is currently awaiting a response from Minnesota Housing Finance Agency and the Minneapolis Community Planning and Economic Development on its applications for 9% low income housing tax credits. Shelby Commons will use 7 of the 12 vouchers to subsidize the rents of the supportive units set aside for high priority homeless. The remaining 5 vouchers will be used on general occupancy units, 1 on a two-bedroom unit, and 4 on three-bedroom units.

Units will be available to residents at varying income levels, with some units set aside at 30%, 50%, 60%, and 80% of the HUD Multifamily Tax Subsidy Projects (MTSP) income limits. In addition to this, all units will be affordable to households at the 60% HUD MTSP income limits. This presents a great opportunity for low income households at or below the 80% income limit to find quality living at a rate which would be very affordable to such households.

Other amenity spaces Shelby Commons has to offer include a community room, community laundry, and a supportive services office. There will also be an outdoor playground for children along with community garden plots. Along Glenwood Avenue, Shelby Commons will feature a unique urban rain garden, and there will be an opportunity for public art, both on the sidewalk/entry plaza and on select portions of the building exterior facade. Because Shelby Commons is a large family development, we have provided parking at a ratio just over 1 to 1, with 47 total parking spaces, 32 of which are in an underground garage, and 15 of which are in a surface lot located at the rear of the building.

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As part of the Land Use Application for Shelby Commons, Woda Cooper Development is also submitting a Conditional Use Permit and a Subdivision Application. The Conditional Use Permit is required for the 15-space surface lot. This property is located in the Downtown Parking Overlay District, so any surface parking as an accessory use requires a conditional use. The Subdivision Application will be used to replat the two tax parcels at 188 ½ Glenwood Ave. These parcels have tax identification numbers of 22-029-24-33-0053, and 22-029-24-33-0058. Woda Cooper Development is seeking to replat this as the land for the Shelby Commons development, and the rest as an outlot. The city of Minneapolis is hoping to construct a Border Avenue Extension project that will use this outlot to extend the existing Border Avenue to Glenwood Avenue as part of the reconstruction surrounding the Green Line Extension project. In speaking with the city, Woda Cooper Development was advised to replat as such to accommodate this future road extension.

Thank you for your consideration of our proposed development. Should you have any questions or comments, please do not hesitate to contact me at 614-396-0024 or pzee@wodagroup.com. I can also be reached by mail at 500 S Front St., 10th Floor, Columbus, OH 43215, Attn: Parker Zee.

Sincerely,



Parker Zee
Assistant Vice President of Development and Data Manager





WODA COOPER COMPANIES

500 South Front St
10th Floor
Columbus, Ohio 43215

Office: 614.396.3200

www.wodagroup.com

October 1, 2020

Heritage Park Neighborhood Association
1000 Olson Memorial Highway
Minneapolis, MN 55411
Attn: Victoria Bayerl

Dear Ms. Victoria Bayerl:

Woda Cooper Development, Inc. is submitting a Land Use Application for a proposed family development located at 188 ½ Glenwood Avenue, Minneapolis, MN 55405 in the North Loop neighborhood. The Land Use Application is for a 4-story apartment building, known as Shelby Commons, an affordable housing development. Shelby Commons will be developed by Woda Cooper Development, Inc. with Project for Pride in Living, Inc. (PPL) as a co-developer. This will provide for the development of 46 low income housing units, all affordable. These units will consist of 11 one-bedroom units, 23 two-bedroom units, and 12 three-bedroom units.

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Sincerely,



Parker Zee
Assistant Vice President of Development and Data Manager





WODA COOPER COMPANIES

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10th Floor
Columbus, Ohio 43215
Office: 614.396.3200
www.wodagroup.com

October 1, 2020

Harrison Neighborhood Association
503 Irving Ave N, Suite 100
Minneapolis, MN 55405
Attn: Nichole Buehler

Dear Ms. Nichole Buehler, Ms. Angela Bonfiglio, and Mr. Denetrick Powers:

Woda Cooper Development, Inc. is submitting a Land Use Application for a proposed family development located at 188 ½ Glenwood Avenue, Minneapolis, MN 55405 in the North Loop neighborhood. The Land Use Application is for a 4-story apartment building, known as Shelby Commons, an affordable housing development. Shelby Commons will be developed by Woda Cooper Development, Inc. with Project for Pride in Living, Inc. (PPL) as a co-developer. This will provide for the development of 46 low income housing units, all affordable. These units will consist of 11 one-bedroom units, 23 two-bedroom units, and 12 three-bedroom units.

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Sincerely,



Parker Zee

Assistant Vice President of Development and Data Manager

